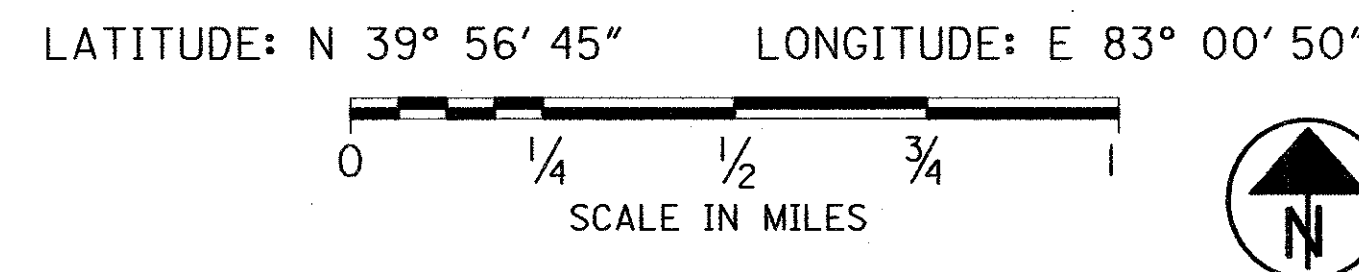


LOCATION MAP



PORTION TO BE IMPROVED \_\_\_\_\_

STATE & FEDERAL ROUTES \_\_\_\_\_

OTHER ROADS \_\_\_\_\_

DESIGN DESIGNATION

CURRENT DAY ADT (2000) \_\_\_\_\_ 123,950

DESIGN YEAR ADT (2020) \_\_\_\_\_ 148,800

DESIGN HOURLY VOLUME (2021) \_\_\_\_\_,---

DIRECTIONAL DISTRIBUTION \_\_\_\_\_ 50 %

TRUCKS (24 HOUR B&C) \_\_\_\_\_ 9 %

DESIGN SPEED \_\_\_\_\_ 60 M.P.H.

LEGAL SPEED \_\_\_\_\_ 55 M.P.H.

DESIGN FUNCTIONAL CLASSIFICATION - IR71 - URBAN INTERSTATE

- SR315 - URBAN EXPRESSWAY

STATE OF OHIO  
DEPARTMENT OF TRANSPORTATION

# FRA-IR71-14.39

# FRA-315-0.00

CITY OF COLUMBUS  
FRANKLIN COUNTY

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PROJECT DESCRIPTION

PROPOSED CONSTRUCTION INCLUDES FULL DEPTH RECONSTRUCTION OF 0.48 MILES OF FOUR LANE DIVIDED URBAN EXPRESSWAY AND THE MILL AND FILL OF 0.56 MILES OF URBAN INTERSTATE. MILL AND FILL OPERATIONS INCLUDE EXIT AND ENTRANCE RAMP TO GREENLAWN AVENUE.

1997 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

PRIMA FACIE SPEED LIMIT

UNDER AUTHORITY OF SECTION 4511.21, DIVISION (I) OF THE REVISED CODE OF OHIO, THE REVISED PRIMA FACIE SPEED LIMITS AS INDICATED HEREIN ARE DETERMINED TO BE REASONABLE AND SAFE, AND ARE HEREBY ESTABLISHED FOR THE DURATION OF THIS PROJECT. THE PRIMA FACIE SPEED LIMIT OR LIMITS HEREBY ESTABLISHED SHALL BECOME EFFECTIVE WHEN APPROPRIATE SIGNS GIVING NOTICE THEREOF ARE ERECTED.

DESIGN EXCEPTIONS - S.R. 315	APPROVAL DATE	SHEET No's
SHOULDER WIDTH	05/17/01	17-18
STOPPING SIGHT DISTANCE	05/17/01	36, 42-44
HORIZONTAL CLEARANCE	05/17/01	9
VERTICAL CLEARANCE	05/17/01	42-43
SUPERELEVATION	05/17/01	5

PLAN PREPARED BY:  
**ms consultants, inc.**  
CONSULTING ENGINEERS & PLANNERS  
2221 SCHROCK ROAD, COLUMBUS, OHIO

ENGINEER'S SEAL

ROADWAY

STRUCTURE



*Larry J. Shannon*  
5-16-02

STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
TC-65.10M	11/01/95	BP-1.1	07/28/00	GR-1.1M	10/21/97	RM-4.3M	10/21/97	CB-2.1M	07/12/95	806 09/09/97	
TC-65.11M	11/01/95	BP-1.2	07/28/00	GR-1.2M	01/03/96	RM-4.4M	10/21/97	CB-2.2M	07/12/95	863 10/12/99	
TC-65.12M	11/01/95	BP-2.1	07/28/00	GR-1.3M	11/30/94	RM-4.5M	10/21/97	CB-2.3M	07/12/95	906 05/05/98	
TC-71.10M	09/01/93	BP-2.2	07/28/00	GR-2.1M	04/14/98					908 03/28/00	
TC-72.20M	09/01/93	BP-2.3	07/28/00	GR-3.1M	10/21/97					1082 11/01/00	
		BP-3.1	07/28/00	GR-3.5M	10/21/97			I-1.2M	09/06/95		
		BP-6.1	07/28/00	GR-4.1M	11/30/94						
		BP-8.1	07/28/00	GR-4.2M	10/21/97						
		BP-9.1	07/28/00	GR-4.3M	10/21/97						
				GR-4.4M	11/30/94						
				GR-5.1M	04/21/95						
				GR-5.2M	11/30/94						
				GR-5.3M	11/30/94						

**UNDERGROUND UTILITIES**  
TWO WORKING DAYS  
BEFORE YOU DIG  
CALL 1-800-362-2764 (TOLL FREE)  
OHIO UTILITIES PROTECTION SERVICE  
NON-MEMBERS  
MUST BE CALLED DIRECTLY

FRA-IR71-14.39  
FRA-315-0.00

RAILROAD INVOLVEMENT

NONE

CONSTRUCTION PROJECT NO.

3009 (00)

PID NO.

20681

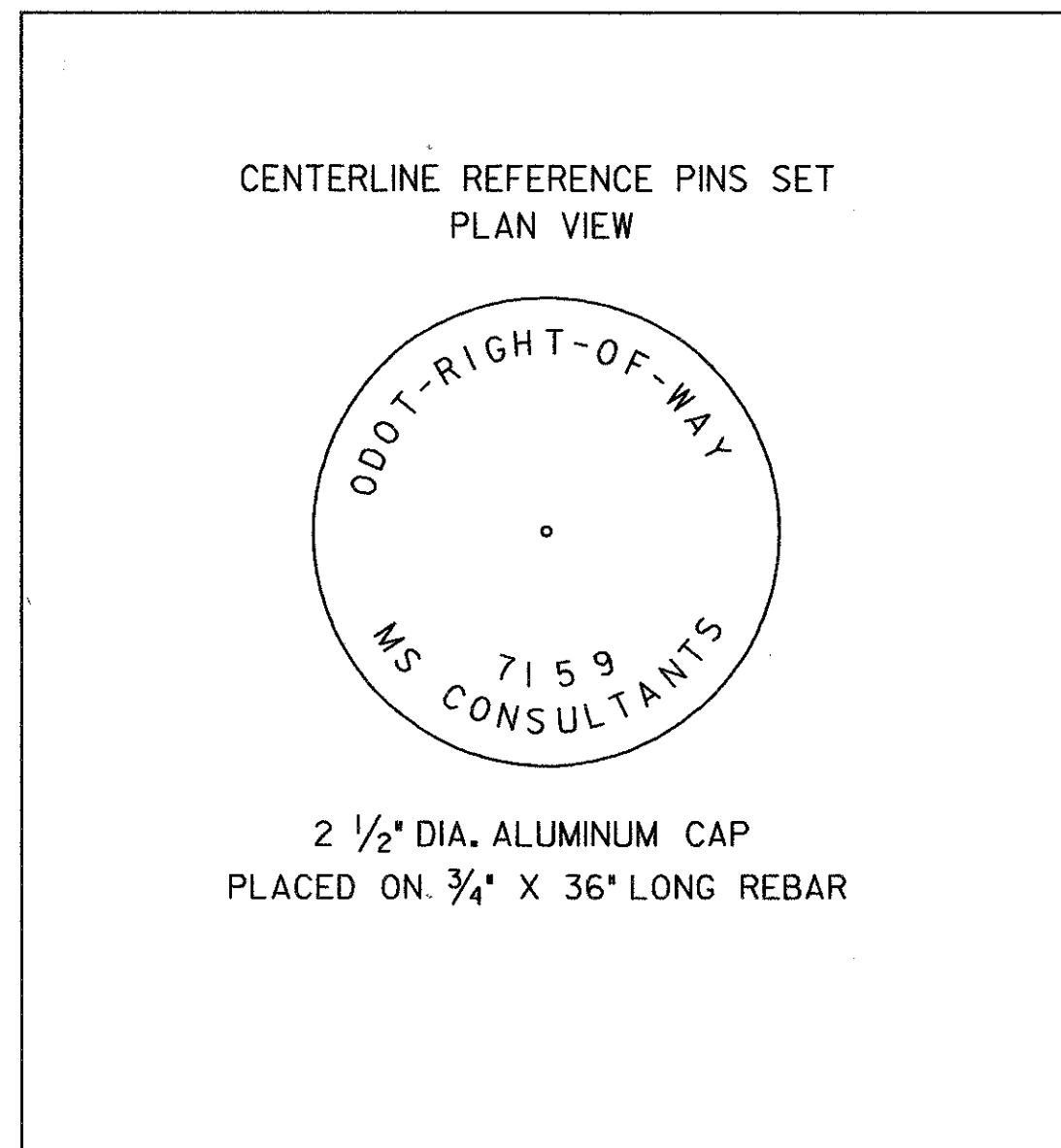
FEDERAL PROJECT NO.

TE21-G000(217)

FRA-171-14.39, FRA-315-0.00

VMS NOS. 1393 & 422  
VIRGINIA MILITARY DISTRICT  
CITY OF COLUMBUS  
FRANKLIN COUNTY, OHIO

PROJECT CONTROL MONUMENTS - CENTERLINE REFERENCE PINS SET STATE PLANE COORDINATES - OH SOUTH ZONE (PROJECT GRID FACTOR = 1.000093619)			
PT.	STATION	OFFSET	TYPE
	NORTHING	EASTING	
10	A-44+00.00	60.00' RT	3/4" REBAR
	709557.3073	1855764.3330	W/ AL. CAP
11	B-44+00.00	95.00' LT.	3/4" REBAR
	618922.5148	1855575.4783	W/ AL. CAP
12	B-46+76.07	100' 00' LT.	3/4" REBAR
	709527.2490	1855536.6443	W/ AL. CAP
13	B-46+76.07	40.00' LT.	3/4" REBAR
	709554.3614	1855590.1695	W/ AL. CAP
14	A-49+05.22	50.00' RT.	3/4" REBAR
	709825.0695	1855617.4949	W/ AL. CAP
15	A-49+05.22	100.00' RT.	3/4" REBAR
	709847.6627	1855662.0993	W/ AL. CAP
16	A-52+05.22	50.00' RT.	3/4" REBAR
	710092.6768	1855487.7398	W/ AL. CAP
17	A-52+05.22	50.00' LT.	3/4" REBAR
	710052.1128	1855396.3365	W/ AL. CAP
18	B-52+40.07	30.00' RT.	3/4" REBAR
	710078.3484	1855371.9698	W/ AL. CAP
19	B-52+40.07	50.00' LT.	3/4" REBAR
	710035.3604	1855304.5010	W/ AL. CAP
20	B-53+10.40	30.00' LT.	3/4" REBAR
	710143.0348	1855342.6120	W/ AL. CAP
21	B-53+10.40	50.00' RT.	3/4" REBAR
	710100.0469	1855275.1432	W/ AL. CAP
22	A-54+72.72	30.00' LT.	3/4" REBAR
	710312.2586	1855316.9205	W/ AL. CAP
23	A-54+72.72	50.00' RT.	3/4" REBAR
	710337.4965	1855392.8352	W/ AL. CAP
24	B-56+10.40	80.00' LT.	3/4" REBAR
	710340.8903	1855082.3649	W/ AL. CAP
25	B-56+10.40	40.00' LT.	3/4" REBAR
	710340.8903	1855082.3649	W/ AL. CAP
26	A-57+72.72	30.00' LT.	3/4" REBAR
	710601.2903	1855231.2204	W/ AL. CAP
27	A-57+72.72	50.00' RT.	3/4" REBAR
	710622.6872	1855308.3059	W/ AL. CAP
28	A-60+91.49	30.00' LT.	3/4" REBAR
	710908.4328	18551459654	W/ AL. CAP
29	A-60+91.49	50.00' RT.	3/4" REBAR
	710929.8298	1855223.0508	W/ AL. CAP
30	B-62+54.28	10.00' RT.	3/4" REBAR
	710980.4620	1854936.1186	W/ AL. CAP
31	B-62+54.28	40.00' LT.	3/4" REBAR
	710969.5068	1854887.3335	W/ AL. CAP
32	A-64+41.49	60.00' RT.	3/4" REBAR
	710272.6920	1855120.2696	W/ AL. CAP



PROJECT CONTROL MONUMENTS - MONUMENTATION FOUND STATE PLANE COORDINATES - OH SOUTH ZONE (PROJECT GRID FACTOR = 1.000093619)			
PT.	STATION	OFFSET	TYPE
	NORTHING	EASTING	ELEVATION
50	B-17+31.66	16.54' LT.	CONC. MON. W/ IP
	706927.0156	1856917.3070	726.00
124	A-21+30.29	27.29' RT.	IP W/ AL. CAP
	707339.3325	1856851.1312	719.71
51	B-35+00.00	65.51' LT.	CONC. MON. W/ IP
	708493.6490	1856098.8513	718.22
121	A-35+64.93	43.89' RT.	IP W/ AL. CAP
	708626.6563	1856217.6680	723.08
52	B-36+12.07	4.11' RT.	CONC. MON. W/ IP
	708625.1072	1856110.3069	719.84
53	B-41+00.06	71.73' LT.	CONC. MON. W/ IP
	709026.2363	1855822.1125	717.13
120	A-44+15.31	35.36' RT.	PK W/ SHINER
	709381.4129	1855825.8032	720.14
119	A-44+43.94	43.88' RT.	IP W/ AL. CAP
	709410.8046	1855820.4675	720.63
54	B-46+75.83	101.68' LT.	CONC. MON. W/ IP
	709226.2819	1855535.2558	712.91
55	B-46+76.04	90.00' RT.	CONC. MON. W/ IP
	A-46+76.00	33.30' RT.	
	709613.0396	1855706.1730	720.53
56	B-52+39.74	39.31' LT.	CONC. MON. W/ IP
	710040.8238	1855313.6931	708.68
113	A-55+55.33	30.42' RT.	IP W/ AL. CAP.
	710409.3305	1855349.2647	703.94
57	B-59+01.53	310.56' LT.	CONC. MON. W/ IP
	710527.6922	1854740.7753	737.61
58	B-59+19.90	276.07' LT.	CONC. MON. W/ IP
	710559.5018	1854765.4141	740.96
127	A-58+73.99	64.08' RT.	IPF DSI CAP
	710724.0268	1855294.7846	705.04
72	B-66+66.07	78.71' LT.	CONC. MON. W/ IP
	711371.7711	1854778.3991	717.83
69	D-483+55.78	53.34' LT.	CONC. MON. W/ IP
	C-483+49.62	91.94' LT.	
	710757.9385	1853758.1337	725.08
68	C-483+49.56	74.67' RT.	CONC. MON. W/ IP
	710593.7134	1853783.0869	723.29
67	D-485+49.85	27.10' RT.	CONC. MON. W/ IP
	C-485+47.97	28.38' LT.	
	710722.1975	1853965.1523	730.67
70	D-486+86.91	61.85' LT.	CONC. MON. W/ IP
	710839.1453	1854079.2566	733.26
66	D-486+89.07	30.15' RT.	CONC. MON. W/ IP
	C-486+85.73	41.67' LT.	
	710749.8743	1854101.6245	733.74
65	C-488+13.76	4.34' LT.	CONC. MON. W/ IP
	710723.1084	1854232.7739	736.96
64	D-489+04.86	34.91' RT.	CONC. MON. W/ IP
	C-488+97.82	68.79' LT.	
	710792.7404	1854313.1640	738.76
71	D-489+72.30	78.69' LT.	CONC. MON. W/ IP
	710918.3977	1854353.9296	738.01
63	D-489+72.53	44.24' RT.	CONC. MON. W/ IP
	710798.5424	1854381.2200	738.94
60	C-490+43.02	91.53' RT.	CONC. MON. W/ IP
	710638.7793	1854464.7914	738.00
61	C-490+43.12	2.48' LT.	CONC. MON. W/ IP
	710732.7434	1854462.0587	741.91
59	C-490+57.95	81.89' RT.	CONC. MON. W/ IP
	710637.3671	1854479.7540	738.43
62	C-490+74.33	11.73' LT.	CONC. MON. W/ IP
	710731.4350	1854493.3281	742.66
107	D-493+92.87	40.10' LT.	IP W/ AL. CAP
	710980.0247	1854769.6813	746.99
74	D-504+04.27	9.04' RT.	CONC. MON. W/ IP
	711229.0055	1855749.5781	738.47
73	D-504+08.44	76.41' LT.	CONC. MON. W/ IP
	711310.9777	1855725.0992	735.14

THE ELEVATIONS SHOWN HEREON ARE BASED  
ON THE NORTH AMERICAN DATUM OF 1988  
(NAVD 88).

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE  
DELINIGATION OF A SURVEY MADE FOR THE OHIO  
DEPARTMENT OF TRANSPORTATION IN 2001 BY  
ms consultants, inc.

THE ESTABLISHMENT OF CENTERLINE REFERENCE  
PINS AND THE LOCATION OF EXISTING MONUMEN-  
TATION FOUND AS SHOWN ON THIS PLAN AS OF  
THIS DATE WERE PERFORMED BY ME OR UNDER  
MY DIRECT SUPERVISION.

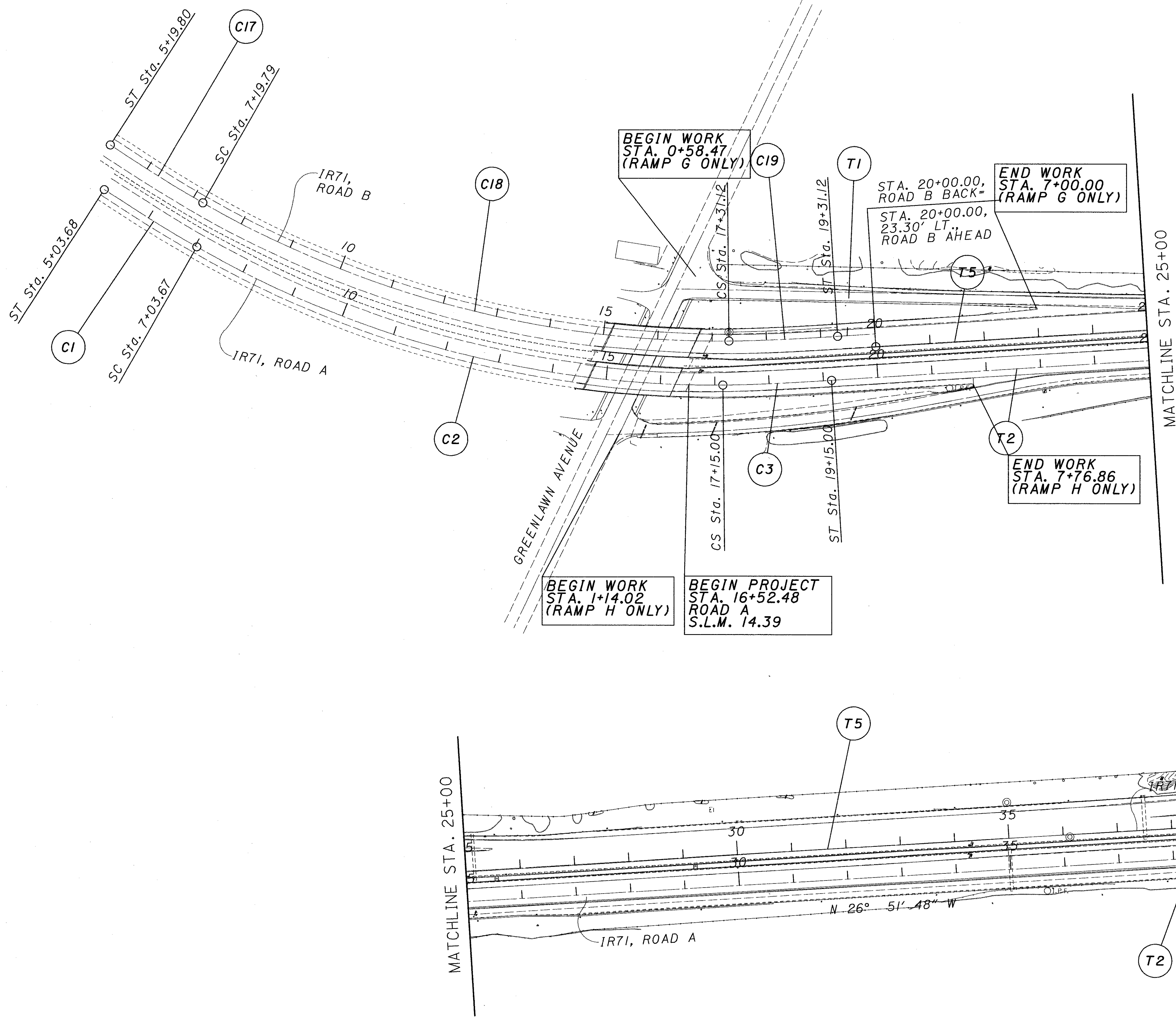
BY John L. Price SURVEYOR'S NAME JOHN L. PRICE  
SURVEYOR NO. 7159 DATE 5/16/02

RECEIVED \_\_\_\_\_, 20\_\_\_\_  
RECORDED \_\_\_\_\_, 20\_\_\_\_  
BOOK \_\_\_\_\_ PAGE \_\_\_\_\_  
COUNTY RECORDER



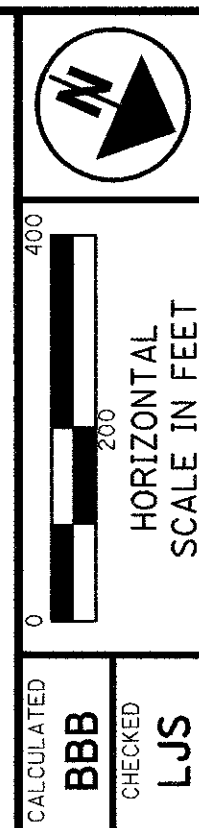
PID NO. 20681  
R/W DESIGNER JLP  
R/W REVIEWER JPV  
CENTERLINE PLAT  
FRA-171-14.39,  
FRA-315-0.00  
2  
89





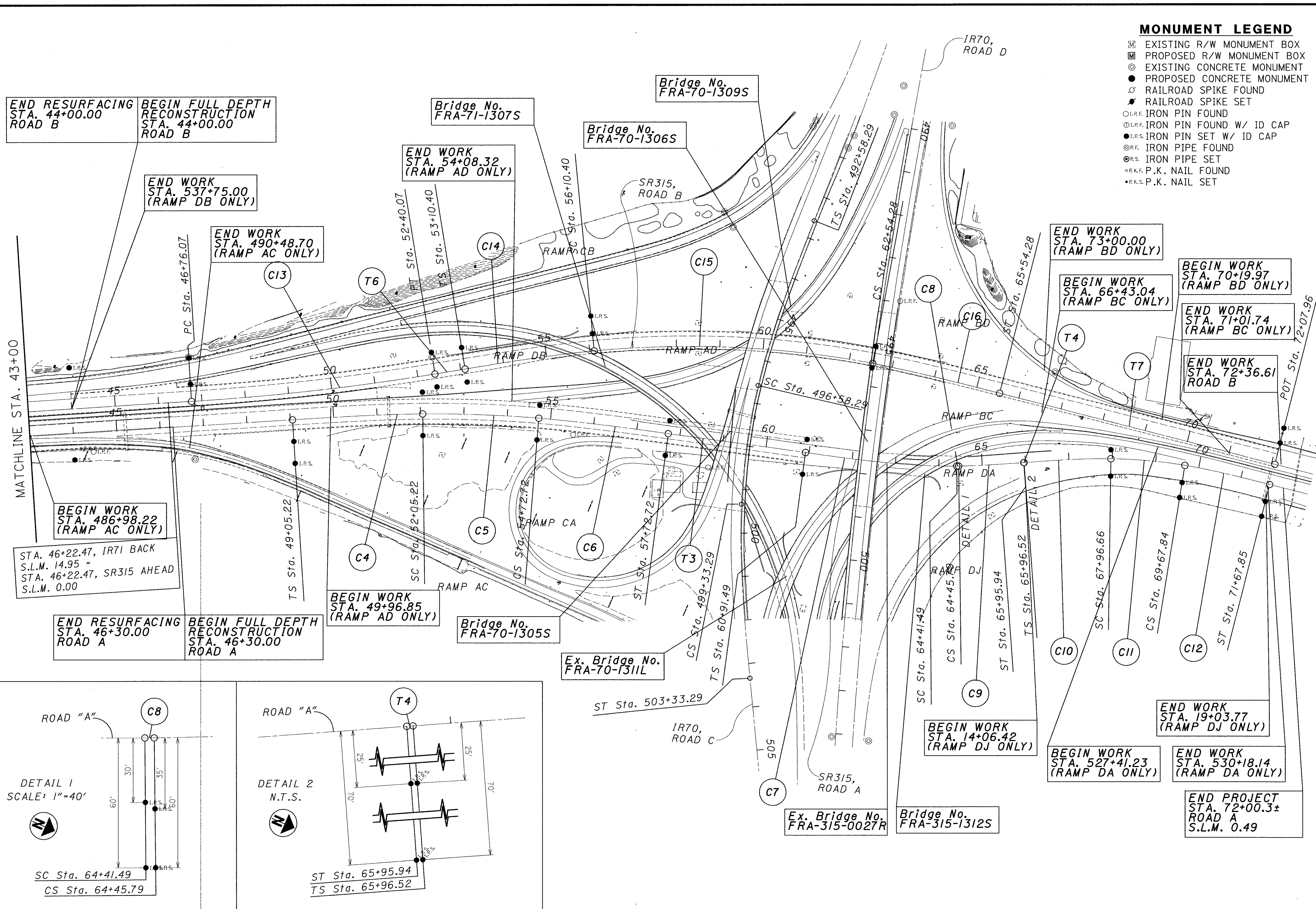
# MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- IRON PIN FOUND
- IRON PIN FOUND W/ ID CAP
- IRON PIN SET W/ ID CAP
- IRON PIPE FOUND
- IRON PIPE SET
- P.K. NAIL FOUND
- P.K. NAIL SET



## SCHEMATIC PLAN

FRA-171-14.39,  
FRA-315-0.00





ALIGNMENT DATA

CALCULATED  
BBB  
CHECKED  
LJS

SCHEMATIC PLAN

FRA-171-14.39,  
FRA-315-0.00

5  
89

C1

PI Sta 6+37.03  
fs = 2° 59' 59.46"  
Ls = 199.99'  
Rc = 1,909.86'  
Dc = 3° 00' 00.00"  
Ys = 3.49'  
Xs = 199.94'  
P = 0.87  
K = 99.99'  
LT = 133.35'  
ST = 66.68'  
TS Sta 5+03.68  
SC Sta 7+03.67

C2

P.I. Sta = 12+21.49  
D = 30° 20' 24" (LT)  
Dc = 3° 00' 00"  
R = 1,909.86'  
T = 517.82'  
L = 1,011.33'  
E = 68.95'  
eMAX = 0.068

C3

PI Sta 17+81.69  
fs = 3° 00' 00.00"  
Ls = 200.00'  
Rc = 1,909.86'  
Dc = 3° 00' 00.00"  
Ys = 3.49'  
Xs = 199.95'  
P = 0.87  
K = 99.99'  
LT = 133.35'  
ST = 66.68'  
CS Sta 17+15.00  
ST STA 19+15.00

C4

PI Sta 51+05.25  
fs = 3° 00' 00.00"  
Ls = 300.00'  
Rc = 2,864.79'  
Dc = 2° 00' 00.00"  
Ys = 5.23'  
Xs = 299.92'  
P = 1.31  
K = 149.99'  
LT = 200.03'  
ST = 100.03'  
TS Sta 49+05.22  
SC Sta 52+05.22

C5

P.I. Sta = 53+39.07  
D = 5° 21' 00" (RT)  
Dc = 2° 00' 00"  
R = 2,864.79'  
T = 133.85'  
L = 267.50'  
E = 3.13'  
eMAX = 0.045

C6

PI Sta 55+72.75  
fs = 3° 00' 00.00"  
Ls = 300.00'  
Rc = 2,864.79'  
Dc = 2° 00' 00.00"  
Ys = 5.23'  
Xs = 299.92'  
P = 1.31  
K = 149.99'  
LT = 200.03'  
ST = 100.03'  
CS Sta 54+72.72  
ST Sta 57+72.72

C7

PI Sta 63+25.08  
fs = 8° 13' 30.00"  
Ls = 350.00'  
Rc = 1,219.06'  
Dc = 4° 42' 00.00"  
Ys = 16.72'  
Xs = 349.28'  
P = 4.18  
K = 174.88'  
LT = 233.59'  
ST = 116.90'  
TS Sta 60+91.49  
SC Sta 64+41.49

C8

P.I. Sta = 64+43.64  
D = 0° 12' 07" (LT)  
\*Dc = 4° 42' 00" (NDC Dc = 4° 15' MAX.)  
R = 1,219.06'  
T = 2.15'  
L = 4.30'  
E = 0.00'  
eMAX = 0.083

C9

PI Sta 64+95.86  
fs = 3° 31' 42.69"  
\*Ls = 150.15'  
(NDC Ls = 221' MIN.)  
Rc = 1,219.06'  
Dc = 4° 42' 00.00"  
Ys = 3.08'  
Xs = 150.09'  
P = 0.77  
K = 75.07'  
LT = 100.12'  
ST = 50.07'  
CS STA 64+45.79  
ST STA 65+95.94  
\*RATE OF TRANS. = 1:185  
(NDC = 1:222)

C10

PI Sta 67+30.00  
fs = 5° 00' 12.59"  
Ls = 200.14'  
Rc = 1,145.92'  
Dc = 4° 59' 59.99"  
Ys = 5.82'  
Xs = 199.99'  
P = 1.46  
K = 100.04'  
LT = 133.48'  
ST = 66.76'  
TS Sta 65+96.67  
SC Sta 67+96.67

C11

P.I. Sta = 68+82.41  
D = 8° 33' 32" (RT)  
\*Dc = 5° 00' 00"  
(NDC Dc = 4° 15' MAX.)  
R = 1,145.92'  
T = 85.75'  
L = 171.18'  
E = 3.20'  
eMAX = 0.060

C12

PI Sta 70+34.56  
fs = 5° 00' 00.89"  
Ls = 200.01'  
Rc = 1,145.92'  
Dc = 4° 59' 59.99"  
Ys = 5.82'  
Xs = 199.86'  
P = 1.45  
K = 99.98'  
LT = 133.39'  
ST = 66.72'  
CS Sta 69+67.85  
ST Sta 71+67.85

C13

P.I. Sta = 49+58.30  
D = 5° 38' 24" (LT)  
Dc = 1° 00' 00"  
R = 5,729.58'  
T = 282.23'  
L = 564.00'  
E = 6.95'  
eMAX = 0.027

C14

PI Sta 55+10.45  
fs = 3° 45' 00.00"  
Ls = 300.00'  
Rc = 2,291.83'  
Dc = 2° 30' 00.00"  
Ys = 6.54'  
Xs = 299.87'  
P = 1.64  
K = 149.98'  
LT = 200.04'  
ST = 100.04'  
TS Sta 53+10.40  
SC Sta 56+10.40

C15

P.I. Sta = 59+34.48  
D = 16° 05' 49" (RT)  
Dc = 2° 30' 00"  
R = 2,291.83'  
T = 324.07'  
L = 643.88'  
E = 22.80'  
eMAX = 0.051

C16

PI Sta 63+54.32  
fs = 3° 45' 00.00"  
Ls = 300.00'  
Rc = 2,291.83'  
Dc = 2° 30' 00.00"  
Ys = 6.54'  
Xs = 299.87'  
P = 1.64  
K = 149.98'  
LT = 200.04'  
ST = 100.04'  
CS Sta 62+54.28  
ST Sta 65+54.28

C17

PI Sta 6+53.15  
fs = 2° 59' 59.46"  
Ls = 199.99'  
Rc = 1,909.86'  
Dc = 3° 00' 00.00"  
Ys = 3.49'  
Xs = 199.94'  
P = 0.87  
K = 99.99'  
LT = 133.35'  
ST = 66.68'  
TS Sta 5+19.80  
SC Sta 7+19.79

C18

P.I. Sta = 12+37.61  
D = 30° 20' 24" (LT)  
Dc = 3° 00' 00"  
R = 1,909.86'  
T = 517.82'  
L = 1,011.33'  
E = 68.95'  
eMAX = 0.068

C19

PI Sta 17+97.81  
fs = 3° 00' 00.00"  
Ls = 200.00'  
Rc = 1,909.86'  
Dc = 3° 00' 00.00"  
Ys = 3.49'  
Xs = 199.95'  
P = 0.87  
K = 99.99'  
LT = 133.35'  
ST = 66.68'  
CS Sta 17+31.12  
ST Sta 19+31.12

T1

STA. = 19+31.12  
STA. = 20+00.00  
L = 68.88'  
BRG. = N 26° 51' 48" W

T2

STA. = 19+15.00  
STA. = 49+05.22  
L = 2,990.22'  
BRG. = N 26° 51' 48" W

T3

STA. = 57+72.72  
STA. = 60+91.49  
L = 318.77'  
BRG. = N 15° 30' 18" W

T4

STA. = 65+95.94  
STA. = 65+96.67  
L = 0.73'  
BRG. = N 27° 27' 55" W

T5

STA. = 20+00.00  
STA. = 46+76.07  
L = 2,676.07'  
BRG. = N 26° 51' 48" W

T6

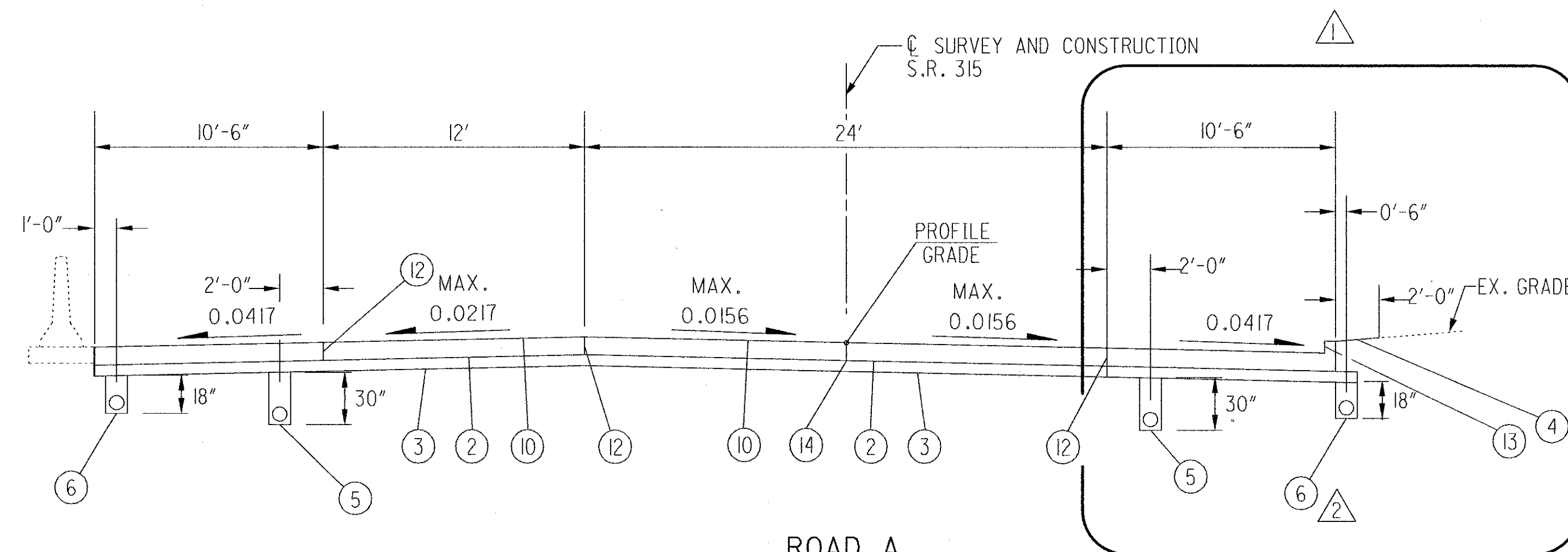
STA. = 52+40.07  
STA. = 53+10.40  
L = 70.33'  
BRG. = N 32° 30' 12" W

T7

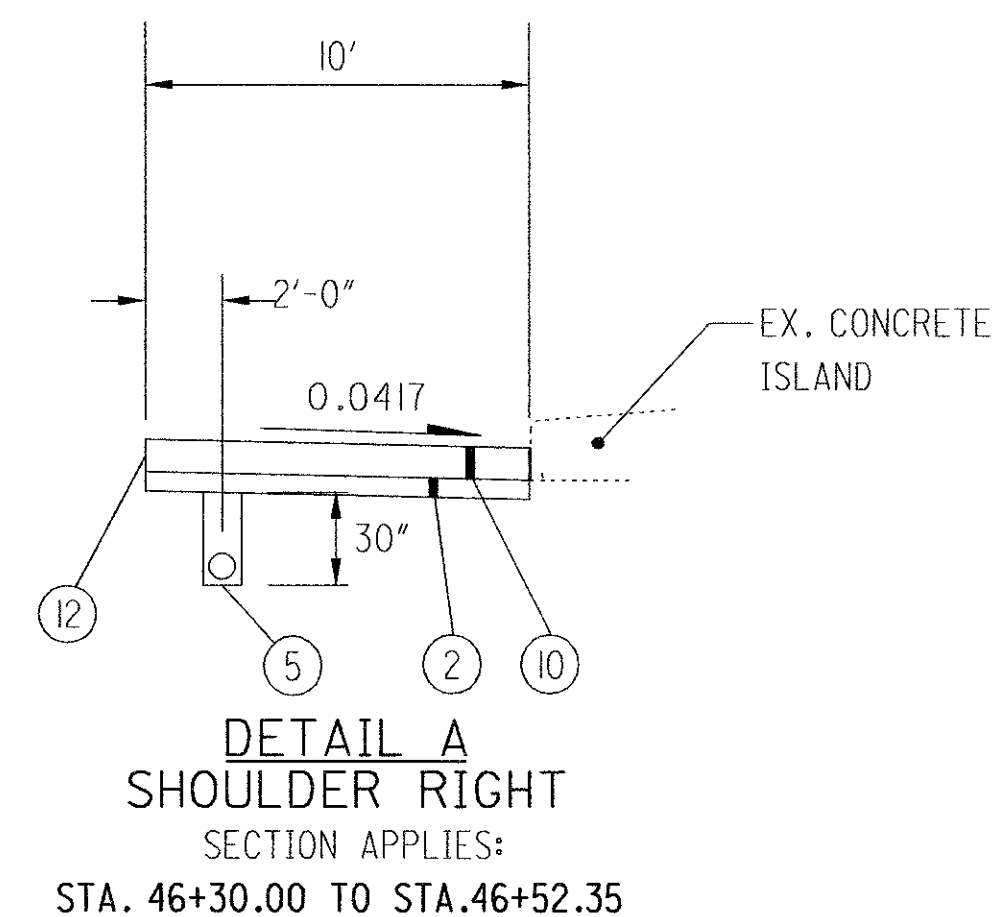
STA. = 65+54.28  
STA. = 72+07.96  
L = 653.68'  
BRG. = N 08° 54' 23" W

\* DESIGN EXCEPTION

DATE: 05/15/2002 08:28:08 PM  
FILENAME: T:\ROAD\IN\050526\306\typicalroad.dgn



ROAD A  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 46+30.00 TO STA.47+00.00 = 70.00 LIN. FT.



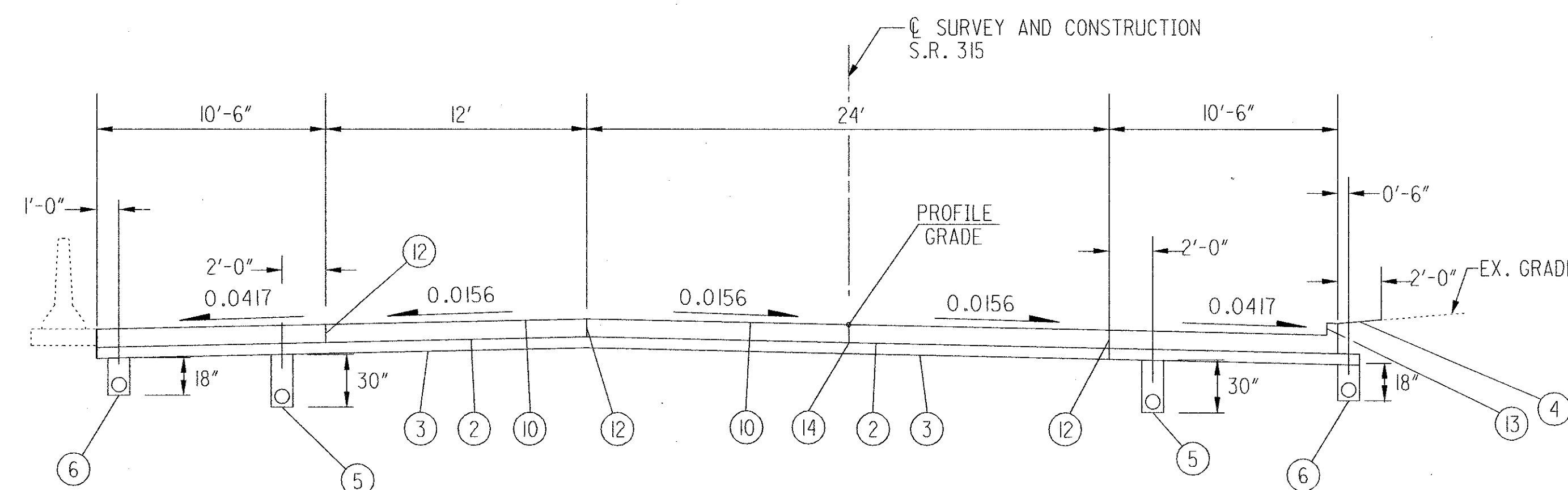
DETAIL A  
SHOULDER RIGHT  
SECTION APPLIES:  
STA. 46+30.00 TO STA.46+52.35

NOTE: SEE PAVEMENT DETAILS ON SHEET 47

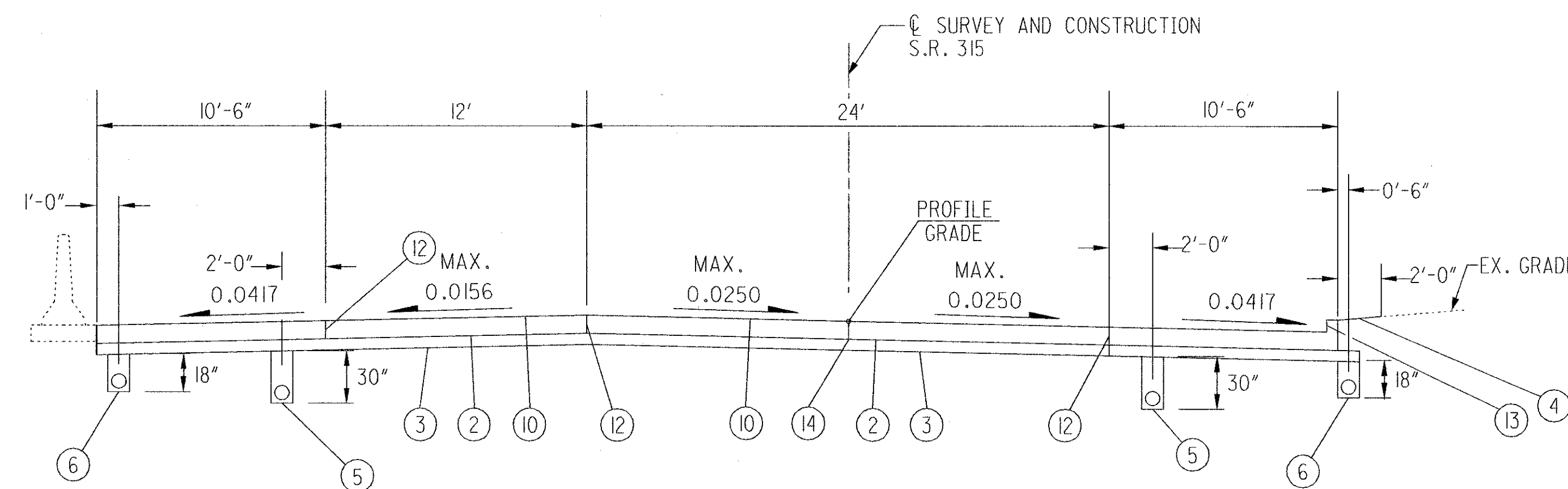
ROAD A

1 SEE DETAIL A

2 BEGIN PROPOSED UNDERDRAIN, STA. 46+53.00, 23' RT.



ROAD A  
NORMAL SECTION  
SECTION APPLIES:  
STA. 47+00.00 TO STA.48+75.00 = 175.00 LIN. FT.



ROAD A  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 48+75.00 TO STA.50+00.00 = 125.00 LIN. FT.

LEGEND

- 1 -
- 2 ITEM 304 6" AGGREGATE BASE
- 3 ITEM 203 SUBGRADE COMPACTION
- 4 ITEM 659 SEEDING AND MULCHING
- 5 ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- 6 ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- 7 ITEM 606 GUARDRAIL, TYPE 5
- 8 -
- 9 -
- 10 ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- 11 ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- 12 STANDARD LONGITUDINAL JOINT
- 13 INTEGRAL CURB TYPE 2-A
- 14 UNTIED LONGITUDINAL JOINT

PROPOSED TYPICAL SECTIONS  
SR 315 - ROAD A

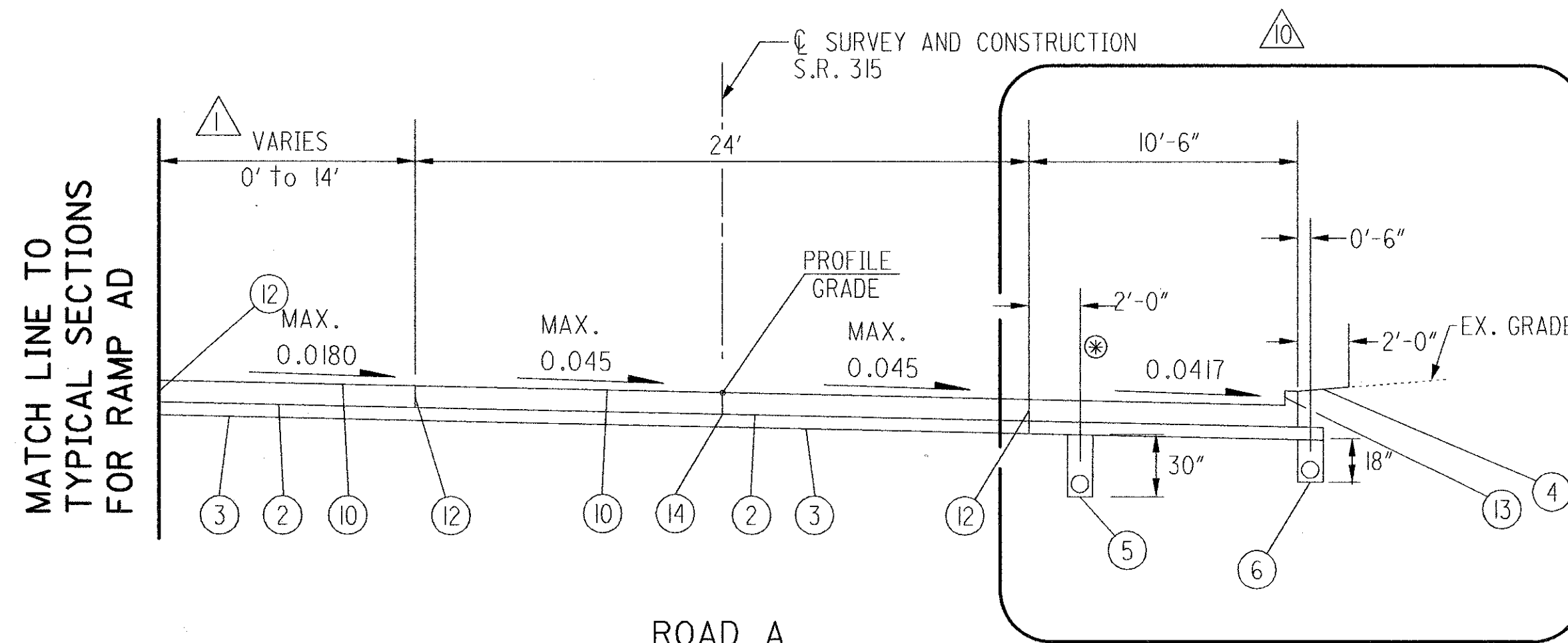
FRA-IR71-14.39,  
FRA-315-0.00

6  
89

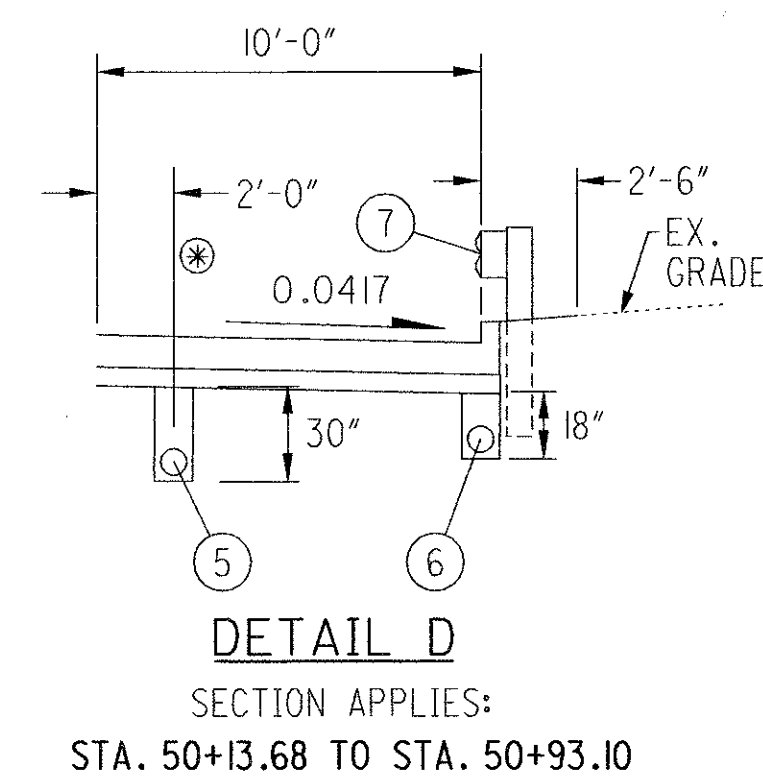
CALCULATED  
BBB  
CHECKED  
LJS



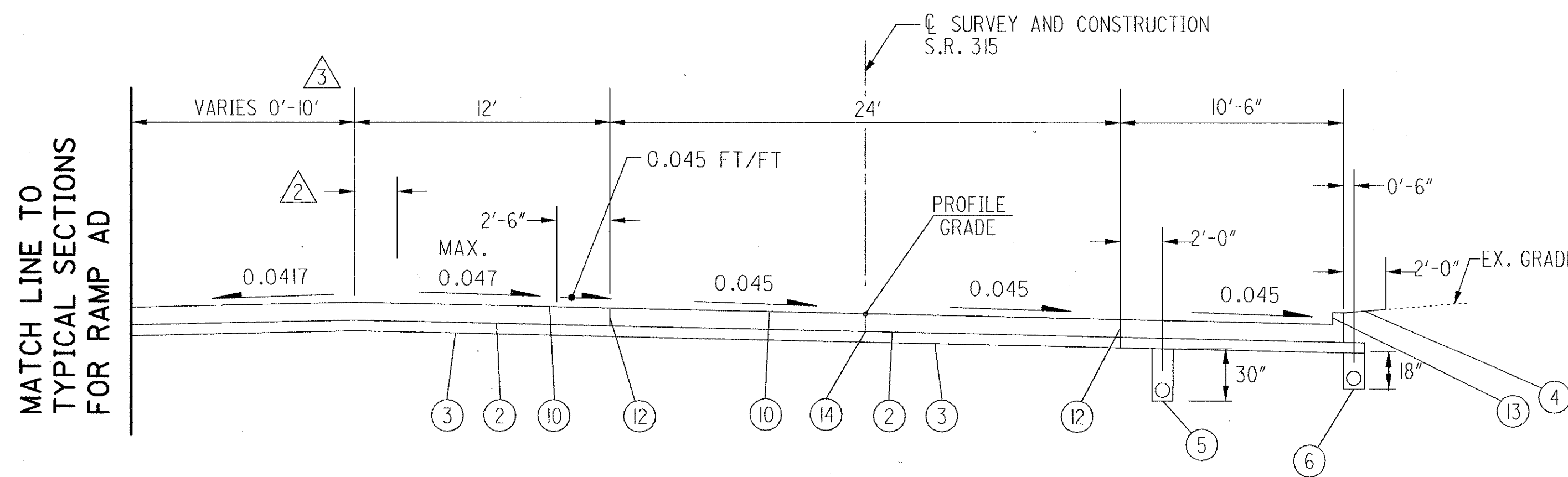
NOTE: SEE PAVEMENT DETAILS ON SHEETS 47, 48, & 49  
⊗ 0.0417 OR RATE OF PAVEMENT SUPERELEVATION, WHICHEVER IS GREATER



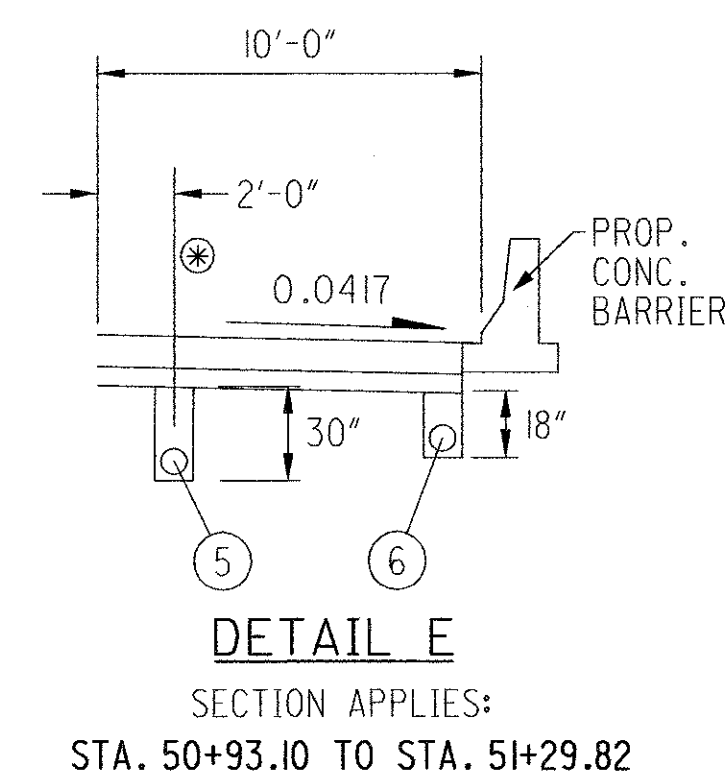
ROAD A  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 50+00.00 TO STA. 53+18.40 = 318.40 LIN. FT.



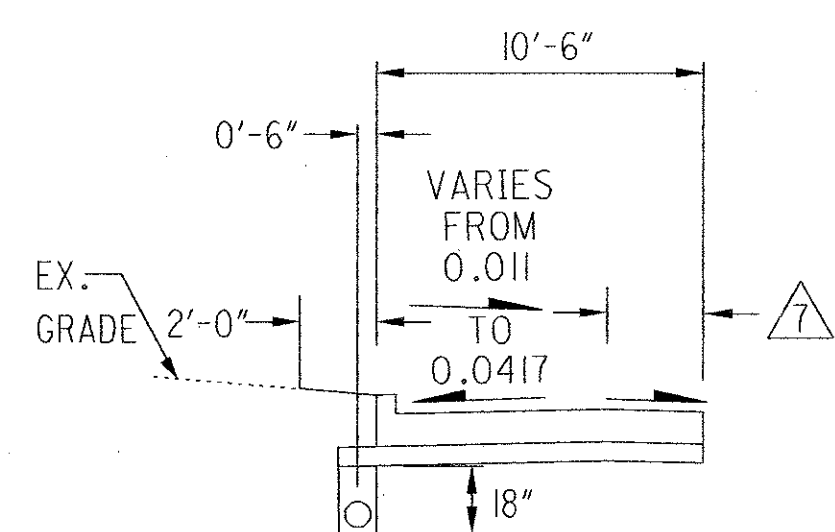
DETAIL D  
SECTION APPLIES:  
STA. 50+13.68 TO STA. 50+93.10



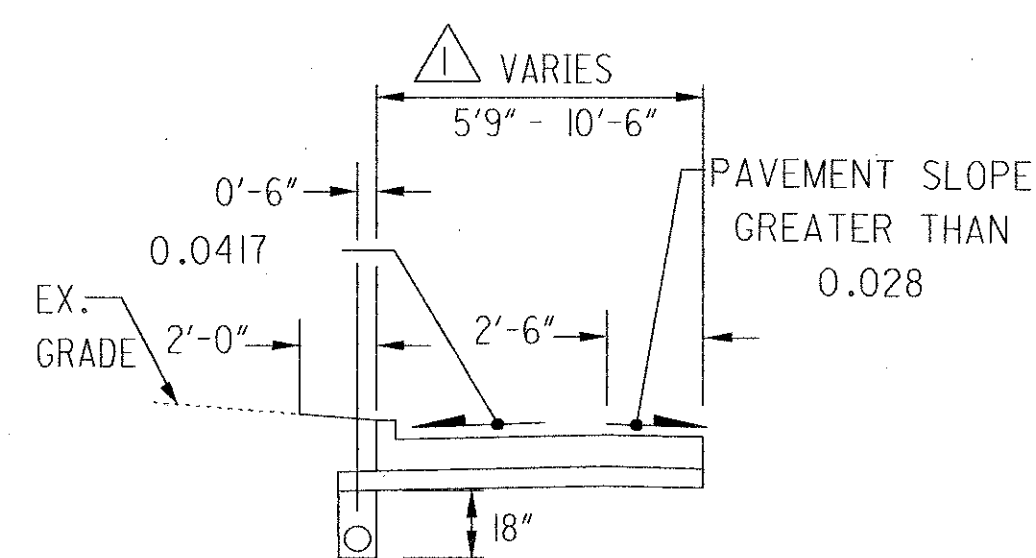
ROAD A  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 53+18.40 TO STA. 54+08.81 = 90.41 LIN. FT.



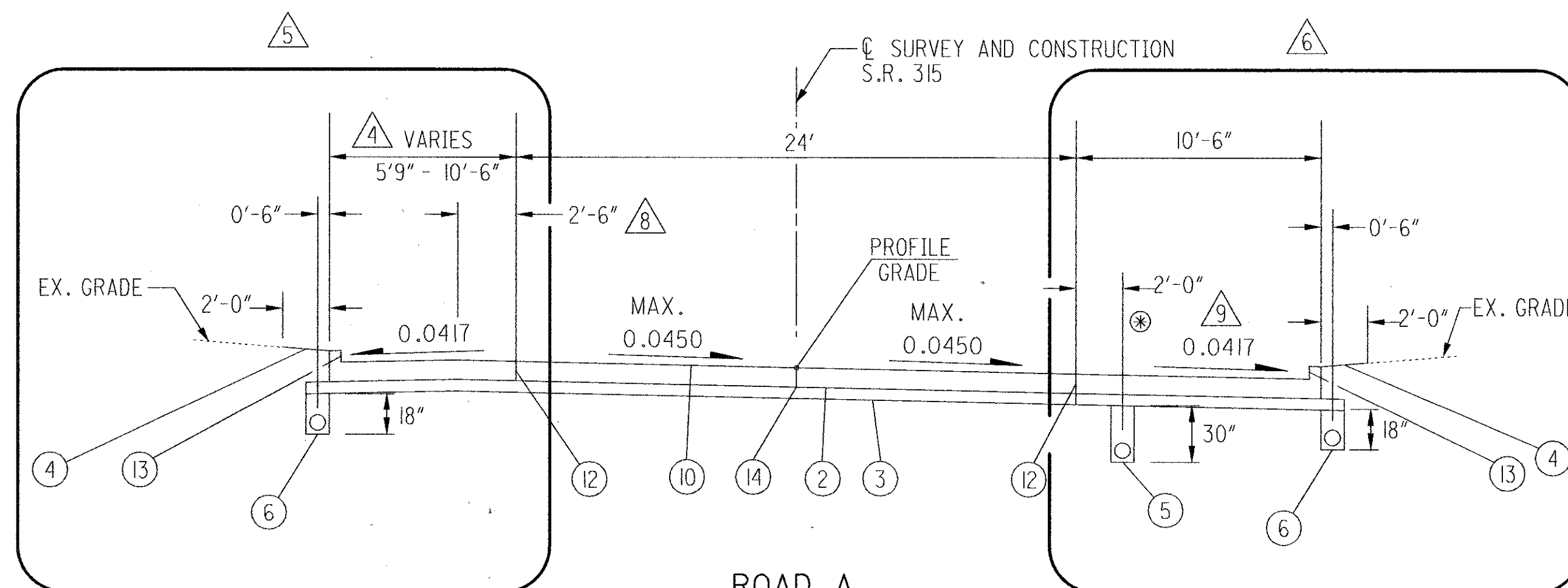
DETAIL E  
SECTION APPLIES:  
STA. 50+93.10 TO STA. 51+29.82



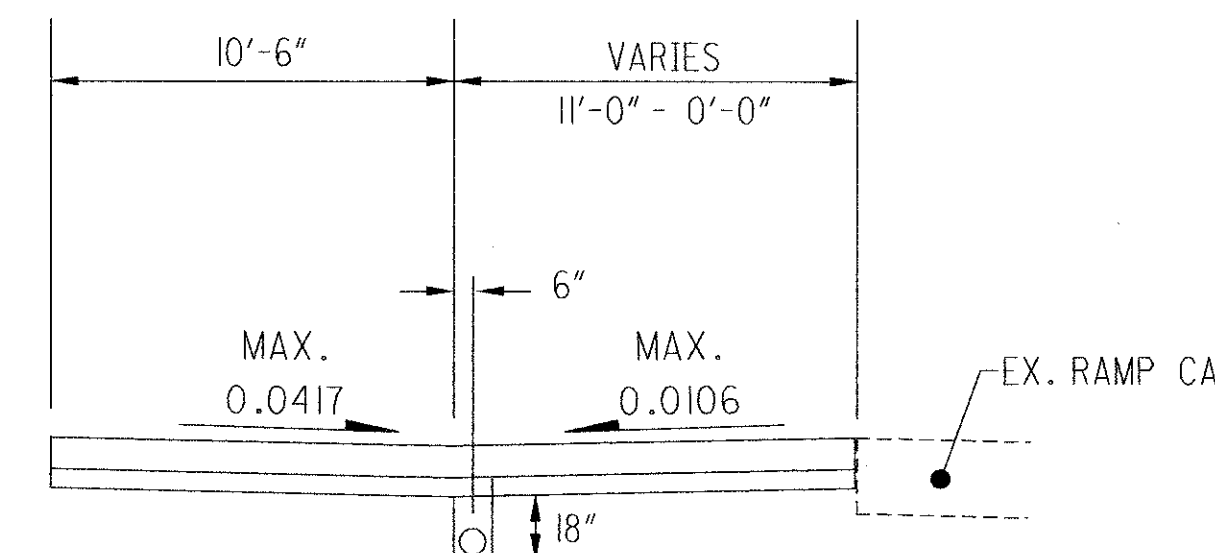
DETAIL C  
SECTION APPLIES:  
STA. 54+08.81 TO STA. 55+08.76



DETAIL A  
SECTION APPLIES:  
STA. 55+85.53 TO STA. 56+68.64



ROAD A  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 54+08.81 TO STA. 56+68.64 = 259.83 LIN. FT.



DETAIL B  
SECTION APPLIES:  
STA. 55+49.64 TO STA. 56+68.01

## ROAD A

- 1 TAPER BEGINS STA. 51+82.46
- 2 EDGE OF PAVEMENT, RAMP AD VARIES 2'-0" TO 0', STA. 53+18.40 TO STA. 53+26.35
- 3 BEGINNING STA. 53+36.13
- 4 CONSTANT WIDTH 10'-6" STA. 54+08.81 TO STA. 55+08.76; CONSTANT WIDTH 5'-9" STA. 56+58.80 TO STA. 56+68.64
- 5 SEE DETAIL "A" AND DETAIL "C"
- 6 SEE DETAIL "B"
- 7 WHEN THE GRADE BREAK EXCEEDS 7%, OFFSET 2'-6" SHOULDER CROWN TRANSITION.
- 8 STA. 55+75.00 TO STA. 56+00.00 = 2'-6" to 0'-0"
- 9 STA. 55+72.72 TO STA. 55+00.00, TRANSITION SHOULDER SLOPE BACK TO 0.0417 FT/FT
- 10 SEE DETAIL "D" AND DETAIL "E"

## LEGEND

- 1 -
- 2 ITEM 304 6" AGGREGATE BASE
- 3 ITEM 203 SUBGRADE COMPACTION
- 4 ITEM 659 SEEDING AND MULCHING
- 5 ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- 6 ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- 7 ITEM 606 GUARDRAIL, TYPE 5
- 8 -
- 9 -
- 10 ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- 11 ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- 12 STANDARD LONGITUDINAL JOINT
- 13 INTEGRAL CURB TYPE 2-A
- 14 UNTIED LONGITUDINAL JOINT

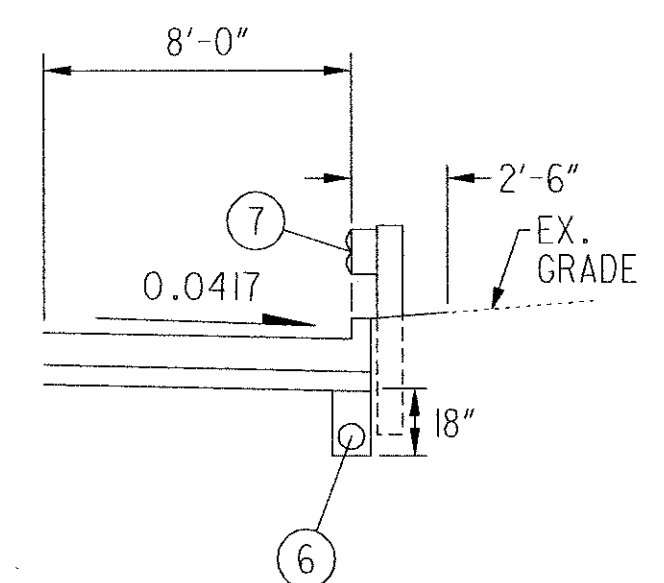
# PROPOSED TYPICAL SECTIONS SR 315 - ROAD A

FRA-IR71-14.39,  
FRA-315-0.00

NOTE: SEE PAVEMENT DETAILS ON SHEETS 49 & 50

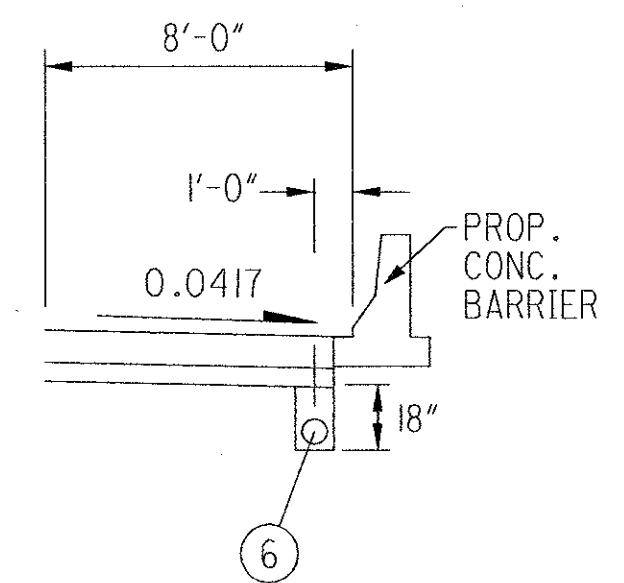
ROAD A

- △ SEE DETAIL "A"
- △ SEE DETAIL "B"
- △ SEE DETAIL "C"
- △ SEE DETAIL "D"



DETAIL C

SECTION APPLIES:  
STA. 57+69.82 TO STA. 58+48.73

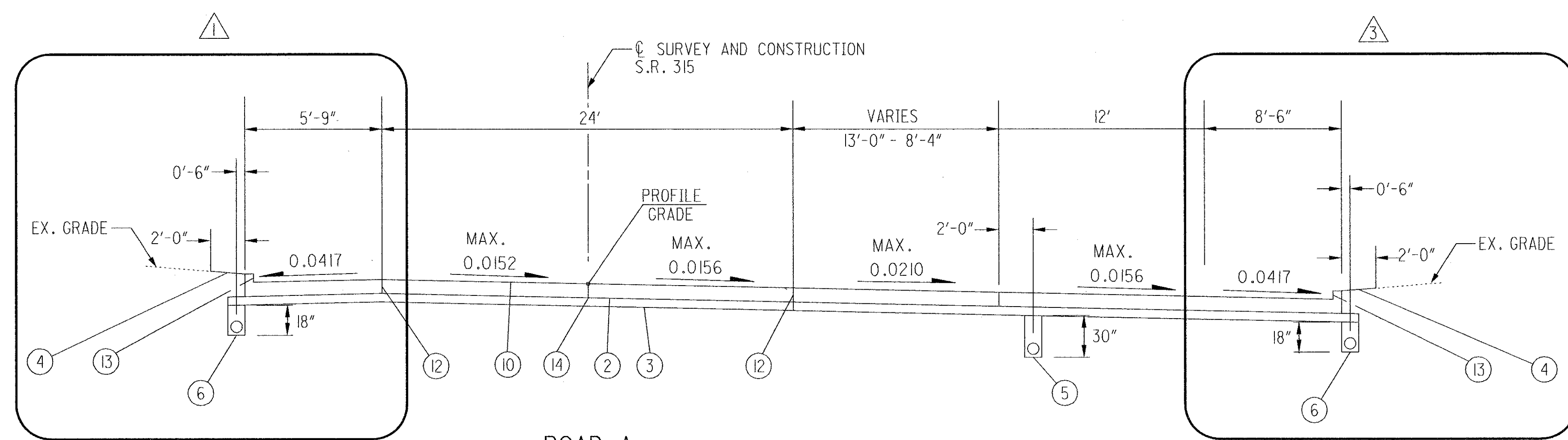


DETAIL D

SECTION APPLIES:  
STA. 58+48.73 TO STA. 59+36.01

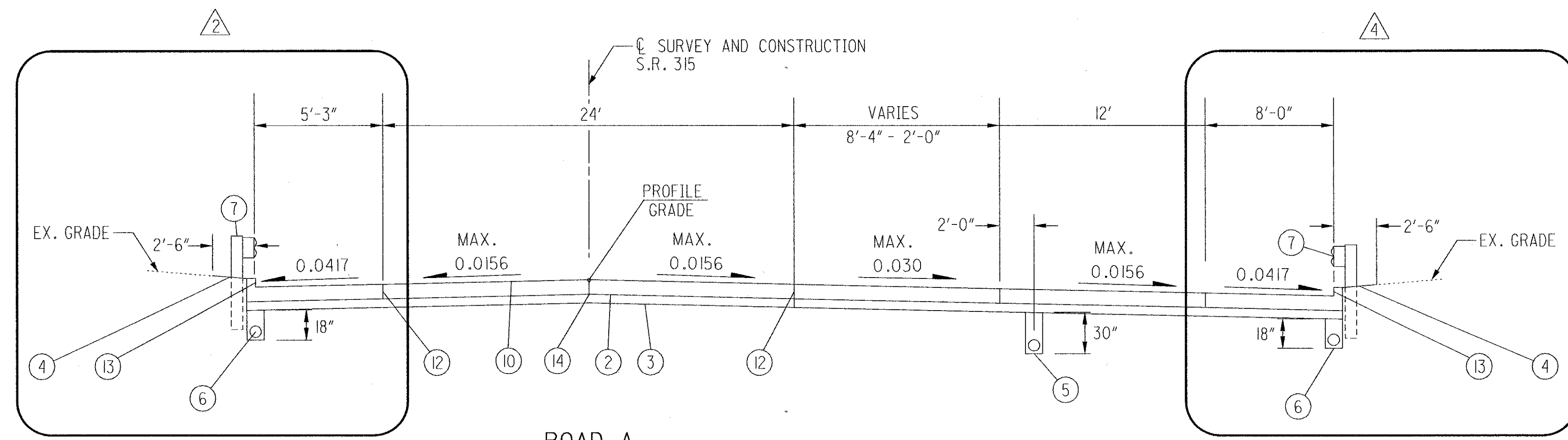
LEGEND

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ -
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT



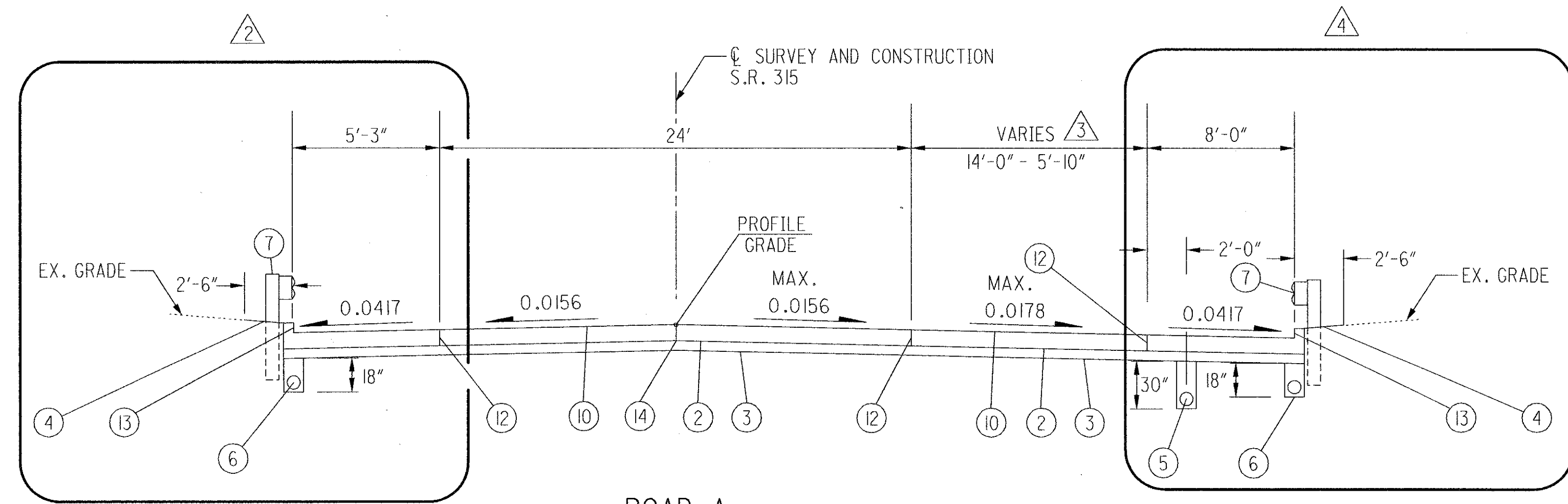
ROAD A  
SUPERELEVATED SECTION

SECTION APPLIES:  
STA. 56+68.64 TO STA. 57+75.00 = 106.36 LIN. FT.



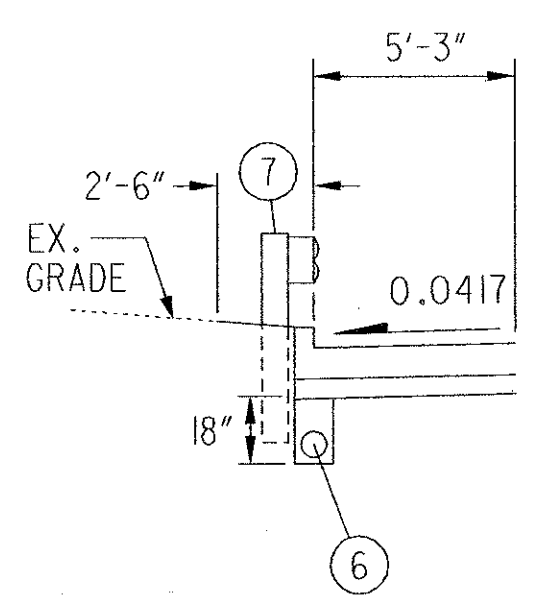
ROAD A  
SUPERELEVATED SECTION

SECTION APPLIES:  
STA. 57+75.00 TO STA. 59+14.50 = 139.50 LIN. FT.



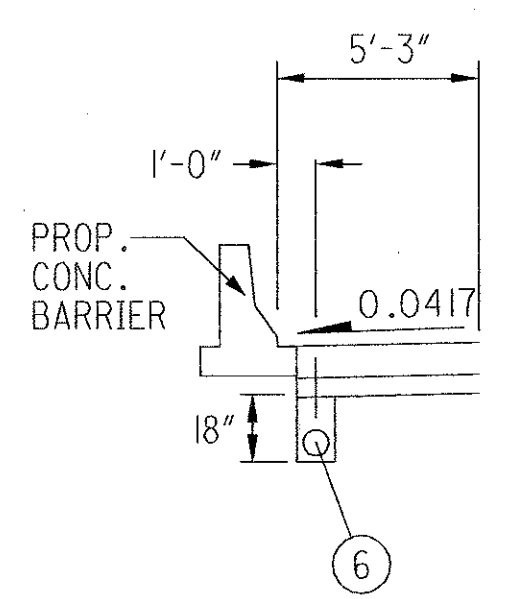
ROAD A  
SUPERELEVATED SECTION

SECTION APPLIES:  
STA. 59+14.50 TO STA. 60+92.80 = 178.30 LIN. FT.



DETAIL A

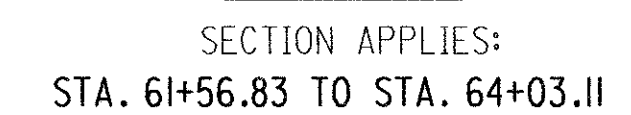
SECTION APPLIES:  
STA. 57+09.54 TO STA. 57+88.51

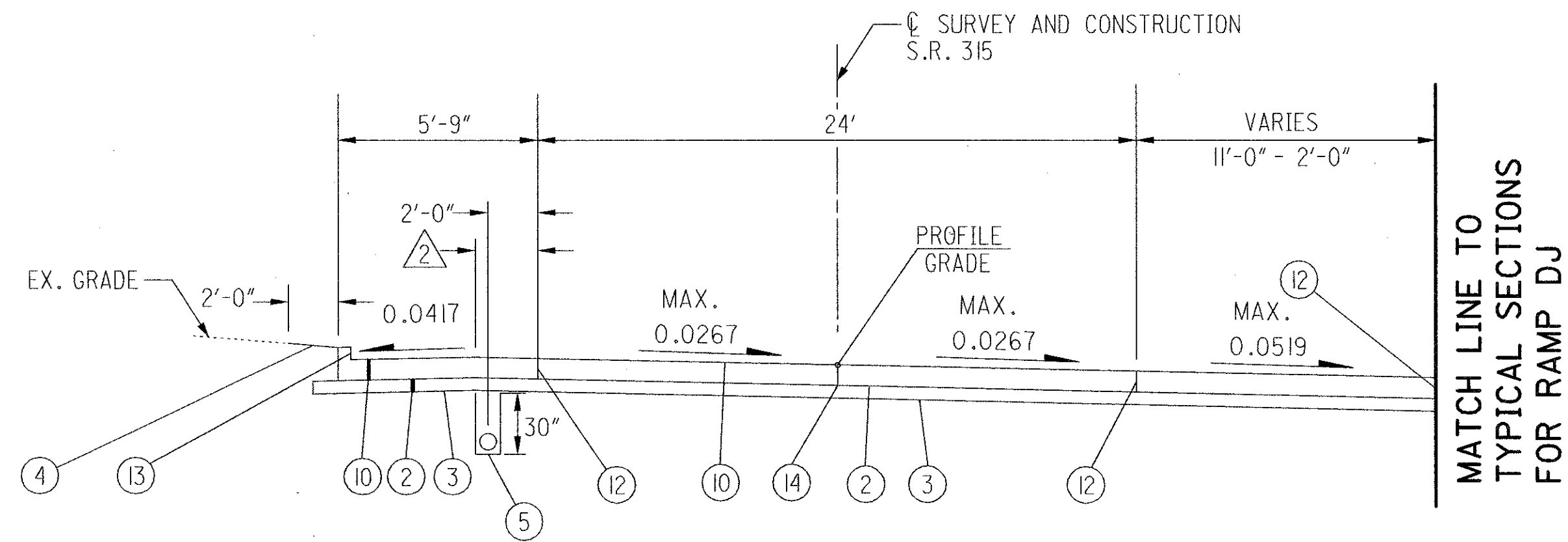


DETAIL B

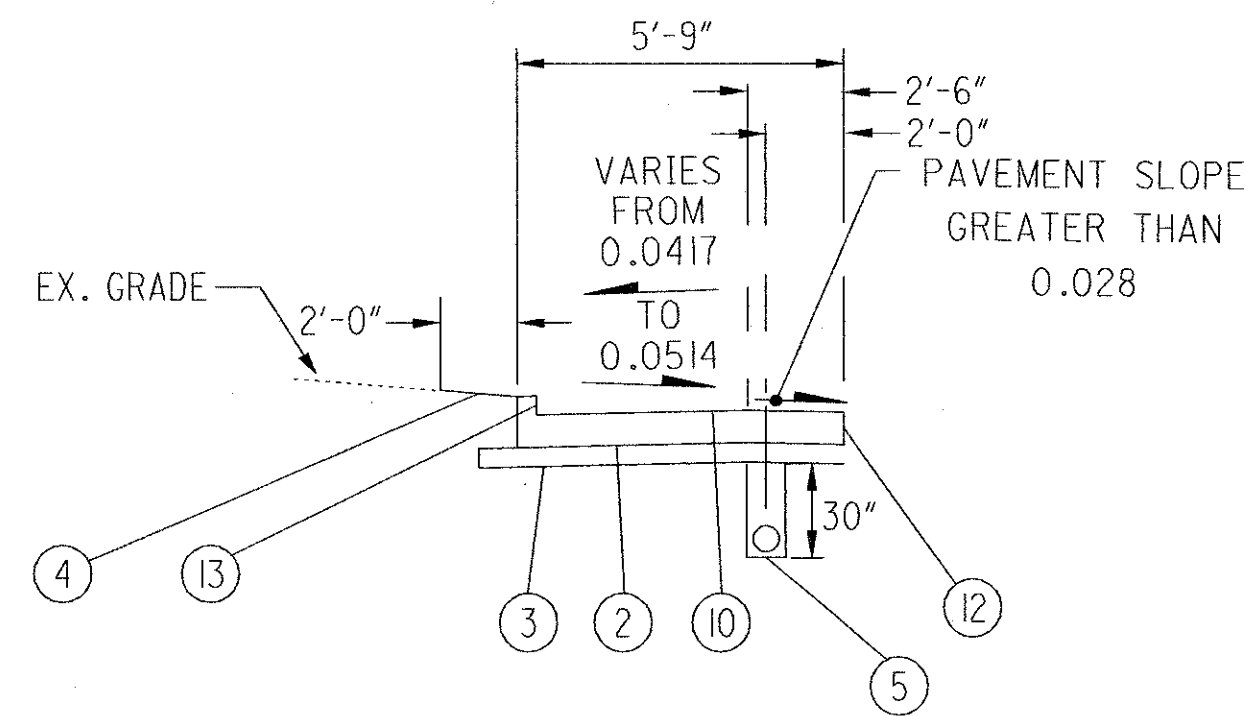
SECTION APPLIES:  
STA. 57+88.51 TO STA. 59+81.19



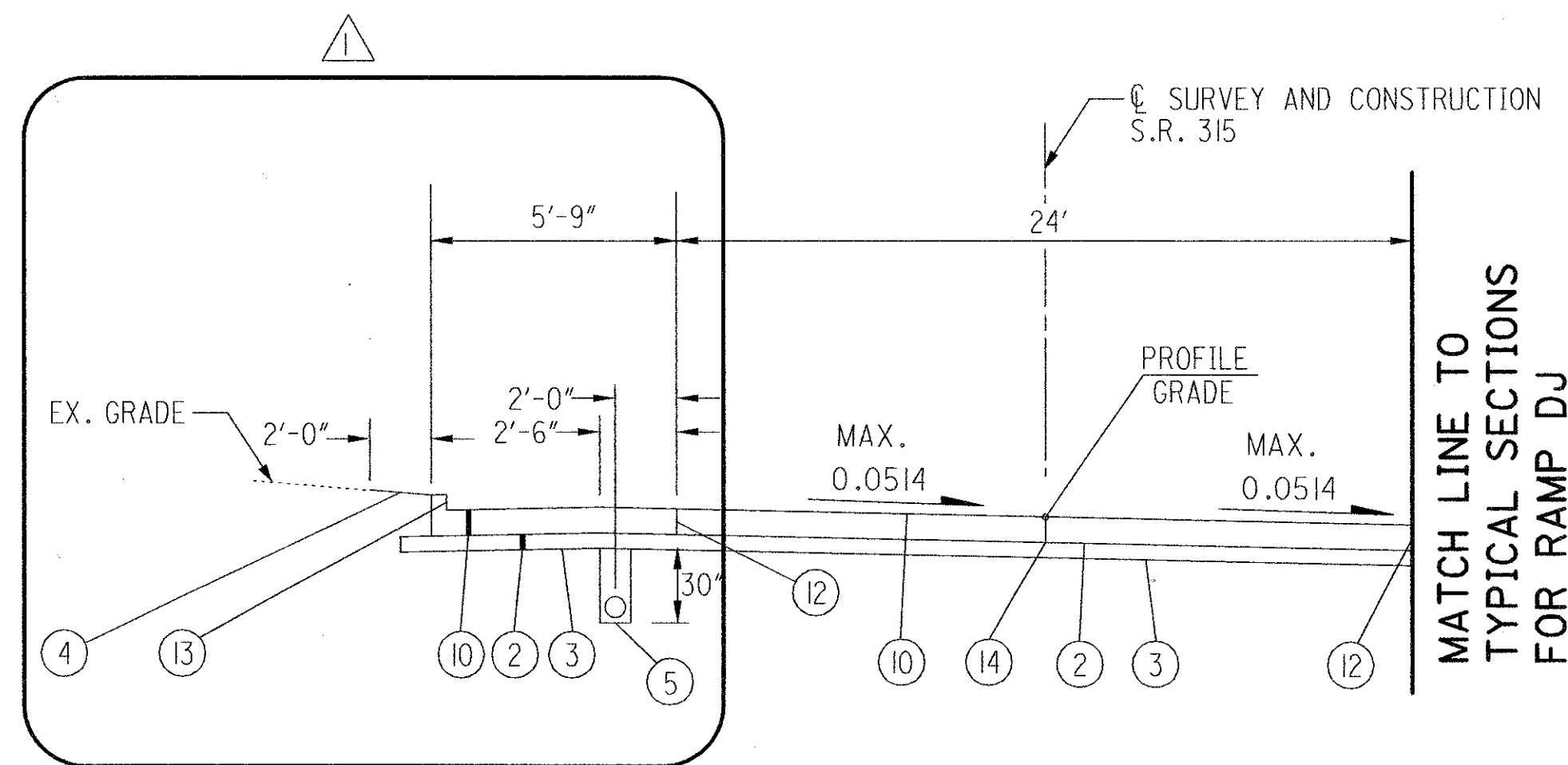




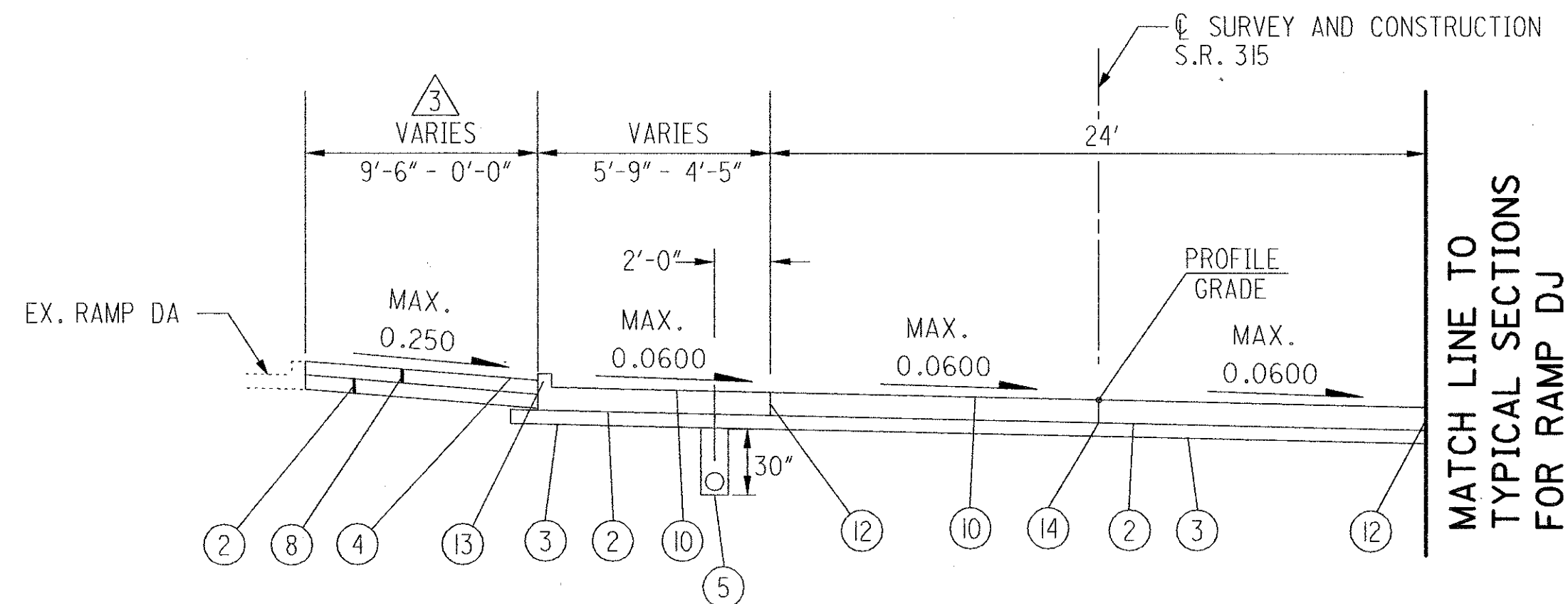
**ROAD A  
 SUPERELEVATED SECTION**  
 SECTION APPLIES:  
 STA. 66+64.13 TO STA.67+29.30 = 65.17 LIN. FT.



**DETAIL A**  
 SECTION APPLIES:  
 STA. 66+63.14 TO STA. 66+64.13



**ROAD A  
 SUPERELEVATED SECTION**  
 SECTION APPLIES:  
 STA. 67+29.30 TO STA.67+92.00 = 62.7 LIN. FT.



**ROAD A  
 SUPERELEVATED SECTION**  
 SECTION APPLIES:  
 STA. 67+92.00 TO STA.68+92.88 = 100.88 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEET 51

**ROAD A**

- ① SEE DETAIL "A"
- ② CROWN SHOULDER TRANSITION  
 STA. 67+00.00 TO 67+25.00 = 0'-0" to 2'-6"  
 STA. 67+25.00 TO 67+29.30 = 2'-6"
- ③ STA. 67+92.00 TO 68+74.14 = 5'-9"  
 STA. 68+74.14 TO 68+92.88 = 5'-9" to 4'-5"

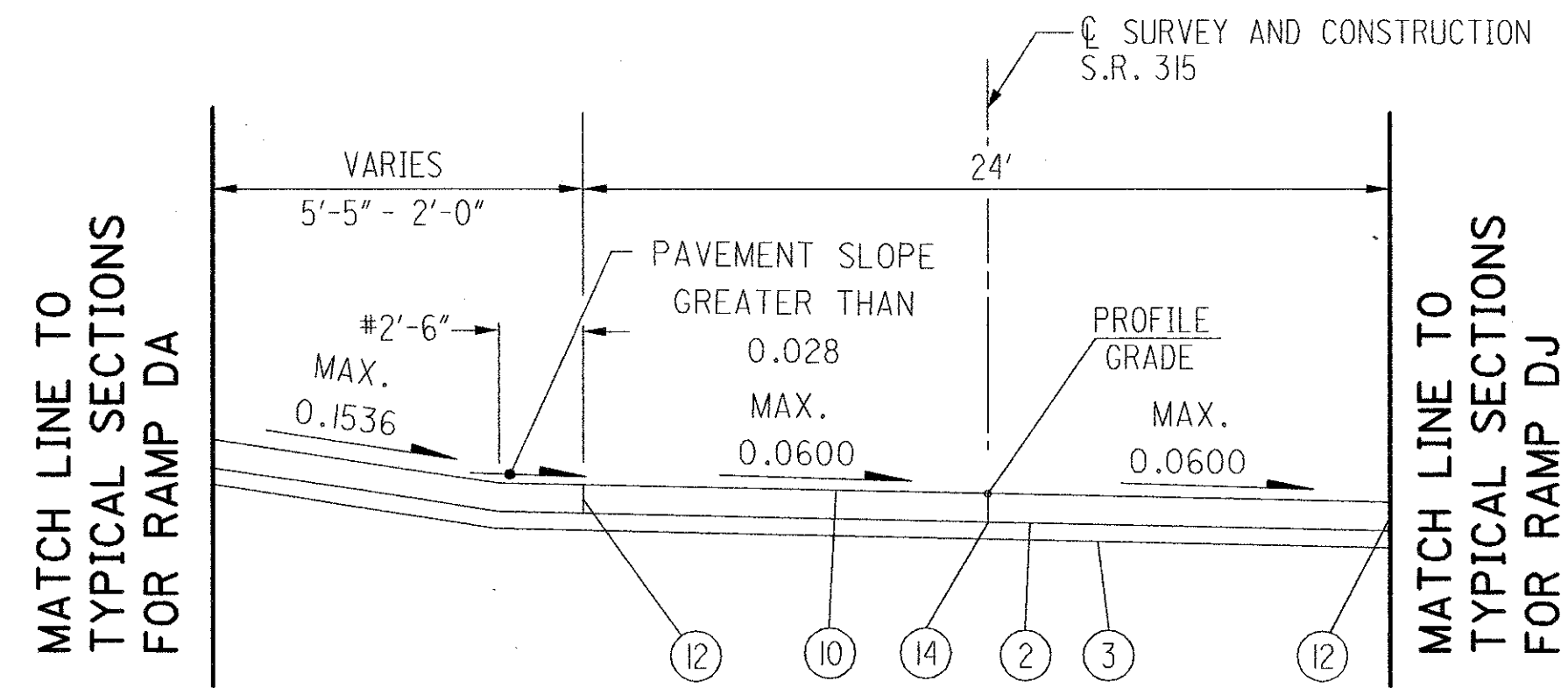
**LEGEND**

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ ITEM 601 6" CONCRETE TRAFFIC ISLAND
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

**PROPOSED TYPICAL SECTIONS  
 SR 315 - ROAD A**

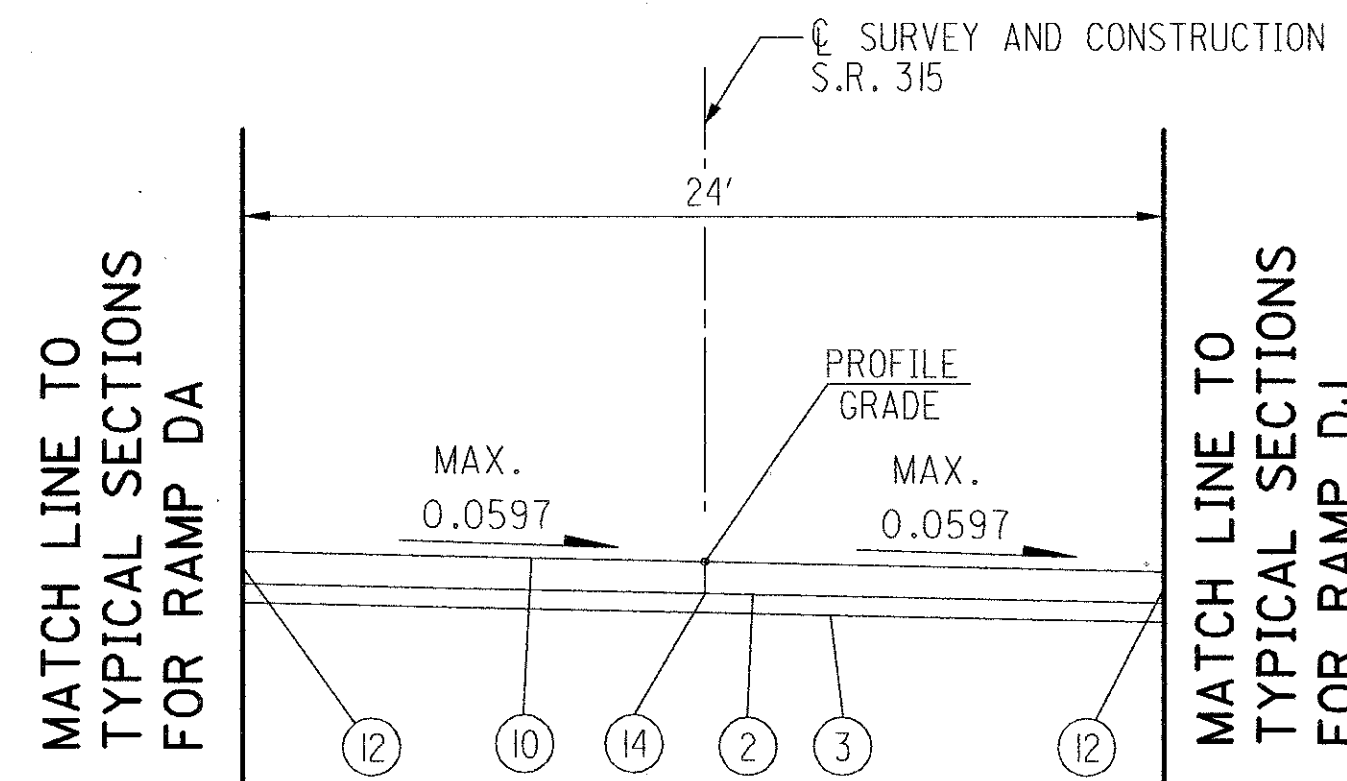
**FRA-IR71-14.39,  
 FRA-315-0.00**



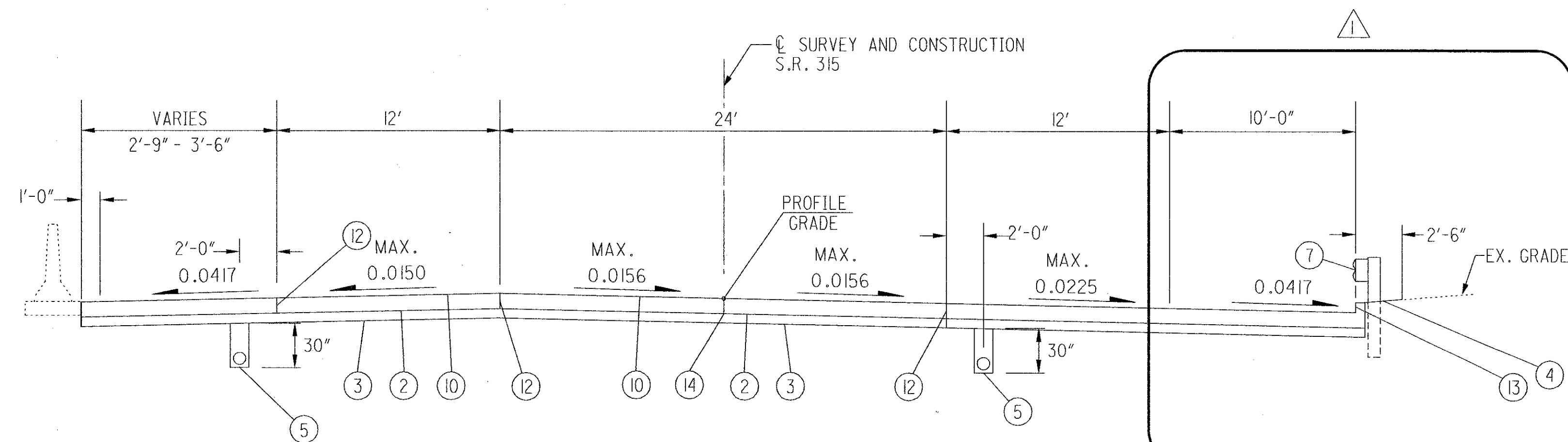


\* 2'-6" TAPERS TO  
 WIDTH OF 2'-0"  
 FOR SECTION WIDTHS  
 LESS THAN 5'-0"

ROAD A  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 68+92.88 TO STA. 69+29.71 = 36.83 LIN. FT.



ROAD A  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 69+29.71 TO STA. 71+67.85 = 238.14 LIN. FT.

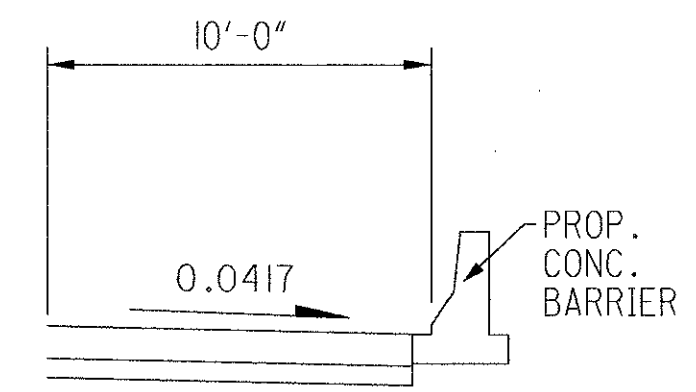


ROAD A  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 71+67.85 TO STA. 72+00.12 = 32.27 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEETS 51 & 52

ROAD A

SEE DETAIL "A"



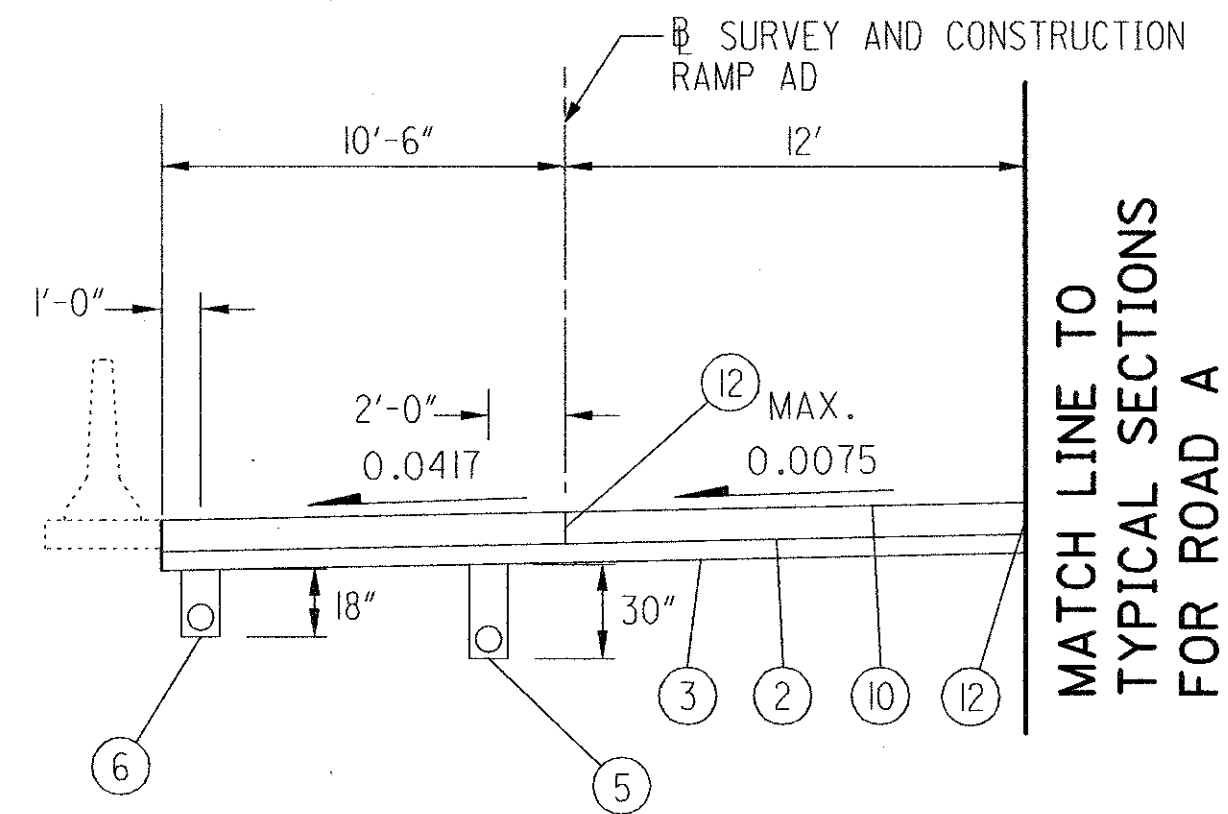
DETAIL A  
 SECTION APPLIES:  
 STA. 71+91.12 TO STA. 72+14.24

# LEGEND

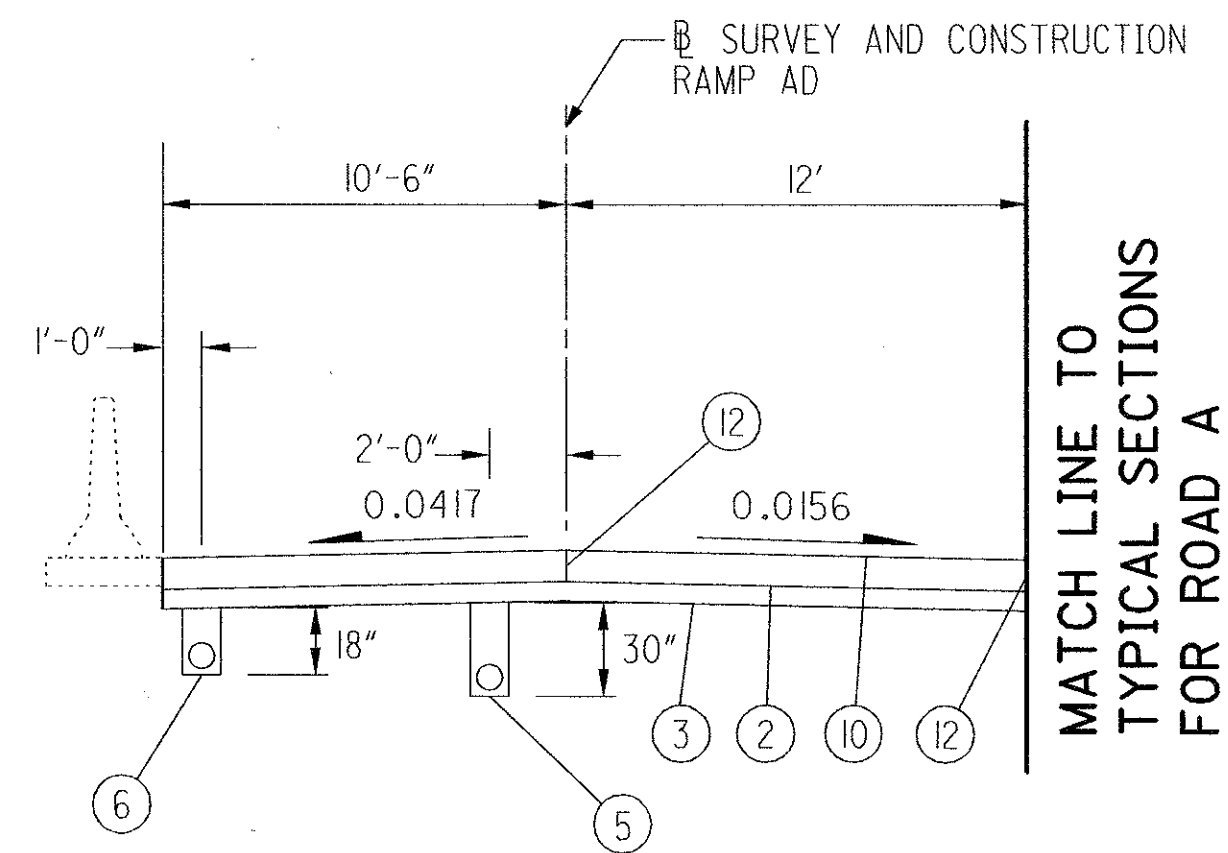
- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ ITEM 601 6" CONCRETE TRAFFIC ISLAND
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

PROPOSED TYPICAL SECTIONS  
 SR 315 - ROAD A

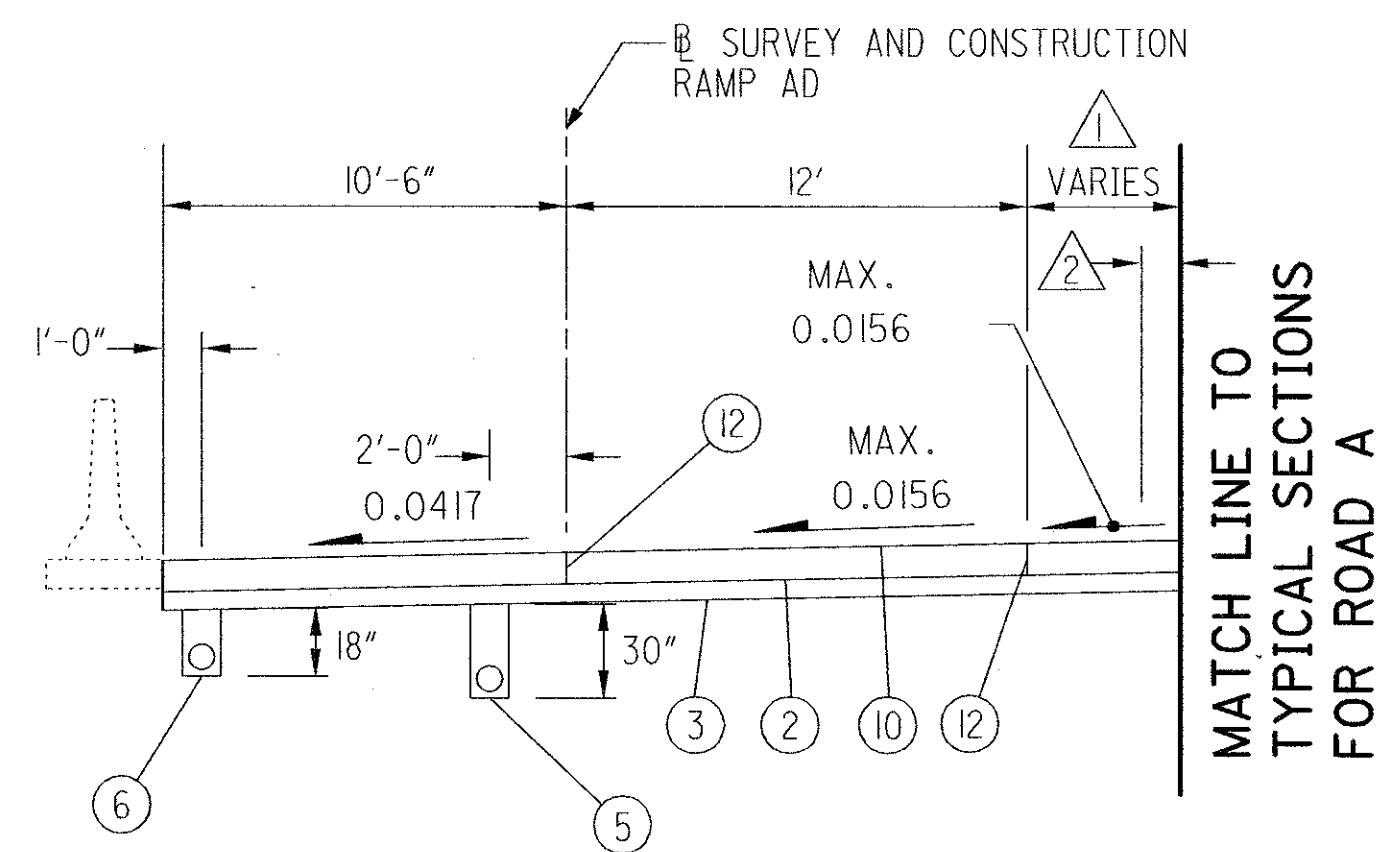
FRA-IR71-14.39,  
 FRA-315-0.00



RAMP AD  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 49+96.85 TO STA. 50+66.67 = 69.82 LIN. FT.



RAMP AD  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 50+66.67 TO STA. 53+04.88 = 238.21 LIN. FT.



RAMP AD  
 SUPERELEVATED SECTION  
 SECTION APPLIES:  
 STA. 53+04.88 TO STA. 54+08.32 = 103.44 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEETS 47 & 48

RAMP AD

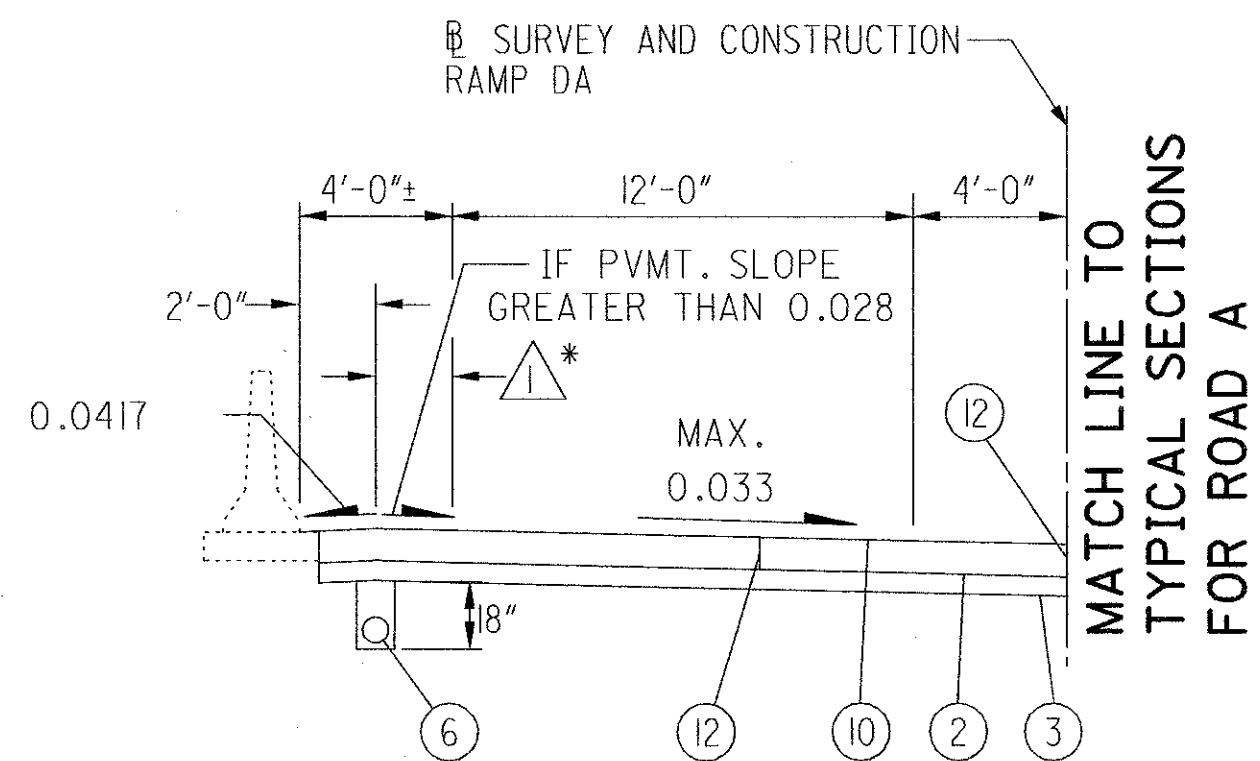
△ STA. 53+04.08 TO STA. 53+34.45 = 2'-0" to 4'-0"  
 STA. 53+34.45 TO STA. 54+08.32 = 4'-0"

△ EDGE OF CONSTRUCTION, ROAD A  
 VARIES 2'-0" TO 0'-0", STA. 53+16.44  
 TO STA. 53+34.45

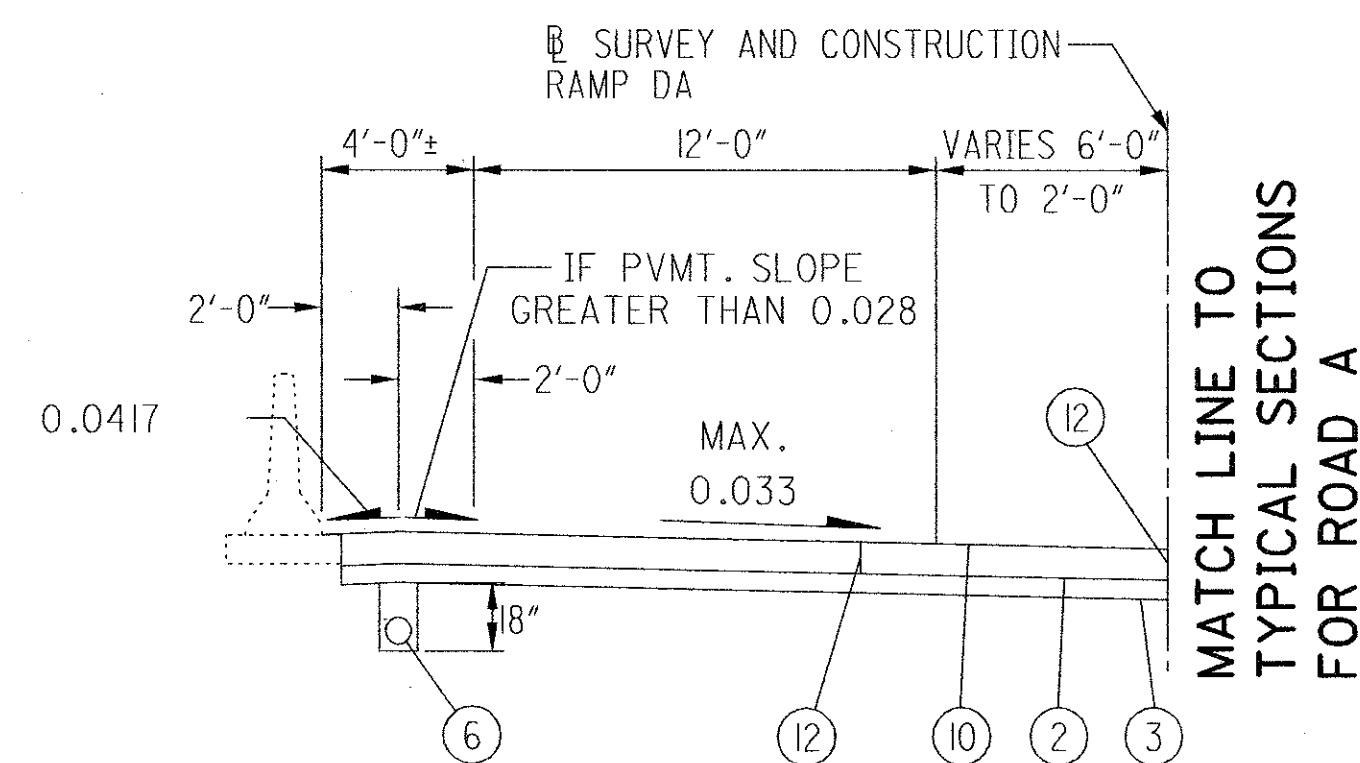
LEGEND

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
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- ⑨ -
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- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

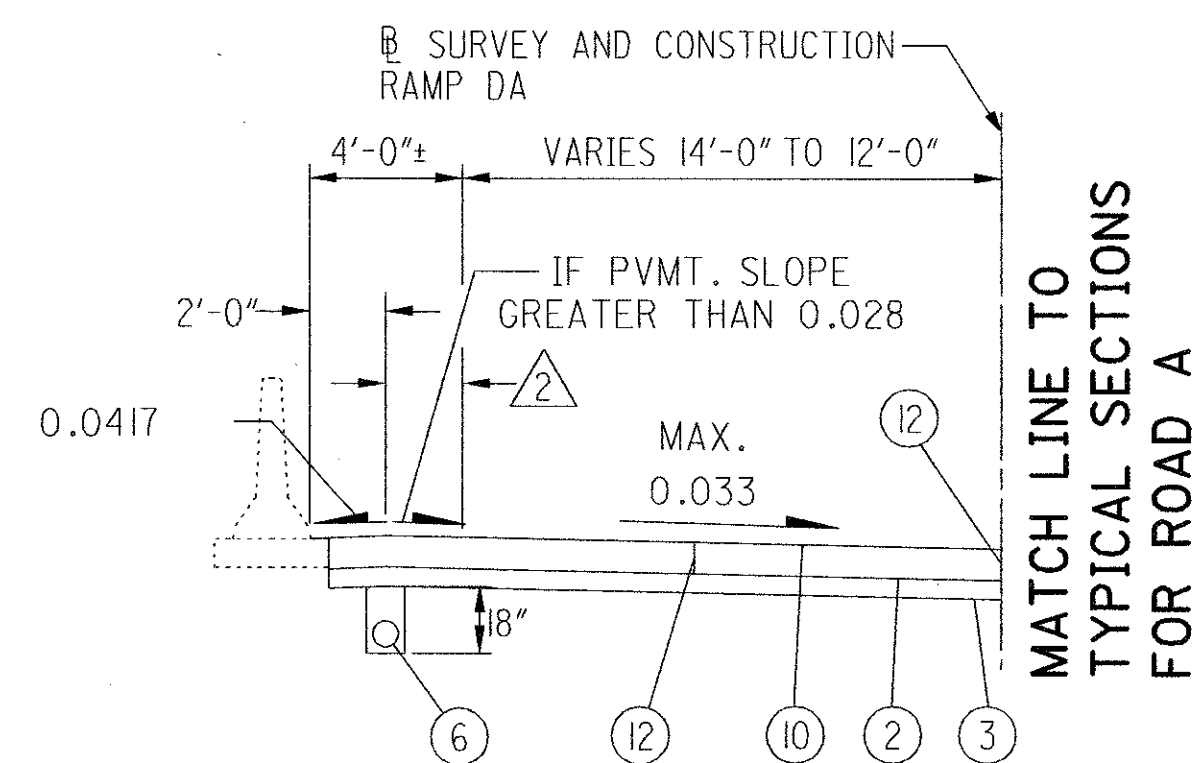




RAMP DA  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 527+41.23 TO STA. 527+78.63 = 37.40 LIN. FT.



RAMP DA  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 527+78.63 TO STA. 528+48.58 = 69.95 LIN. FT.



RAMP DA  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 528+48.58 TO STA. 530+18.14 = 169.56 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEETS 51 & 52

### RAMP DA

- ① 2'-0" CROWN SHOULDER TRANSITION  
TO BEGIN AT STA. 527+66.23
- ② CROWN SHOULDER TRANSITION  
STA. 528+48.58 to STA. 529+25.21 = 2'-0"  
STA. 529+25.21 to STA. 529+50.21 = 2'-0" to 0'-0"
- \* STA. 527+41.23 TO STA. 527+66.23 EXISTING  
SHOULDER SLOPE TO TRANSITION TO PROPOSED

### LEGEND

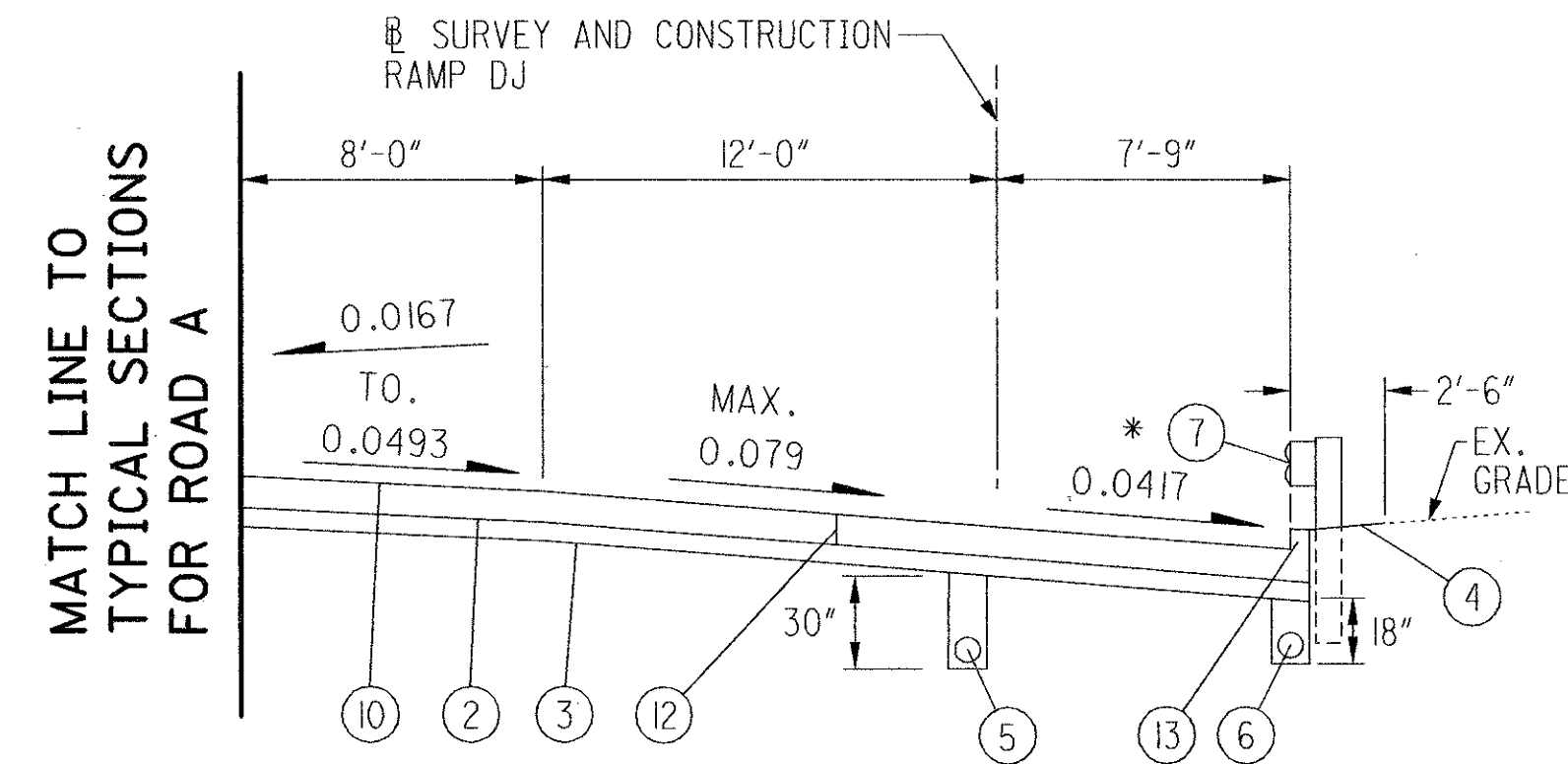
- ① -  
② ITEM 304 6" AGGREGATE BASE  
③ ITEM 203 SUBGRADE COMPACTION  
④ ITEM 659 SEEDING AND MULCHING  
⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)  
⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)  
⑦ ITEM 606 GUARDRAIL, TYPE 5  
⑧ -  
⑨ -  
⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT  
⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)  
⑫ STANDARD LONGITUDINAL JOINT  
⑬ INTEGRAL CURB TYPE 2-A  
⑭ UNTIED LONGITUDINAL JOINT

PROPOSED TYPICAL SECTIONS  
SR 315 - RAMP DA

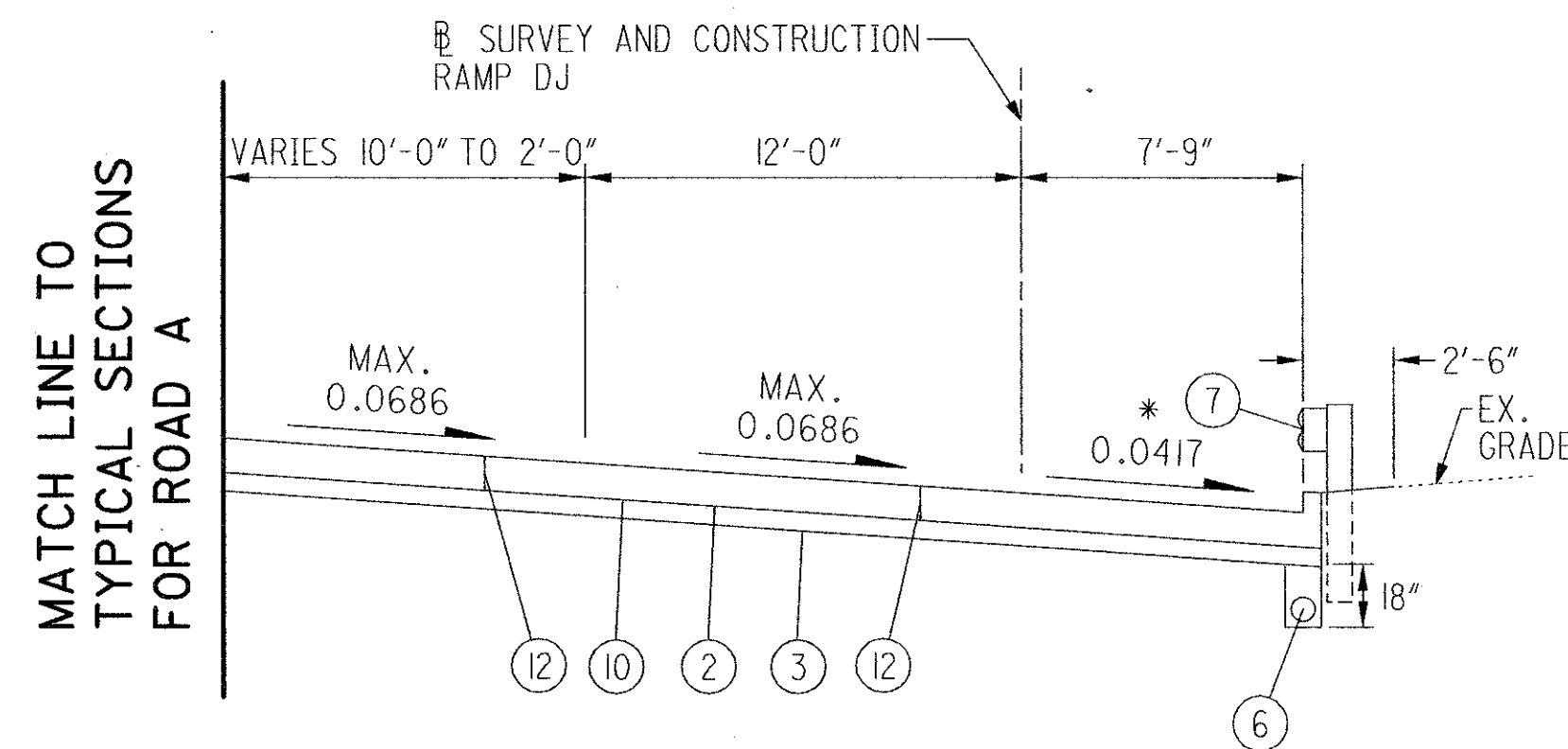
FRA-IR71-14.39,  
FRA-315-0.00

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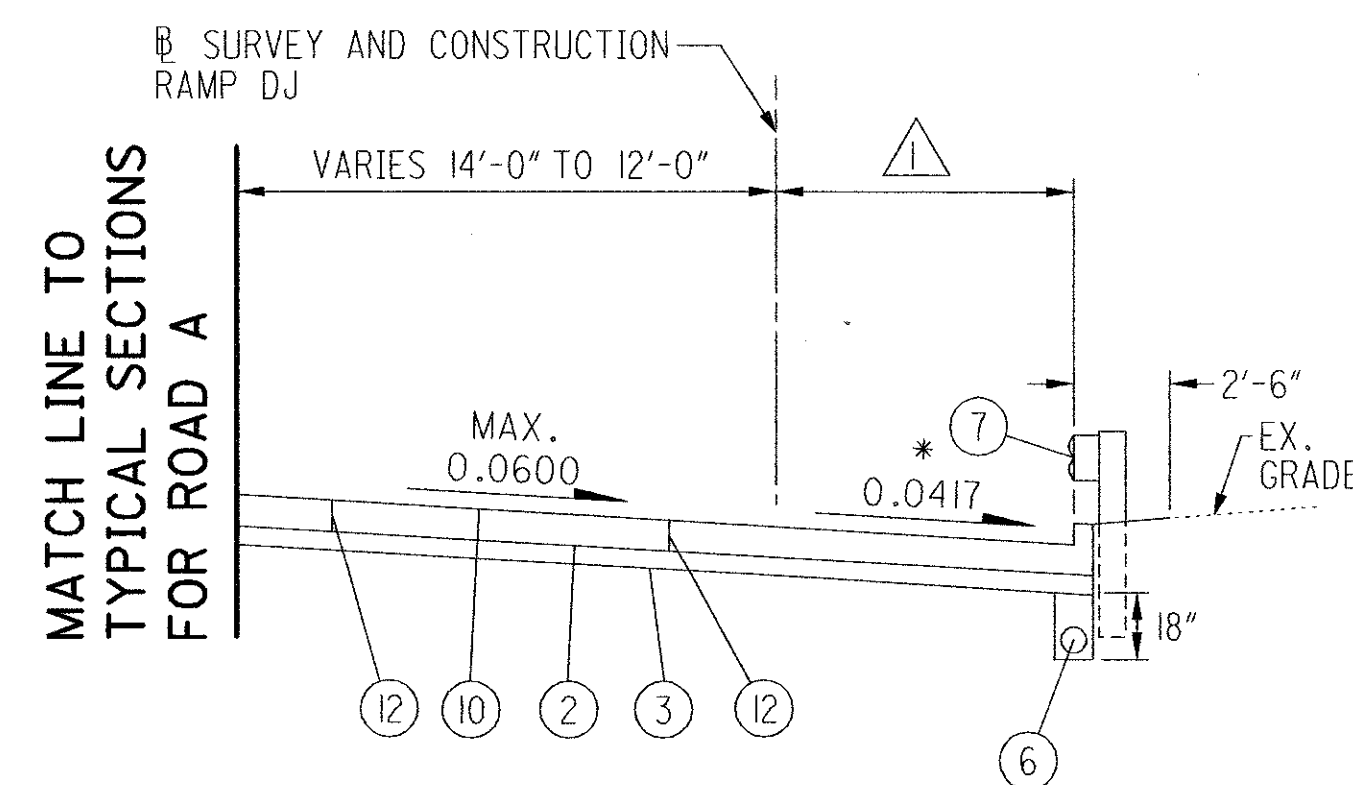
CALCULATED  
BBB  
CHECKED  
LJS



RAMP DJ  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 14+06.42 TO STA. 14+67.43 = 61.01 LIN. FT.



RAMP DJ  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 14+67.43 TO STA. 15+75.17 = 107.74 LIN. FT.



RAMP DJ  
SUPERELEVATED SECTION  
SECTION APPLIES:  
STA. 15+75.17 TO STA. 19+03.77 = 328.60 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEETS 51 & 52

RAMP DJ

△ STA. 15+75.17 TO STA. 15+96.45 = 7'-9"  
STA. 15+96.45 TO STA. 530+18.14 = 7'-9" TO 10'-6"

\* 0.0417 OR RATE OF SUPER  
WHICH EVER IS GREATER

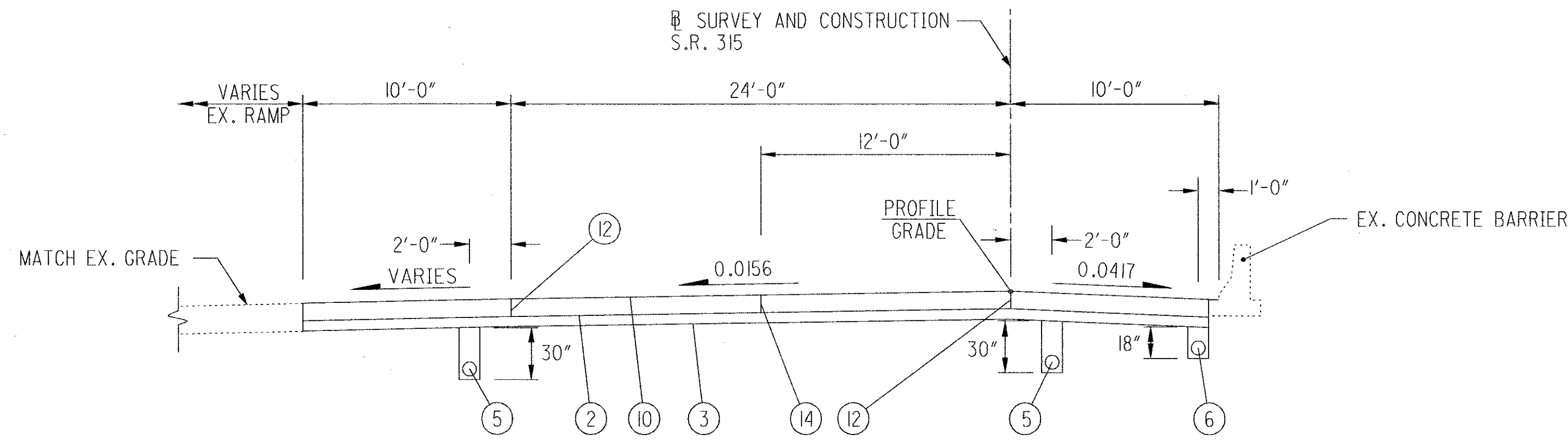
# LEGEND

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
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- ⑧ -
- ⑨ -
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- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

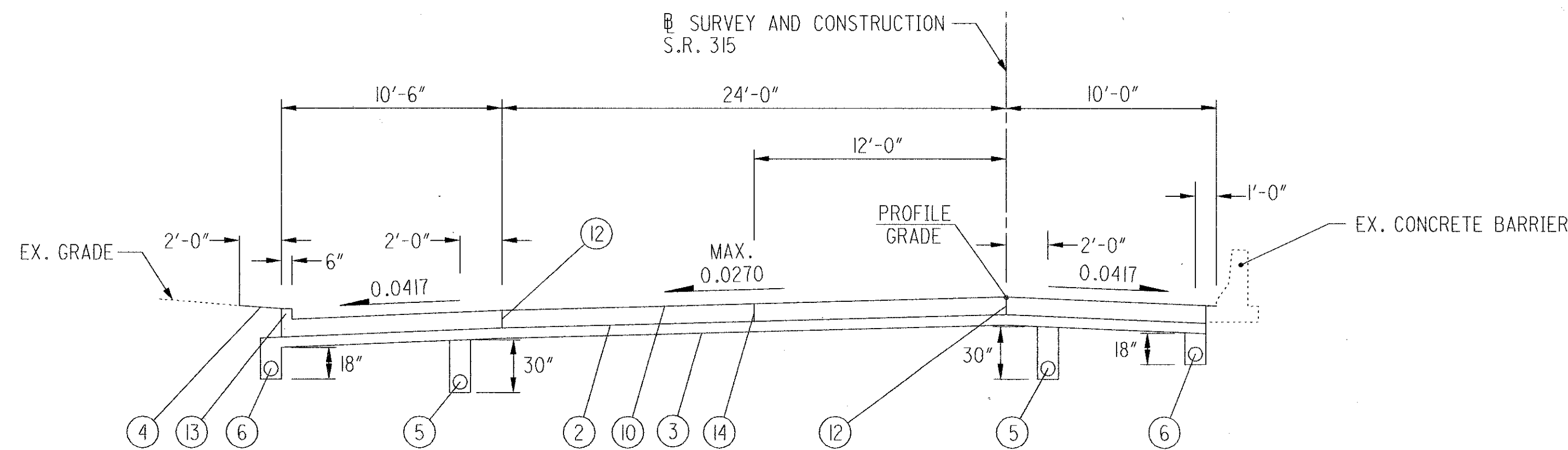
PROPOSED TYPICAL SECTIONS  
SR 315 - RAMP DJ

FRA-IR71-14.39,  
FRA-315-0.00

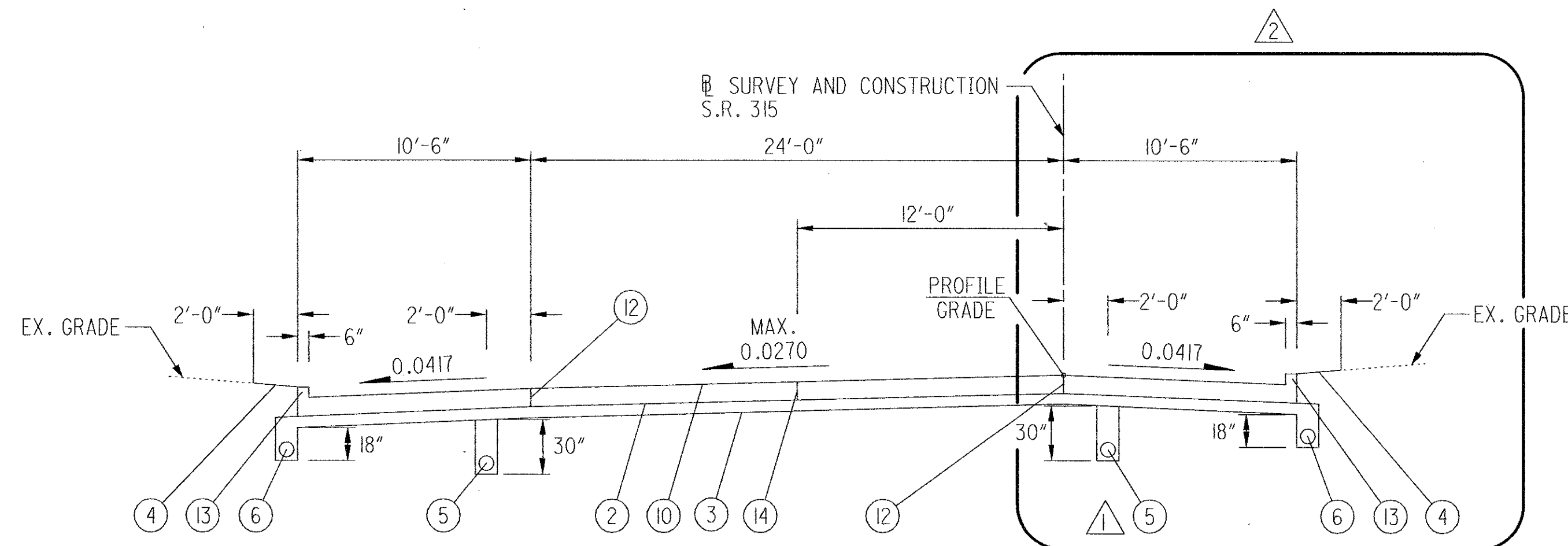
14  
89



**ROAD B  
 NORMAL SECTION**  
 SECTION APPLIES:  
 STA. 44+00.00 TO STA. 46+44.59 = 244.59 LIN. FT.



**ROAD B  
 SUPERELEVATED SECTION**  
 SECTION APPLIES:  
 STA. 46+44.59 TO STA. 51+00.00 = 455.41 LIN. FT.



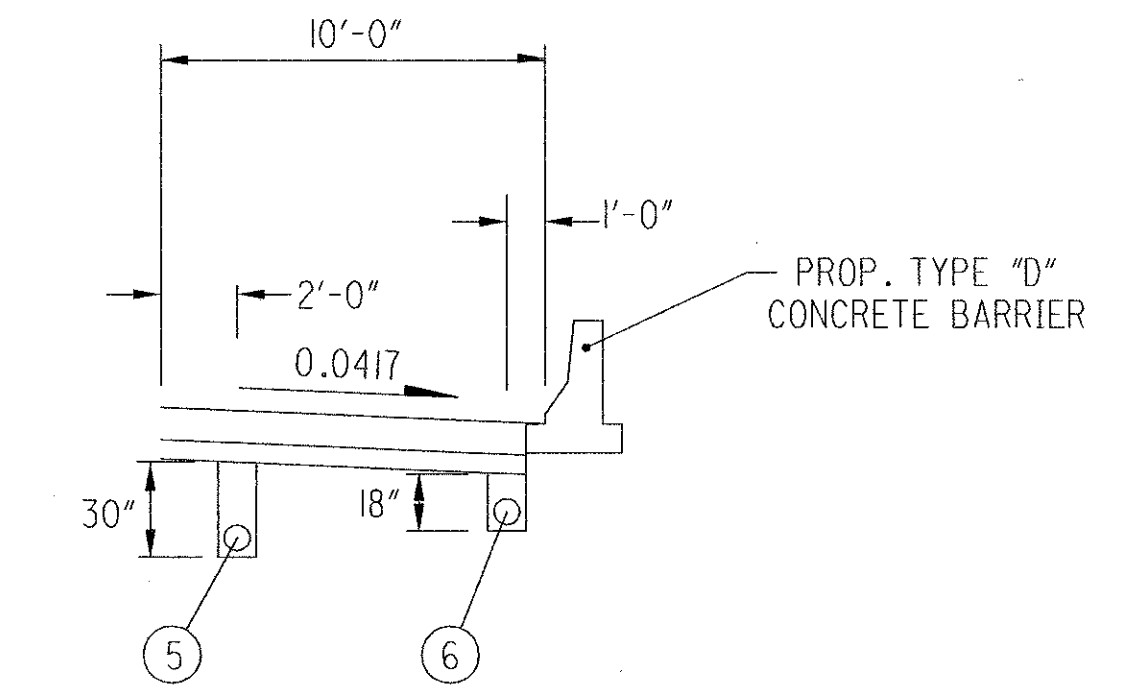
**ROAD B  
 SUPERELEVATED SECTION**  
 SECTION APPLIES:  
 STA. 51+00.00 TO STA. 53+10.33 = 210.33 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEET 46, 47, & 48

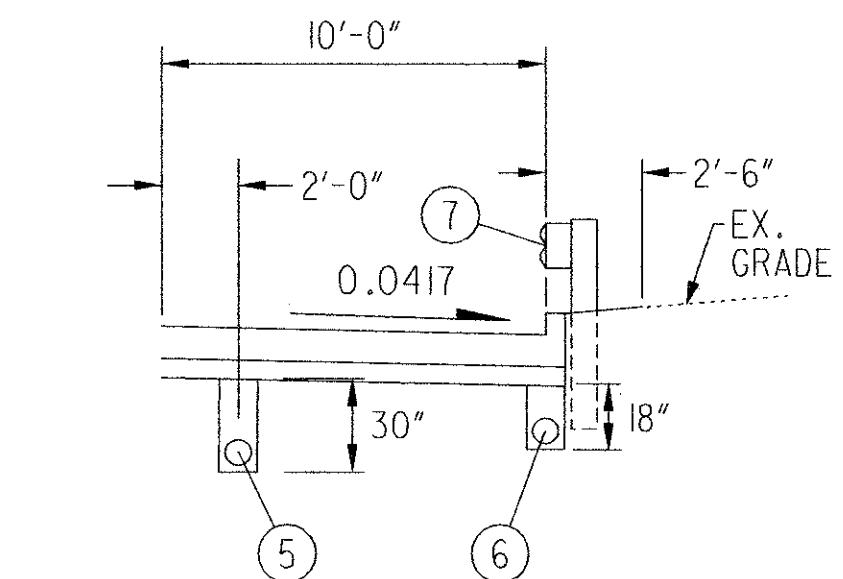
**ROAD B**

END UNDERDRAIN AT STA. 53+00.00

SEE DETAIL "A" AND DETAIL "B"



**DETAIL A**  
 SECTION APPLIES:  
 STA. 51+00.00 TO STA. 51+19.68



**DETAIL B**  
 SECTION APPLIES:  
 STA. 51+19.68 TO STA. 52+11.30

**LEGEND**

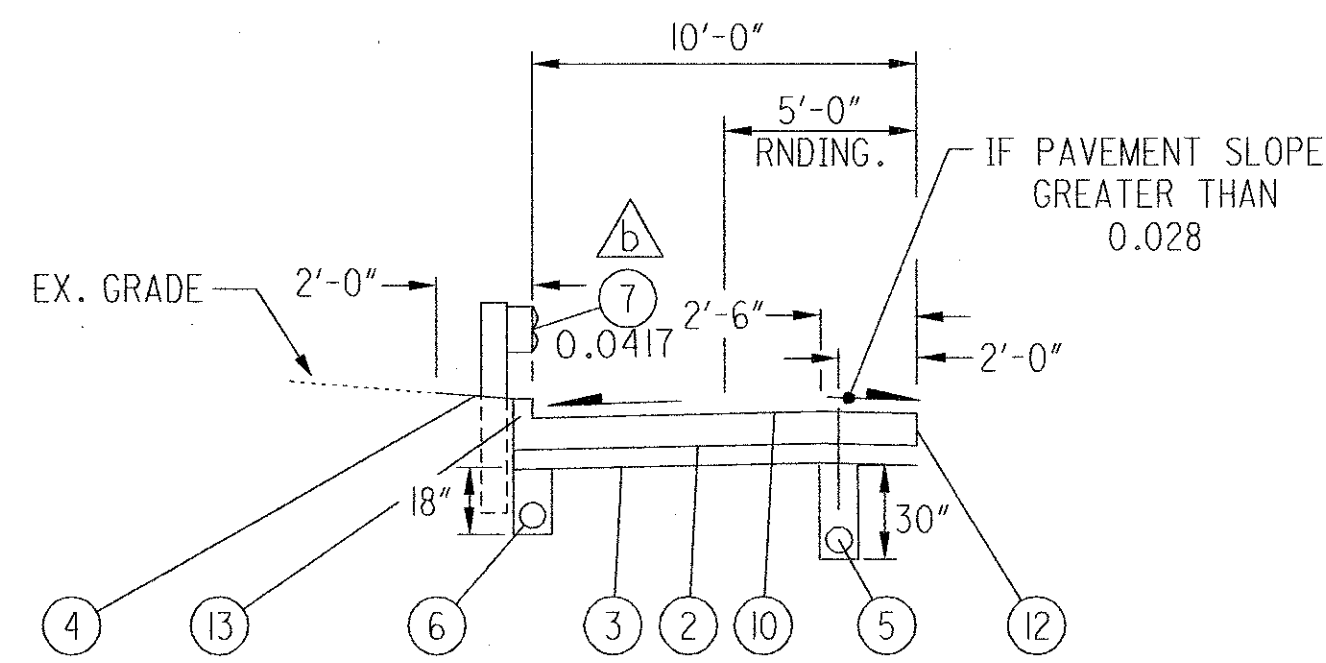
- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ -
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

**PROPOSED TYPICAL SECTIONS  
 SR 315 - ROAD B**

**FRA-IR71-14.39,  
 FRA-315-0.00**

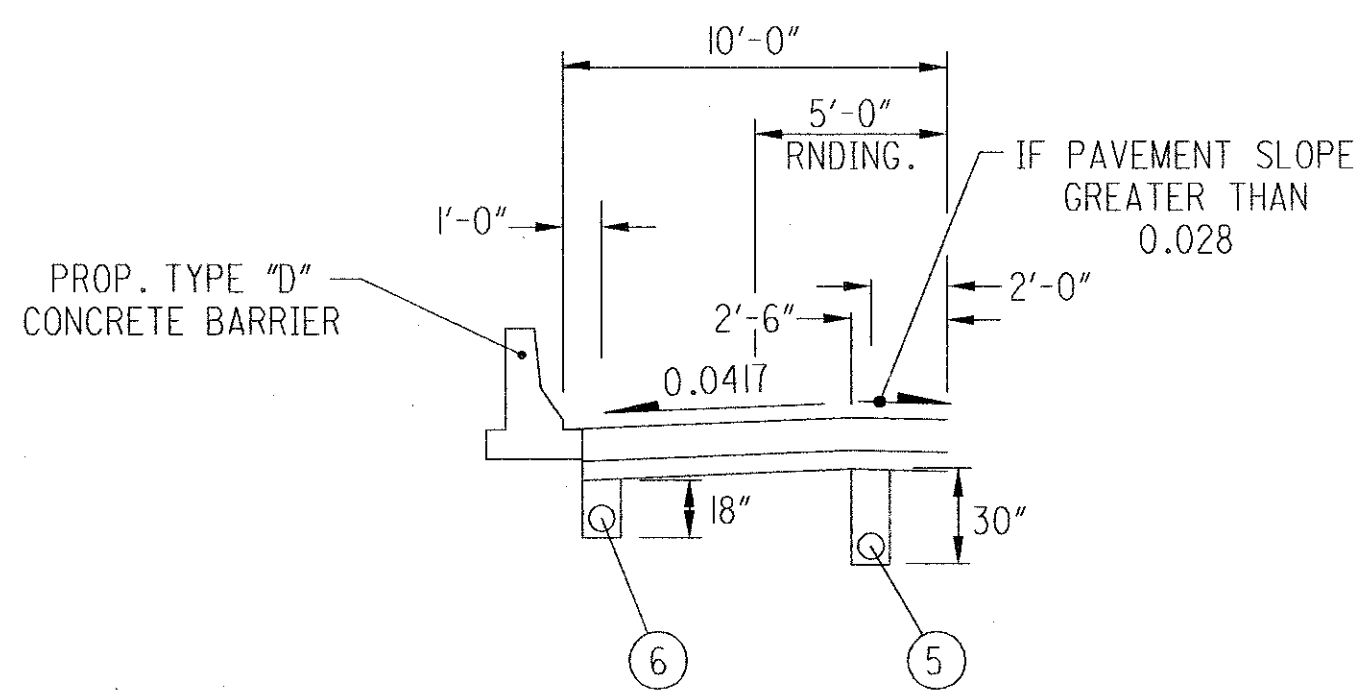


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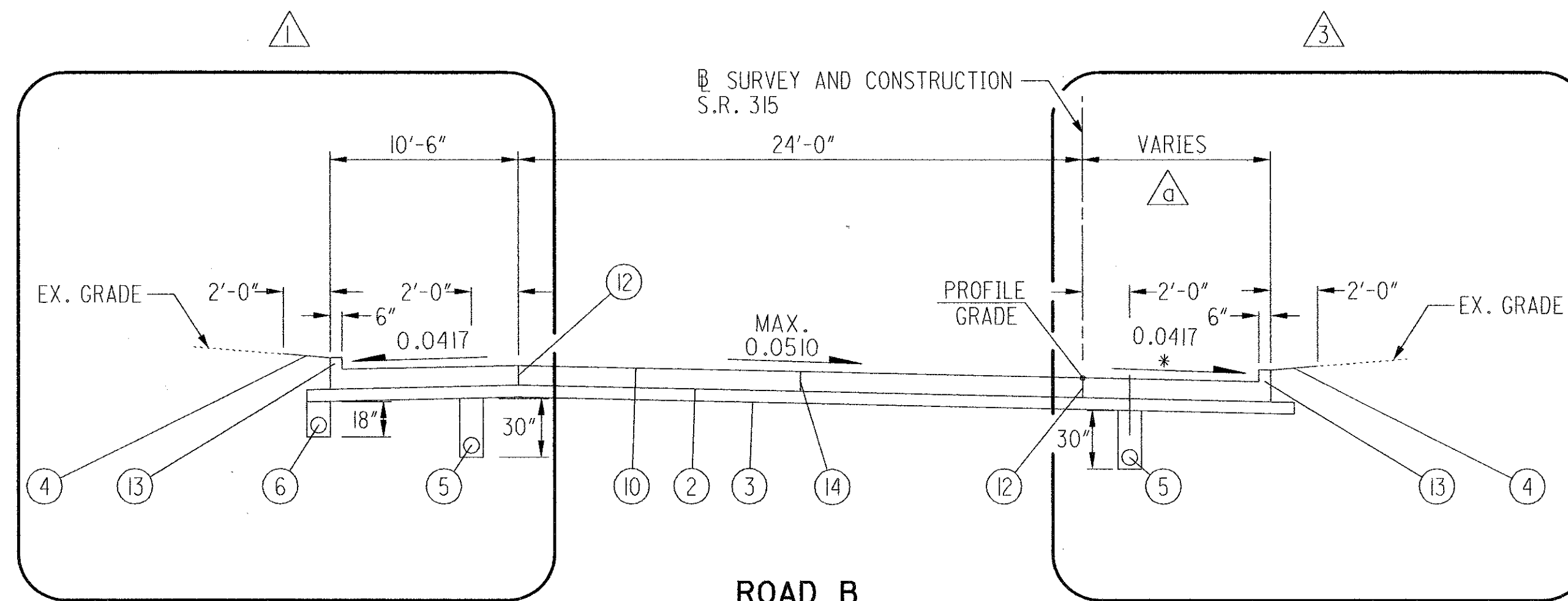
**DETAIL A**

SECTION APPLIES:  
STA. 54+75.00 TO STA. 64+14.60



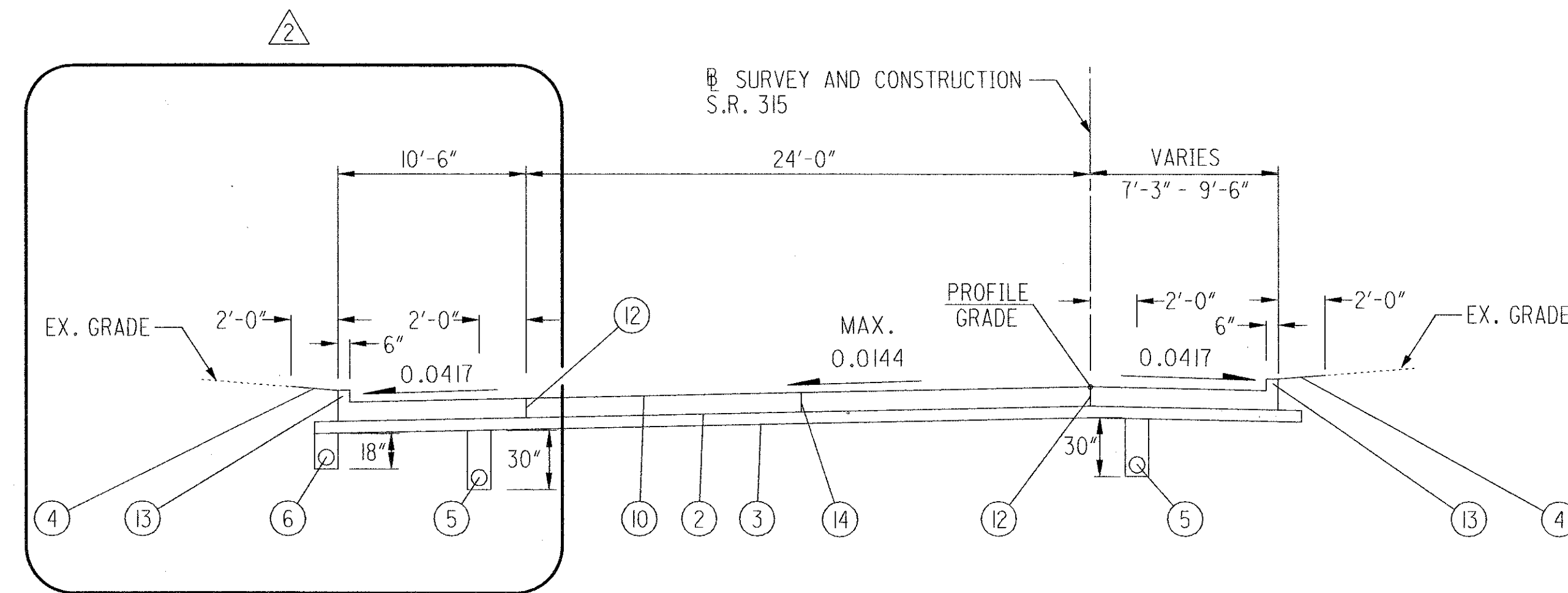
**DETAIL B**

SECTION APPLIES:  
STA. 53+24.97 TO STA. 55+71.57  
STA. 59+70.07 TO STA. 60+56.28



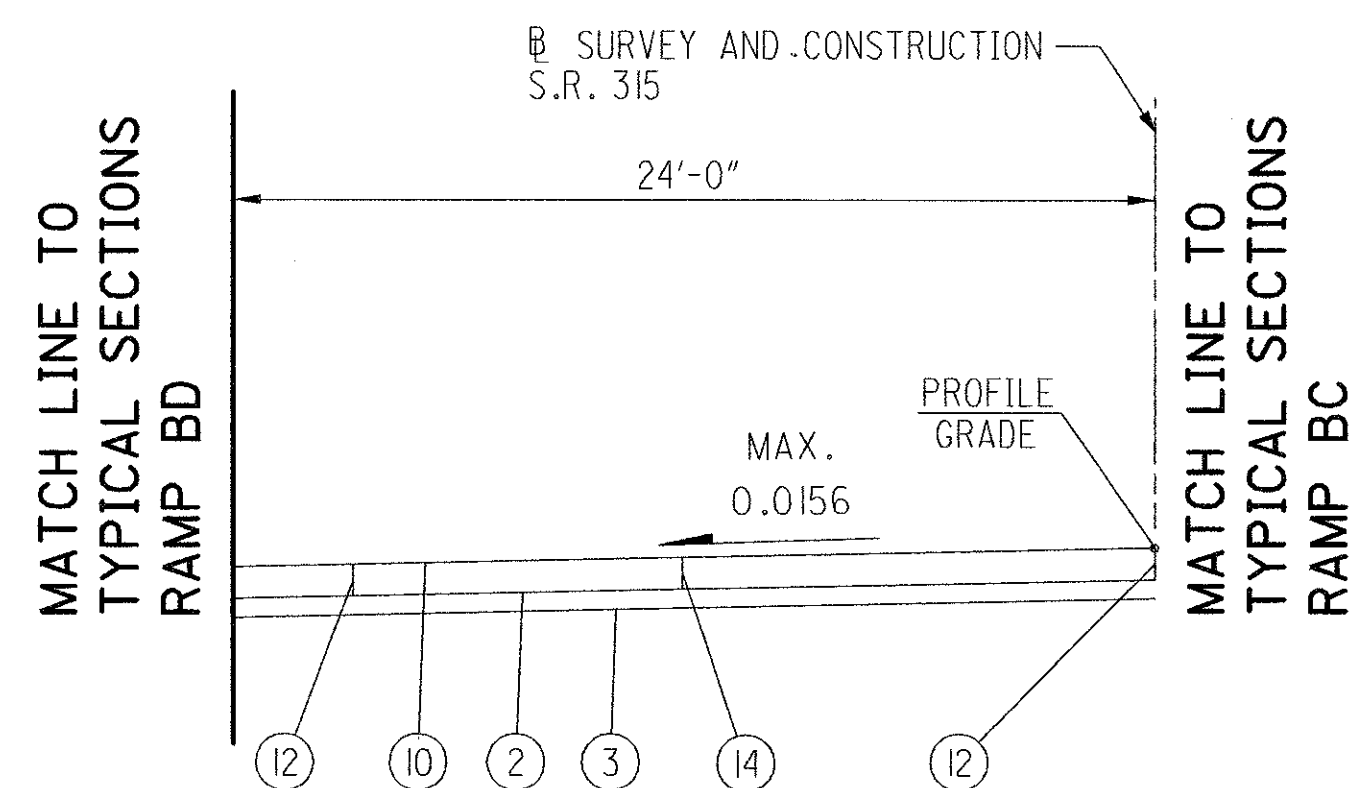
**ROAD B  
SUPERELEVATED SECTION**

SECTION APPLIES:  
STA. 53+10.33 TO STA. 65+54.21 = 1243.88 LIN. FT.



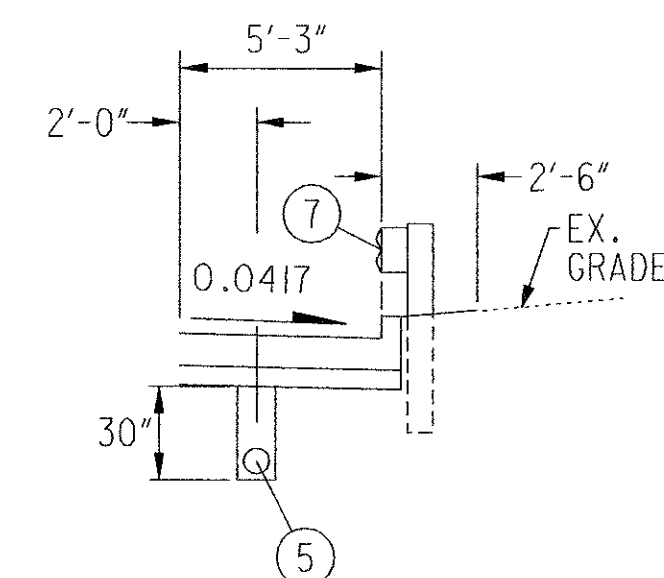
**ROAD B  
SUPERELEVATED SECTION**

SECTION APPLIES:  
STA. 65+54.21 TO STA. 66+52.69 = 98.48 LIN. FT.



**ROAD B  
NORMAL SECTION**

SECTION APPLIES:  
STA. 66+52.69 TO STA. 69+38.72 = 286.03 LIN. FT.



**DETAIL D**

SECTION APPLIES:  
STA. 56+46.42 TO STA. 57+25.42  
STA. 60+09.39 TO STA. 65+38.49

NOTE: SEE PAVEMENT DETAILS ON SHEET 48, 49, 50, & 51

**ROAD B**

\* OR RATE OF SUPER IF GREATER

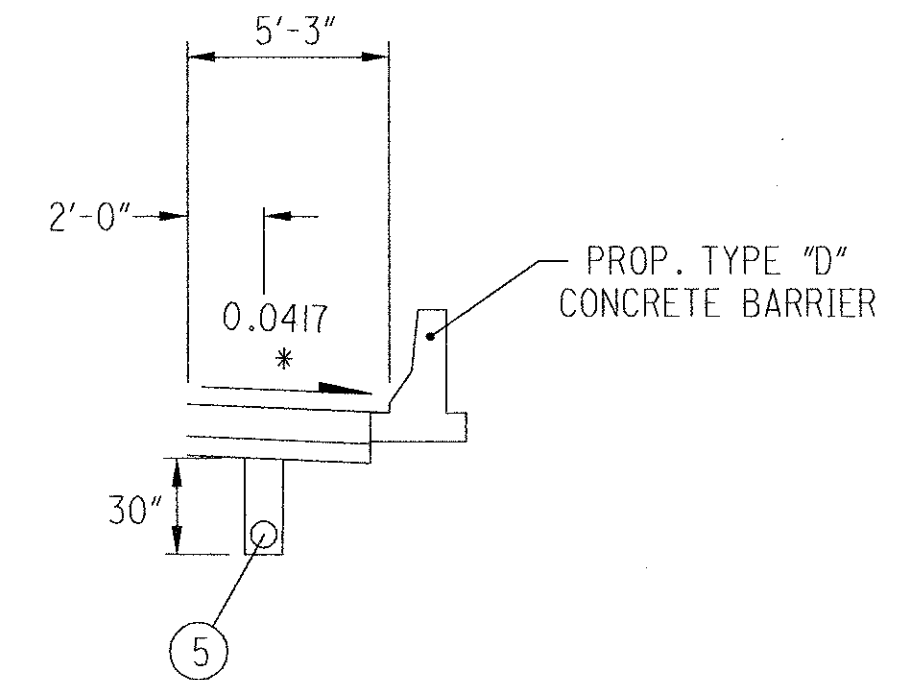
1 SEE DETAIL "A" AND DETAIL "B"

2 STA. 65+54.21 TO STA. 66+54.81

3 SEE DETAIL "C" AND DETAIL "D"

a STA. 53+10.33 TO STA. 53+74.98 = 10'-6"  
STA. 53+74.98 TO STA. 55+17.52 = 10'-6" TO 5'-9"  
STA. 55+17.52 TO STA. 64+94.23 = 5'-9"  
STA. 64+94.23 TO STA. 65+54.21 = 5'-9" TO 7'-3"

b GUARDRAIL  
STA. 55+71.57 TO STA. 57+12.97  
STA. 60+56.26 TO STA. 64+35.30



**DETAIL C**

SECTION APPLIES:  
STA. 55+55.54 TO STA. 56+46.41  
STA. 59+12.91 TO STA. 60+09.37

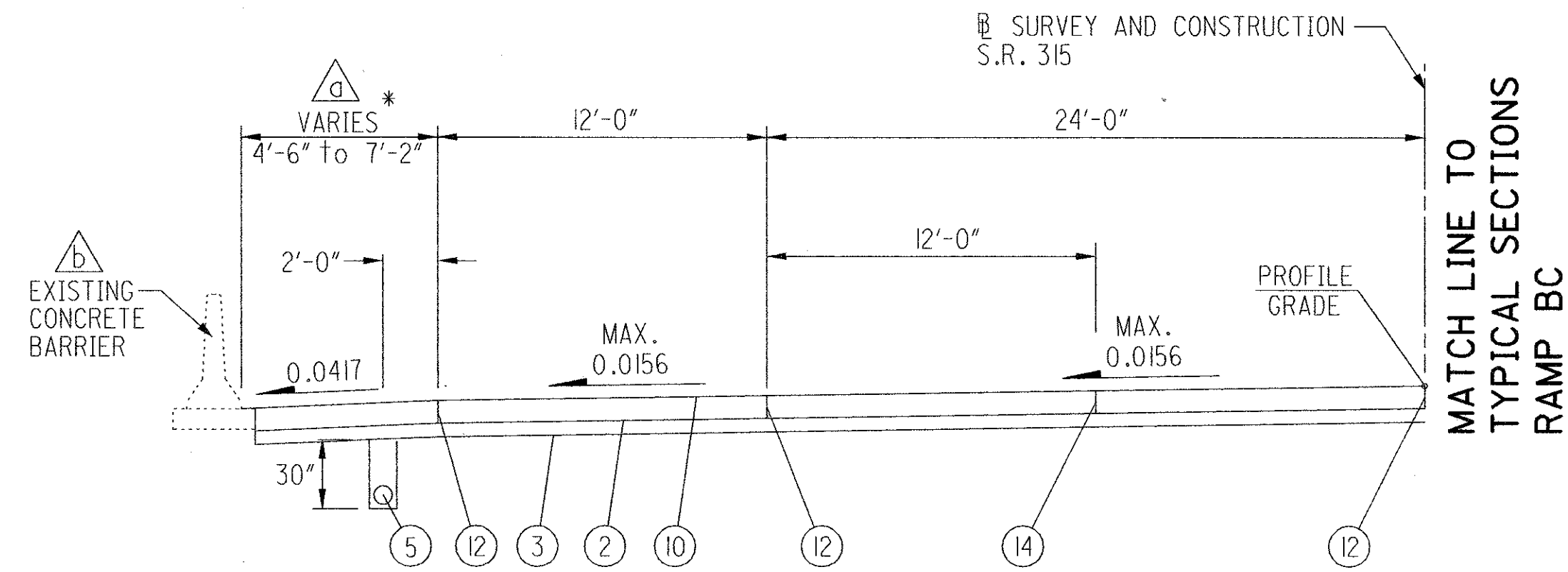
**LEGEND**

- 1 -
- 2 ITEM 304 6" AGGREGATE BASE
- 3 ITEM 203 SUBGRADE COMPACTION
- 4 ITEM 659 SEEDING AND MULCHING
- 5 ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- 6 ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- 7 ITEM 606 GUARDRAIL, TYPE 5
- 8 -
- 9 -
- 10 ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- 11 ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- 12 STANDARD LONGITUDINAL JOINT
- 13 INTEGRAL CURB TYPE 2-A
- 14 UNTIED LONGITUDINAL JOINT

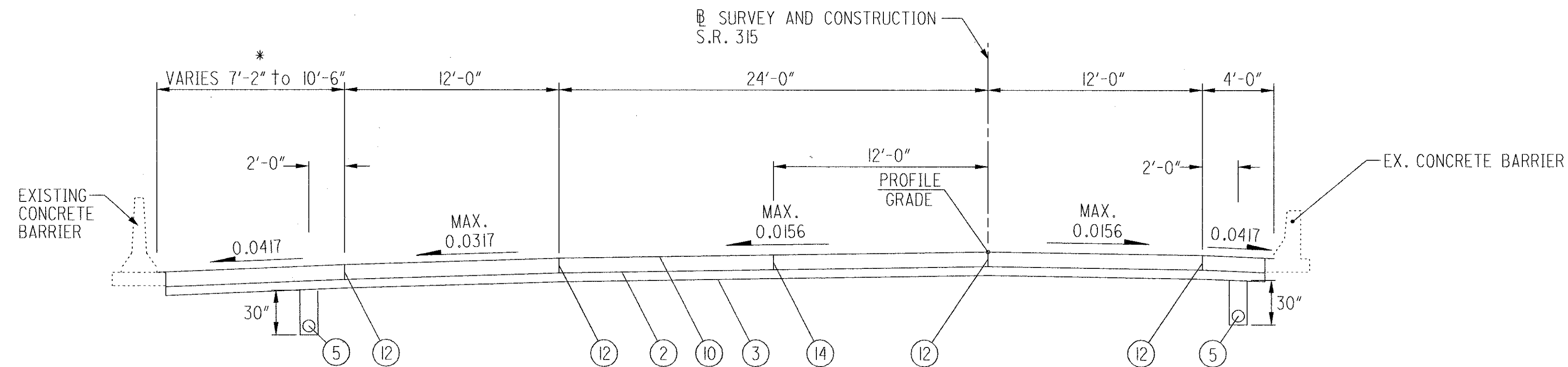
**PROPOSED TYPICAL SECTIONS  
SR 315 - ROAD B**

**FRA-IR71-14.39,  
FRA-315-0.00**

16  
89



**ROAD B  
 NORMAL SECTION**  
 SECTION APPLIES:  
 STA. STA. 69+38.72 TO STA. 71+16.14 = 177.42 LIN. FT.



**ROAD B  
 NORMAL SECTION**  
 SECTION APPLIES:  
 STA. 71+16.14 TO STA. 72+36.61 = 120.47 LIN. FT.

NOTE: SEE PAVEMENT DETAILS ON SHEET 51 & 52

**ROAD B**

△ STA. 69+38.72 to STA. 70+49.98 = 4'-6"  
 STA. 70+49.98 to STA. 71+16.14 = 4'-6" to 7'-2"

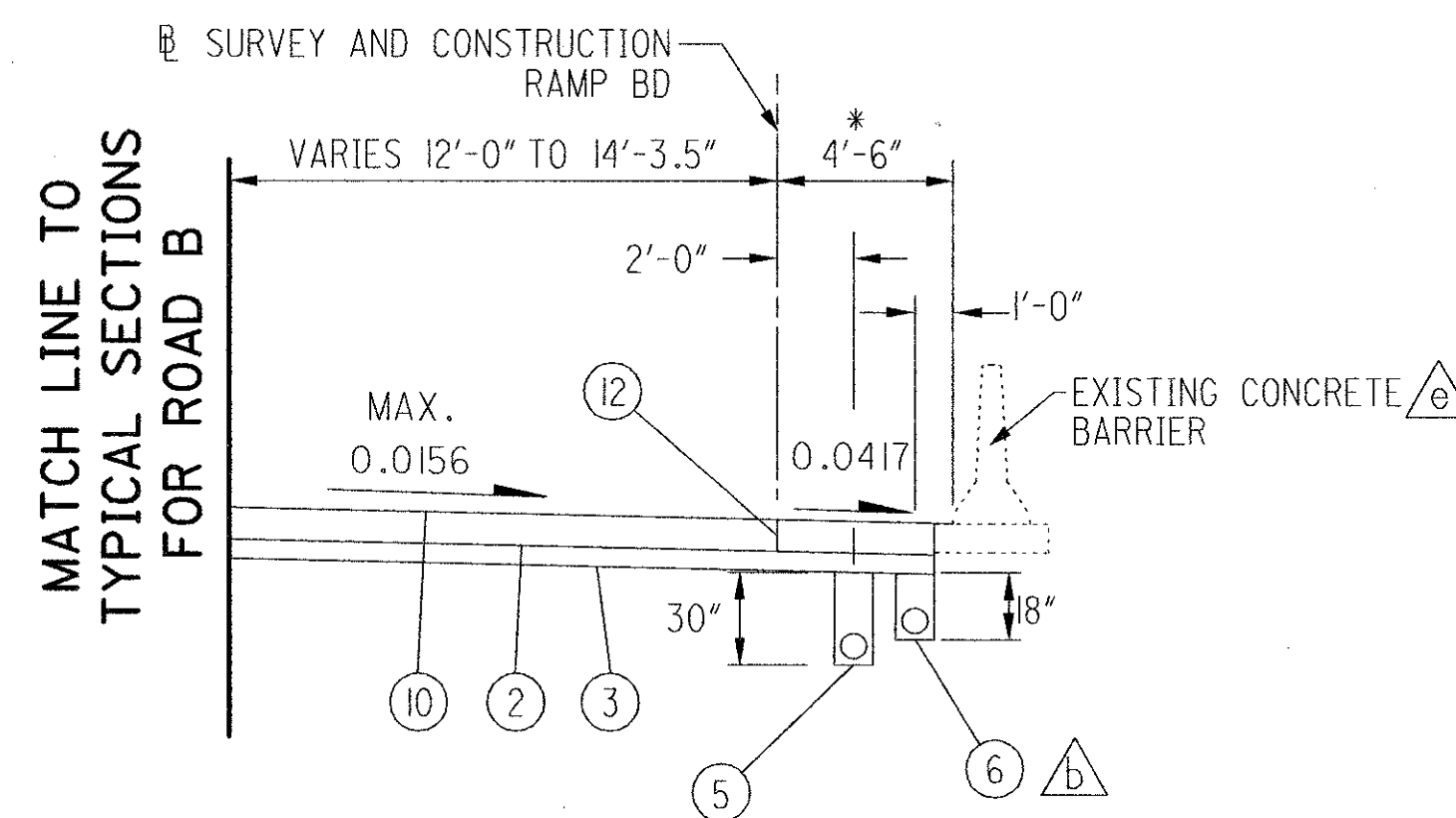
△ REPLACE EXISTING CONCRETE BARRIER  
 STA. 69+38.72 to STA. 69+55.62

\* DESIGN EXCEPTION  
 (NDC = 10'-0")

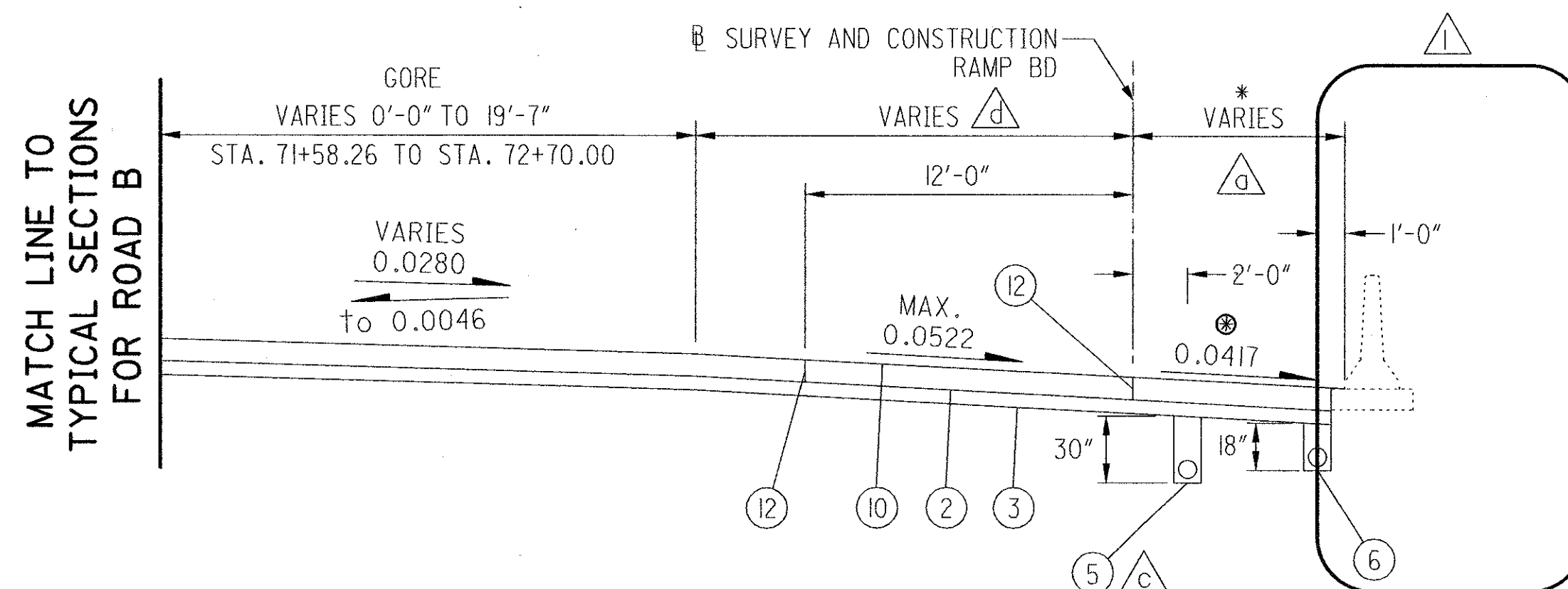
**LEGEND**

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ -
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

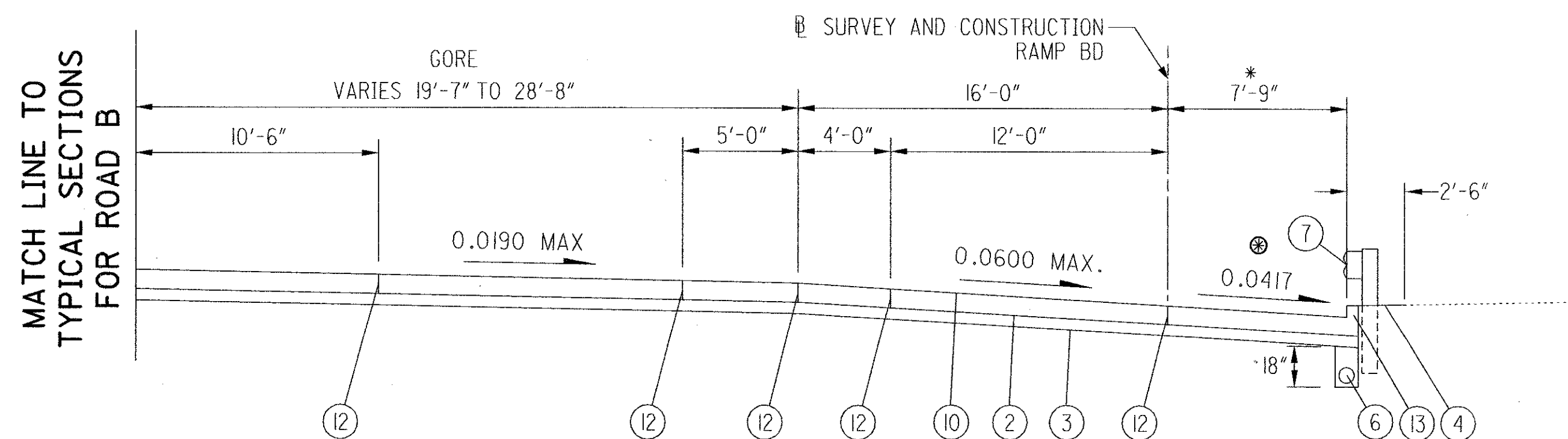
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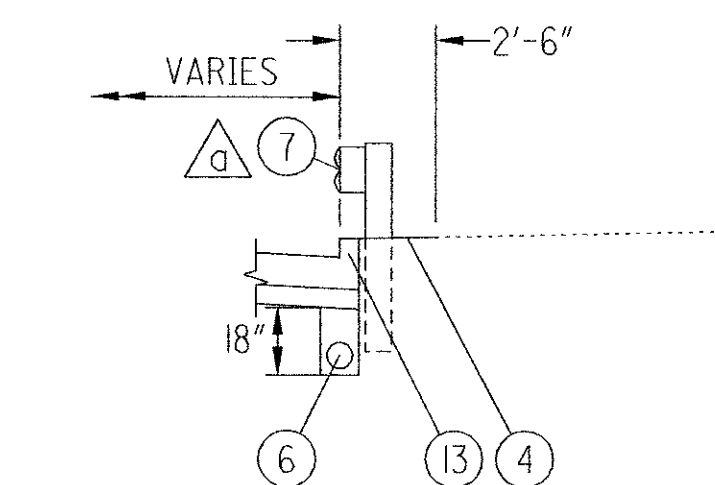
**RAMP BD  
NORMAL SECTION**  
SECTION APPLIES:  
STA. 70+19.97 TO STA. 71+35.00 = 115.03 LIN. FT.



**RAMP BD  
SUPERELEVATED SECTION**  
SECTION APPLIES:  
STA. 71+35.00 TO STA. 72+70.00 = 135.00 LIN. FT.



**RAMP BD  
SUPERELEVATED SECTION**  
SECTION APPLIES:  
STA. 72+70.00 TO STA. 73+00.00 = 30.00 LIN. FT.



**DETAIL A**  
SECTION APPLIES:  
STA. 72+00.00 TO STA. 72+70.00

NOTE: SEE PAVEMENT DETAILS ON SHEET 51

**RAMP BD**

- \* DESIGN EXCEPTION  
(NDC = 8'-0")
- ⊗ OR RATE OF SUPER IF GREATER
- △ STA. 70+19.97 to STA. 71+71.56 = 4'-6"  
STA. 71+71.56 to STA. 72+70.00 = 4'-6" to 7'-9"
- △ BEGIN UNDERDRAIN AT STA. 70+32.00
- △ END UNDERDRAIN STA. 71+13.08
- △ STA. 71+35.00 to STA. 71+58.26 = 14'-3.5" to 16'-0"  
STA. 71+58.26 to STA. 72+70.00 = 16'-0"
- △ SEE DETAIL "A"
- △ REPLACE EXISTING CONCRETE BARRIER  
STA. 70+19.97 TO STA. 70+3.08  
STA. 70+33.15 TO STA. 70+53.17

**LEGEND**

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ -
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

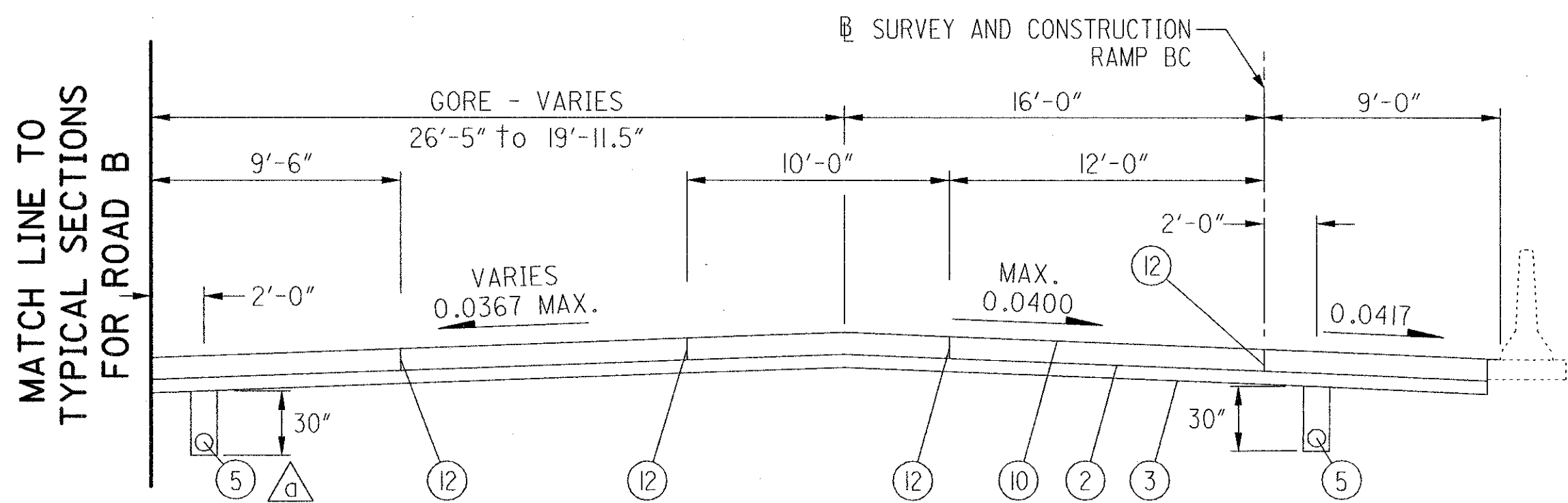
NOTE: GORE MEASUREMENTS ARE PERPENDICULAR TO ROAD B.

**PROPOSED TYPICAL SECTIONS  
SR 315 - RAMP BD**

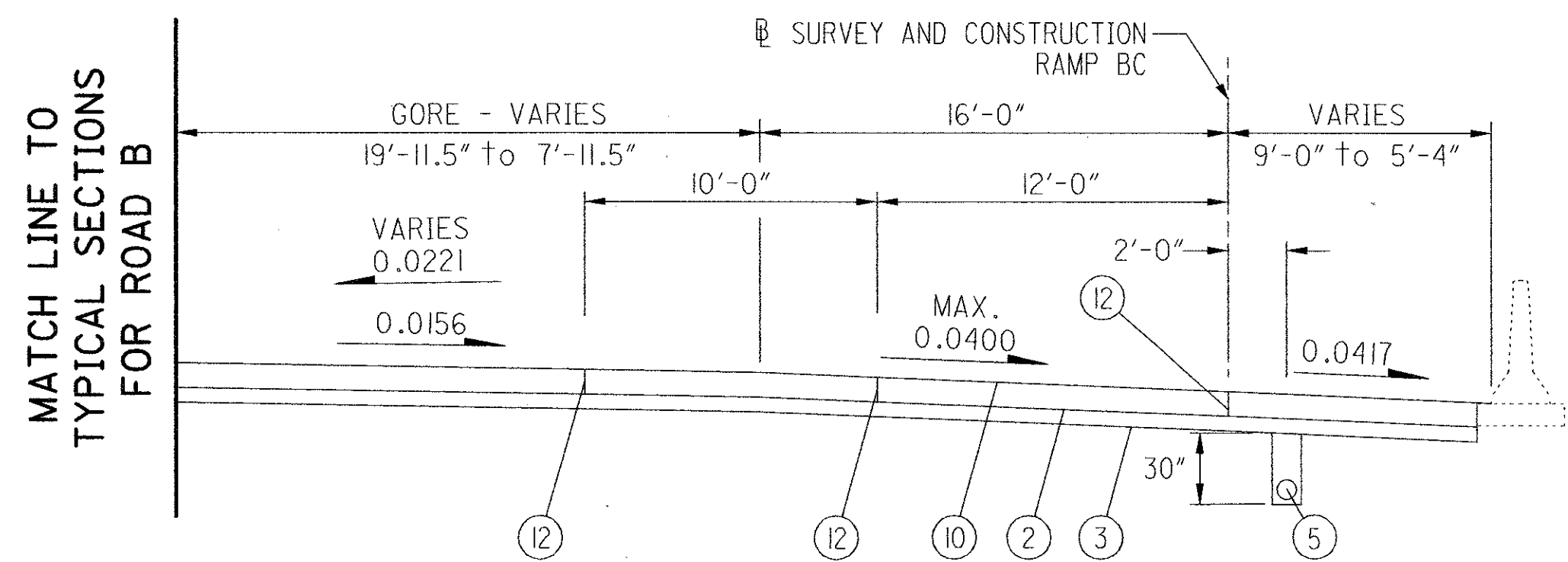
**FRA-IR71-14.39,  
FRA-315-0.00**

18  
89

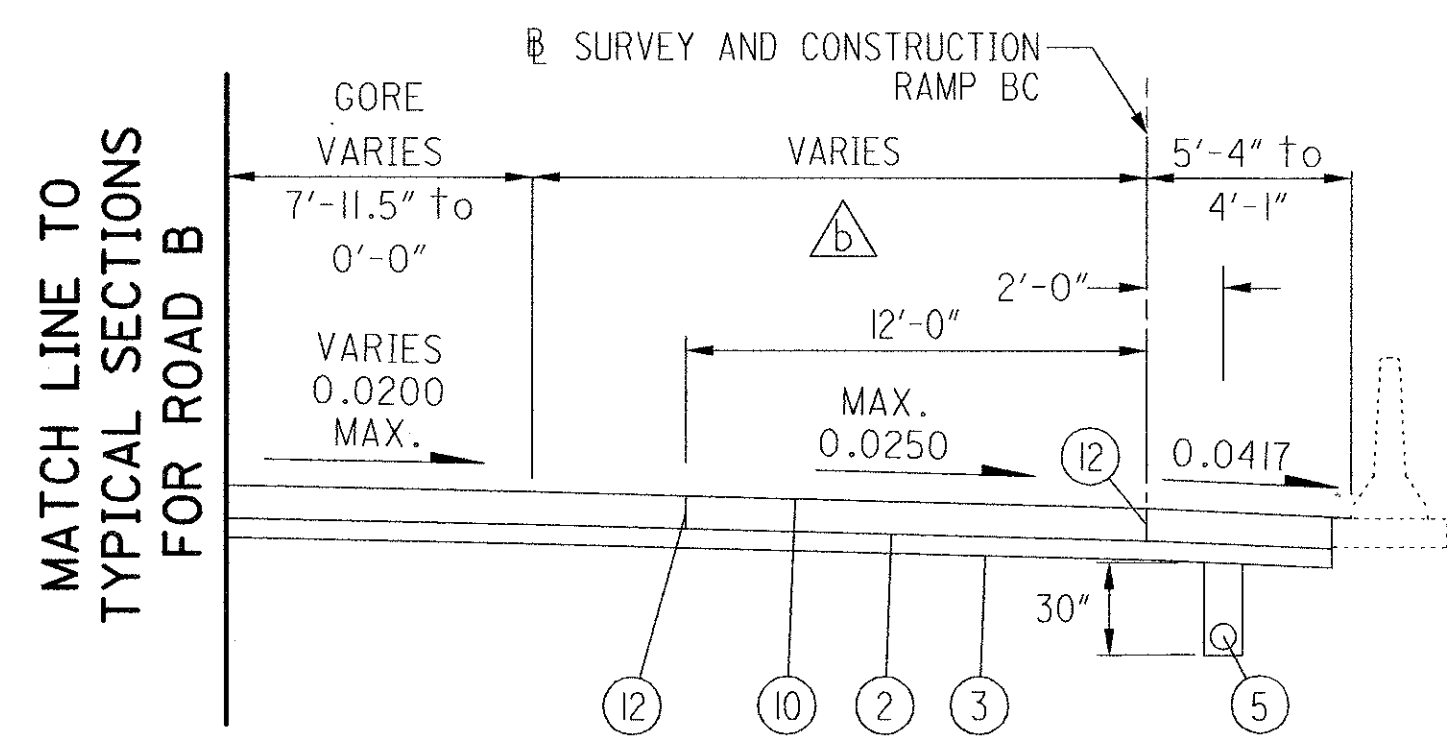




**RAMP BC  
SUPERELEVATED SECTION**  
SECTION APPLIES:  
STA. 66+40.99 TO STA. 66+81.50 = 40.51 LIN. FT.



**RAMP BC  
SUPERELEVATED SECTION**  
SECTION APPLIES:  
STA. 66+81.50 TO STA. 68+00.00 = 118.50 LIN. FT.



**RAMP BC  
SUPERELEVATED SECTION**  
SECTION APPLIES:  
STA. 68+00.00 TO STA. 71+01.74 = 301.74 LIN. FT.

**RAMP BC**

END UNDERDRAIN AT STA. 66+95.00

STA. 68+00.00 TO STA. 69+27.26 = 16'-0"  
STA. 69+27.26 TO STA. 71+01.74 = 16'-0" TO 12'-0"

**LEGEND**

- ① -
- ② ITEM 304 6" AGGREGATE BASE
- ③ ITEM 203 SUBGRADE COMPACTION
- ④ ITEM 659 SEEDING AND MULCHING
- ⑤ ITEM 605 4" PIPE UNDERDRAIN (30" DEPTH)
- ⑥ ITEM 605 4" PIPE UNDERDRAIN (18" DEPTH)
- ⑦ ITEM 606 GUARDRAIL, TYPE 5
- ⑧ -
- ⑨ -
- ⑩ ITEM 451 10" REINFORCED CONCRETE PAVEMENT
- ⑪ ITEM 605 4" PIPE UNDERDRAIN (UNCLASSIFIED)
- ⑫ STANDARD LONGITUDINAL JOINT
- ⑬ INTEGRAL CURB TYPE 2-A
- ⑭ UNTIED LONGITUDINAL JOINT

NOTE: GORE MEASUREMENTS ARE PERPENDICULAR  
TO ROAD B.

PROFILE AND ALIGNMENT  
(RESURFACING)

THE PROPOSED PAVEMENT RESURFACING FROM STA. 16+52.48 TO STA. 46+30.00, ROAD A, AND STA. 16+95.89 TO STA. 44+00.00, ROAD B, SHALL FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PREVIOUS CONSTRUCTION PLANS, LISTED BELOW SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 6 OFFICE, 400 EAST WILLIAM STREET, DELAWARE, OHIO. THE PROPOSED ASPHALT CONCRETE OVERLAY SHALL HAVE A UNIFORM THICKNESS OF 1.5 INCHES AND SHALL BE APPLIED AFTER A MAXIMUM OF 1.5 INCHES OF SURFACE BITUMINOUS MATERIAL HAS BEEN REMOVED. SEE TYPICAL SECTIONS FOR DETAILS.

REFERENCE PROJECTS:

PROJECT NO. I-609(9), ALSO KNOWN AS  
FRA-62-12.56/FRA-3-13.70  
PROJECT NO. I-70-3(39)96, ALSO KNOWN AS  
FRA-70-12.31S

GENERAL NOTES

FRA-171-14.39,  
FRA-315-0.00

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														ROADWAY "A"									
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR	WIDTH	SLOPE	CORR	ELEV
Begin	46+00.00																	46+00.00	720.50				
	46+25.00	ROAD AD - DROP LANE																46+25.00	720.40				
	46+30.00	720.37	-0.26	-0.0217	12.00									720.63	0.10	0.0083	12.00	46+30.00	720.53	12.00	-0.0142	-0.17	720.36
	46+50.00	720.27	-0.24	-0.0200	12.00									720.51	0.13	0.0108	12.00	46+50.00	720.38	12.00	-0.0156	-0.19	720.19
	46+75.00	720.10	-0.21	-0.0175	12.00									720.31	0.16	0.0133	12.00	46+75.00	720.15	12.00	-0.0156	-0.19	719.96
	47+00.00	719.90	-0.19	-0.0156	12.00									720.09	0.19	0.0156	12.00	47+00.00	719.90	12.00	-0.0156	-0.19	719.71
	47+25.00	719.66	-0.19	-0.0156	12.00									719.85	0.19	0.0156	12.00	47+25.00	719.66	12.00	-0.0156	-0.19	719.48
	47+50.00	719.39	-0.19	-0.0156	12.00									719.58	0.19	0.0156	12.00	47+50.00	719.39	12.00	-0.0156	-0.19	719.20
	47+75.00	719.08	-0.19	-0.0156	12.00									719.27	0.19	0.0156	12.00	47+75.00	719.08	12.00	-0.0156	-0.19	718.90
	48+00.00	718.74	-0.19	-0.0156	12.00									718.93	0.19	0.0156	12.00	48+00.00	718.74	12.00	-0.0156	-0.19	718.55
	48+25.00	718.36	-0.19	-0.0156	12.00									718.55	0.19	0.0156	12.00	48+25.00	718.36	12.00	-0.0156	-0.19	718.18
	48+50.00	717.95	-0.19	-0.0156	12.00									718.14	0.19	0.0156	12.00	48+50.00	717.95	12.00	-0.0156	-0.19	717.76
	48+75.00	717.50	-0.19	-0.0156	12.00									717.69	0.19	0.0156	12.00	48+75.00	717.50	12.00	-0.0156	-0.19	717.32
	49+00.00	717.03	-0.19	-0.0156	12.00									717.22	0.20	0.0167	12.00	49+00.00	717.02	12.00	-0.0167	-0.20	716.82
	49+05.22	716.93	-0.19	-0.0156	12.00									717.12	0.20	0.0167	12.00	49+05.22	716.92	12.00	-0.0167	-0.20	716.72
TS	49+25.00	716.56	-0.17	-0.0145	12.00									716.73	0.21	0.0175	12.00	49+25.00	716.52	12.00	-0.0175	-0.21	716.31
	49+50.00	716.12	-0.14	-0.0120	12.00									716.26	0.24	0.0200	12.00	49+50.00	716.02	12.00	-0.0200	-0.24	715.78
	49+75.00	715.67	-0.11	-0.0096	12.00					GORE BETWEEN AD AND A				715.79	0.27	0.0224	12.00	49+75.00	715.52	12.00	-0.0224	-0.27	715.25
	50+00.00	715.23	-0.09	-0.0071	12.00									715.32	0.30	0.0249	12.00	50+00.00	715.02	12.00	-0.0249	-0.30	714.72
	50+25.00	714.73	-0.05	-0.0042	12.00	714.78				714.85	0.00	-0.0047	0.17	714.85	0.33	0.0273	12.00	50+25.00	714.52	12.00	-0.0273	-0.33	714.19
	50+50.00	714.29	-0.02	-0.0017	12.00	714.31				714.38	0.00	-0.0022	0.43	714.38	0.36	0.0298	12.00	50+50.00	714.02	12.00	-0.0298	-0.36	713.66
	50+75.00	713.85	0.01	0.0008	12.00	713.84				713.91	0.00	0.0002	0.79	713.91	0.39	0.0322	12.00	50+75.00	713.52	12.00	-0.0322	-0.39	713.13
	51+00.00	713.41	0.03	0.0025	12.00	713.38				713.44	0.00	0.0027	1.28	713.44	0.42	0.0347	12.00	51+00.00	713.02	12.00	-0.0347	-0.42	712.60
	51+25.00	712.97	0.06	0.0050	12.00	712.91				712.98	0.01	0.0051	1.90	712.97	0.45	0.0371	12.00	51+25.00	712.52	12.00	-0.0371	-0.45	712.07
	51+50.00	712.55	0.09	0.0075	12.00	712.46				712.52	0.02	0.0076	2.70	712.50	0.48	0.0396	12.00	51+50.00	712.02	12.00	-0.0396	-0.48	711.54
	51+75.00	712.12	0.12	0.0100	12.00	712.00				712.06	0.04	0.0100	3.67	712.02	0.50	0.0420	12.00	51+75.00	711.52	12.00	-0.0420	-0.50	711.02
	52+00.00	711.69	0.15	0.0125	12.00	711.54				711.61	0.06	0.0125	4.81	711.55	0.53	0.0445	12.00	52+00.00	711.02	12.00	-0.0445	-0.53	710.49
SC	52+05.22									711.51	0.06	0.0122	5.11	711.45	0.53	0.0442	12.00	52+05.22	710.92	12.00	-0.0442	-0.53	710.39
	52+25.00	711.24	0.16	0.0133	12.00	711.08				711.14	0.08	0.0130	6.23	711.06	0.54	0.0450	12.00	52+25.00	710.52	12.00	-0.0450	-0.54	709.98
	52+50.00	710.71	0.11	0.0092	12.00	710.60				710.66	0.10	0.0130	7.86	710.56	0.54	0.0450	12.00	52+50.00	710.02	12.00	-0.0450	-0.54	709.48
	52+75.00	710.18	0.06	0.0050	12.00	710.12				710.19	0.13	0.0130	9.82	710.06	0.54	0.0450	12.00	52+75.00	709.52	12.00	-0.0450	-0.54	708.98
	53+00.00	709.66	0.01	0.0008	12.00	709.65				709.72	0.16	0.0130	12.12	709.56	0.54	0.0450	12.00	53+00.00	709.02	12.00	-0.0450	-0.54	708.48
Tr Jnt	53+18.40									709.35	0.16	0.0130	12.00	709.19	0.54	0.0450	12.00	53+18.40	708.65	12.00	-0.0450	-0.54	708.11
	53+25.00	709.10	-0.04	-0.0033	12.00	709.14				709.22	0.16	0.0130	12.00	709.06	0.54	0.0450	12.00	53+25.00	708.52	12.00	-0.0450	-0.54	707.98
	53+50.00	708.52	-0.08	-0.0067	12.00	708.60				708.71	0.15	0.0125	12.00	708.56	0.54	0.0450	12.00	53+50.00	708.02	12.00	-0.0450	-0.54	707.48
	53+75.00	707.90	-0.14	-																			



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																		ROADWAY "A"					
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR	WIDTH	SLOPE	CORR	ELEV
Nor Cr	58+76.72													700.64	-0.17	-0.0142	12.00	58+76.72	700.81	12.00	-0.0156	-0.19	700.62
	59+00.00													700.43	-0.19	-0.0156	12.00	59+00.00	700.62	12.00	-0.0156	-0.19	700.43
Trns Jt	59+14.50													700.32	-0.19	-0.0156	12.00	59+14.50	700.50	12.00	-0.0156	-0.19	700.32
	59+25.00													700.23	-0.19	-0.0156	12.00	59+25.00	700.42	12.00	-0.0156	-0.19	700.23
	59+50.00													700.03	-0.19	-0.0156	12.00	59+50.00	700.22	12.00	-0.0156	-0.19	700.03
	59+75.00													699.83	-0.19	-0.0156	12.00	59+75.00	700.02	12.00	-0.0156	-0.19	699.83
	60+00.00													699.63	-0.19	-0.0156	12.00	60+00.00	699.82	12.00	-0.0156	-0.19	699.63
	60+25.00													699.43	-0.19	-0.0156	12.00	60+25.00	699.62	12.00	-0.0156	-0.19	699.43
Nor Cr	60+32.85													699.37	-0.19	-0.0156	12.00	60+32.85	699.56	12.00	-0.0158	-0.19	699.37
	60+37.50													699.34	-0.19	-0.0156	12.00	60+37.50	699.53	12.00	-0.0146	-0.17	699.35
	60+50.00													699.26	-0.19	-0.0156	12.00	60+50.00	699.45	12.00	-0.0112	-0.13	699.31
	60+75.00													699.15	-0.19	-0.0156	12.00	60+75.00	699.33	12.00	-0.0044	-0.05	699.28
TS	60+91.47													699.10	-0.19	-0.0156	12.00	60+91.47	699.29	12.00	0.0000	0.00	699.29
	61+00.00													699.09	-0.19	-0.0156	12.00	61+00.00	699.28	12.00	0.0023	0.03	699.30
	61+25.00													699.09	-0.19	-0.0156	12.00	61+25.00	699.27	12.00	0.0091	0.11	699.38
	61+50.00													699.14	-0.19	-0.0156	12.00	61+50.00	699.33	12.00	0.0158	0.19	699.52
Rem Cr	61+50.09													699.14	-0.19	-0.0158	12.00	61+50.09	699.33	12.00	0.0158	0.19	699.52
	61+75.00													699.17	-0.27	-0.0226	12.00	61+75.00	699.44	12.00	0.0226	0.27	699.71
	62+00.00													699.26	-0.35	-0.0293	12.00	62+00.00	699.61	12.00	0.0293	0.35	699.96
	62+25.00													699.40	-0.43	-0.0361	12.00	62+25.00	699.83	12.00	0.0361	0.43	700.27
	62+50.00													699.60	-0.51	-0.0428	12.00	62+50.00	700.12	12.00	0.0428	0.51	700.63
	62+75.00													699.86	-0.59	-0.0496	12.00	62+75.00	700.45	12.00	0.0496	0.59	701.05
	63+00.00													700.17	-0.68	-0.0563	12.00	63+00.00	700.85	12.00	0.0563	0.68	701.52
	63+25.00													700.54	-0.76	-0.0631	12.00	63+25.00	701.30	12.00	0.0631	0.76	702.06
	63+50.00													700.97	-0.84	-0.0698	12.00	63+50.00	701.81	12.00	0.0698	0.84	702.65
	63+75.00													701.45	-0.92	-0.0766	12.00	63+75.00	702.37	12.00	0.0767	0.92	703.29
R† ES	63+75.20													701.46	-0.92	-0.0766	12.00	63+75.20	702.38	12.00	0.0767	0.92	703.30
HP Shldr	63+86.60													701.70	-0.95	-0.0792	12.00	63+86.60	702.65	12.00	0.0792	0.95	703.60
R† EP	63+90.30													701.79	-0.96	-0.0800	12.00	63+90.30	702.75	12.00	0.0800	0.96	703.71
	64+00.00													702.02	-0.98	-0.0817	12.00	64+00.00	703.00	12.00	0.0817	0.98	703.98
C/L Lanes	64+12.00													702.32	-0.99	-0.0825	12.00	64+12.00	703.31	12.00	0.0825	0.99	704.30
	64+25.00													702.67	-1.00	-0.0833	12.00	64+25.00	703.67	12.00	0.0833	1.00	704.67
L† EP	64+38.80													703.07	-1.00	-0.0833	12.00	64+38.80	704.07	12.00	0.0833	1.00	705.07
	SC													703.15	-1.00	-0.0833	12.00	64+41.47	704.15	12.00	0.0833	1.00	705.15
CS	64+45.77													703.28	-1.00	-0.0833	12.00	64+45.77	704.28	12.00	0.0833	1.00	705.28
	64+50.00													703.41	-1.00	-0.0833	12.00	64+50.00	704.41	12.00	0.0833	1.00	705.41
L† ES	64+53.00													703.50	-1.00	-0.0833	12.00	64+53.00	704.50	12.00	0.0833	1.00	705.50
	64+75.00													704.25	-0.95	-0.0792	12.00	64+75.00	705.20	12.00	0.0792	0.95	706.15
	65+00.00													705.18	-0.84	-0.0698	12.00	65+00.00	706.02	12.00	0.0698	0.84	706.86
	65+25.00													706.14	-0.70	-0.0586	12.00	65+25.00	706.84	12.00	0.0586	0.70	707.54
	65+37.50													706.61	-0.64	-0.0529	12.00	65+37.50	707.25	12.00	0.0529	0.64	707.89
	65+50.00													707.09	-0.57	-0.0473	12.00	65+50.00	707.66	12.00	0.0473	0.57	708.23
	65+75.00													708.05	-0.43	-0.0360	12.00	65+75.00	708.48	12.00	0.0360	0.43	708.91
	65+95.77													708.84	-0.32	-0.0267	12.00	65+95.77	709.16	12.00	0.0267	0.32	709.48
ST	65+96.87													708.88	-0.31	-0.0262	12.00	65+96.87	709.20	12.00	0.0262	0.31	709.51
TS	66+00.00													709.00	-0.30	-0.0248	12.00	66+00.00	709.30	12.00	0.0248	0.30	709.60
	66+25.00													709.96	-0.16	-0.0135	12.00	66+25.00	710.12	12.00	0.0135	0.16	710.28
	66+33.23													710.27	-0.12	-0.0098	12.00	66+33.23	710.39	12.00	0.0098	0.12	710.51
	66+50.00													710.91	-0.03	-0.0023	12.00	66+50.00	710.94	12.00	0.0023	0.03	710.97
Nose, DJ	66+55.00													711.10	0.00	0.0000	12.00	66+55.00	711.10	12.00	0.0000	0.00	711.10
	66+64.28													711.45	0.04	0.0035	12.00	66+64.28	711.41	12.00	-0.0035	-0.04	711.37
	66+75.00													711.85	0.09	0.0075	12.00	66+75.00	711.76	12.00	-0.0075	-0.09	711.67
	67+00.00													712.78	0.20	0.0169	12.00	67+00.00	712.58	12.00	-0.0169	-0.20	712.38
Tr Jnt	67+25.00													713.72	0.32	0.0263	12.00	67+25.00	713.40	12.00	-0.0263	-0.32	713.08
	67+44.00													714.42	0.40	0.0334	12.00	67+44.00	714.02	12.00	-0.0334	-0.40	713.62
	67+50.00													714.65	0.43	0.0357	12.00	67+50.00	714.22	12.00	-0.0357	-0.43	713.79
	67+62.50													715.11	0.48	0.0404	12.00	67+62.50	714.63	12.00	-0.0404	-0.48	714.15
Shldr, DA	67+75.00													715.58	0.54	0.0450	12.00	67+75.00	715.04	12.00	-0.0450	-0.54	714.50
	67+92.00													716.21	0.62	0.0514	12.00	67+92.00	715.60	12.00	-0.0514	-0.62	714.98
SC	67+96.87													716.40	0.64	0.0533	12.00	67+96.87	715.76	12.00	-0.0533	-0.64	715.12
	68+00.00													716.51	0.65	0.0544	12.00	68+00.00	715.86	12.00	-0.0544	-0.65	715.21

CALCULATED	ESP	CHECKED	TJH
ROADWAY "A" SUPERELEVATION TABLE			
FRA-IR71-14.39			
FRA-315-0.00			
22			
89			

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																		ROADWAY "A"					
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR	WIDTH	SLOPE	CORR	ELEV
	68+25.00													717.39	0.71	0.0592	12.00	68+25.00	716.68	12.00	-0.0592	-0.71	715.97
	68+50.00	ROAD DA - ADD LANE				ROAD DA - DROP LANE				GORE BETWEEN DA AND A				718.18	0.72	0.0600	12.00	68+50.00	717.46	12.00	-0.0600	-0.72	716.74
	68+75.00													718.88	0.72	0.0600	12.00	68+75.00	718.16	12.00	-0.0600	-0.72	717.44
Nose, DA	68+93.26	720.66	0.36	0.0300	12.00	720.30	0.12	0.0300	4.00	720.18	0.83	0.1536	5.43	719.35	0.72	0.0600	12.00	68+93.26	718.63	12.00	-0.0600	-0.72	717.91
	69+00.00	720.70	0.38	0.0320	12.00	720.32	0.13	0.0320	4.00	720.19	0.68	0.1448	4.72	719.51	0.72	0.0600	12.00	69+00.00	718.79	12.00	-0.0600	-0.72	718.07
	69+25.00	720.81	0.38	0.0320	12.00	720.43	0.13	0.0320	4.00	720.30	0.25	0.1036	2.40	720.05	0.72	0.0600	12.00	69+25.00	719.33	12.00	-0.0600	-0.72	718.61
	69+29.80	720.86	0.38	0.0320	12.00	720.48	0.13	0.0320	4.00	720.35	0.20	0.1015	2.00	720.15	0.72	0.0600	12.00	69+29.80	719.43	12.00	-0.0600	-0.72	718.71
	69+50.00	721.06	0.38	0.0320	12.00					720.68	0.18	0.0404	4.51	720.50	0.70	0.0583	12.00	69+50.00	719.80	12.00	-0.0583	-0.70	719.10
CS	69+68.05		0.38	0.0320	12.00						0.00	0.0320		720.77	0.68	0.0567	12.00	69+68.05	720.09	12.00	-0.0567	-0.68	719.41
	69+75.00	721.34	0.38	0.0320	12.00					720.95	0.10	0.0320	3.10	720.86	0.67	0.0558	12.00	69+75.00	720.19	12.00	-0.0558	-0.67	719.52
	69+99.00	721.54	0.38	0.0320	12.00					721.16	0.06	0.0320	2.00	721.09	0.61	0.0507	12.00	69+99.00	720.48	12.00	-0.0558	-0.67	719.81
	70+00.00	721.55	0.45	0.0320	13.97									721.10	0.60	0.0504	12.00	70+00.00	720.50	12.00	-0.0504	-0.60	719.89
	70+10.00	721.23	0.15	0.0106	14.20									721.08	0.44	0.0367	12.00	70+10.00	720.64	12.00	-0.0400	-0.48	720.16
	70+12.50		0.00	0.0320										721.18	0.56	0.0467	12.00	70+12.50	720.62	12.00	-0.0467	-0.56	720.06
	70+25.00	721.68	0.42	0.0320	13.23									721.25	0.51	0.0429	12.00	70+25.00	720.74	12.00	-0.0429	-0.51	720.23
	70+50.00	721.81	0.41	0.0320	12.76									721.40	0.42	0.0354	12.00	70+50.00	720.98	12.00	-0.0354	-0.42	720.56
	70+60.00	721.49	0.05	0.0039	12.80									721.44	0.26	0.0217	12.00	70+60.00	721.18	12.00	-0.0333	-0.40	720.78
	70+75.00	721.90	0.35	0.0279	12.49									721.55	0.33	0.0279	12.00	70+75.00	721.22	12.00	-0.0279	-0.33	720.89
	71+00.00	721.95	0.25	0.0204	12.23									721.70	0.24	0.0204	12.00	71+00.00	721.46	12.00	-0.0204	-0.24	721.22
	71+09.00	721.71	-0.04	-0.0033	12.00									721.75	0.12	0.0100	12.00	71+09.00	721.63	12.00	-0.0283	-0.34	721.29
Rem Cr	71+16.05		0.00	0.0146										721.80	0.19	0.0156	12.00	71+16.05	721.61	12.00	-0.0156	-0.19	721.43
	71+25.00	722.02	0.14	0.0114	12.06									721.89	0.19	0.0156	12.00	71+25.00	721.70	12.00	-0.0156	-0.19	721.51
	71+50.00	722.16	0.03	0.0024	12.01									722.13	0.19	0.0156	12.00	71+50.00	721.94	12.00	-0.0156	-0.19	721.75
	71+61.10	722.13	-0.08	-0.0067	12.00									722.21	0.11	0.0092	12.00	71+61.10	722.10	12.00	-0.0217	-0.26	721.84
ST	71+68.05	722.25	-0.05	-0.0041	12.00									722.30	0.19	0.0156	12.00	71+68.05	722.11	12.00	-0.0156	-0.19	721.93
	71+75.00	722.29	-0.08	-0.0066	12.00									722.37	0.19	0.0156	12.00	71+75.00	722.18	12.00	-0.0156	-0.19	721.99
	72+00.00	722.42	-0.19	-0.0156	12.00									722.61	0.19	0.0156	12.00	72+00.00	722.42	12.00	-0.0156	-0.19	722.23
App SI	72+00.40	722.45	-0.18	-0.0150	12.00									722.63	0.20	0.0167	12.00	72+00.40	722.43	12.00	-0.0150	-0.18	722.25
Bridge	72+26.80	722.58	-0.15	-0.0125	12.00									722.73	0.15	0.0125	12.00	72+26.80	722.58	12.00	-0.0156	-0.19	722.39



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				ROADWAY "A"																				
LEFT SIDE				STATION	PROF GR	RIGHT SIDE				WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	PT DESCR	STATION	
ELEV	CORR	SLOPE	WIDTH			WIDTH	SLOPE	CORR	ELEV															
				46+00.00	720.50																		Begin	46+00.00
				46+25.00	720.40																			46+25.00
720.63	0.10	0.0083	12.00	46+30.00	720.53	12.00	-0.0142	-0.17	720.36															46+30.00
720.51	0.13	0.0108	12.00	46+50.00	720.38	12.00	-0.0156	-0.19	720.19															46+50.00
720.31	0.16	0.0133	12.00	46+75.00	720.15	12.00	-0.0156	-0.19	719.96															46+75.00
720.09	0.19	0.0156	12.00	47+00.00	719.90	12.00	-0.0156	-0.19	719.71															47+00.00
719.85	0.19	0.0156	12.00	47+25.00	719.66	12.00	-0.0156	-0.19	719.48															47+25.00
719.58	0.19	0.0156	12.00	47+50.00	719.39	12.00	-0.0156	-0.19	719.20															47+50.00
719.27	0.19	0.0156	12.00	47+75.00	719.08	12.00	-0.0156	-0.19	718.90															47+75.00
718.93	0.19	0.0156	12.00	48+00.00	718.74	12.00	-0.0156	-0.19	718.55															48+00.00
718.55	0.19	0.0156	12.00	48+25.00	718.36	12.00	-0.0156	-0.19	718.18															48+25.00
718.14	0.19	0.0156	12.00	48+50.00	717.95	12.00	-0.0156	-0.19	717.76															48+50.00
717.69	0.19	0.0156	12.00	48+75.00	717.50	12.00	-0.0156	-0.19	717.32															48+75.00
717.22	0.20	0.0167	12.00	49+00.00	717.02	12.00	-0.0167	-0.20	716.82															49+00.00
717.12	0.20	0.0167	12.00	49+05.22	716.92	12.00	-0.0167	-0.20	716.72													TS	49+05.22	
716.73	0.21	0.0175	12.00	49+25.00	716.52	12.00	-0.0175	-0.21	716.31															49+25.00
716.26	0.24	0.0200	12.00	49+50.00	716.02	12.00	-0.0200	-0.24	715.78															49+50.00
715.79	0.27	0.0224	12.00	49+75.00	715.52	12.00	-0.0224	-0.27	715.25															49+75.00
715.32	0.30	0.0249	12.00	50+00.00	715.02	12.00	-0.0249	-0.30	714.72															50+00.00
714.85	0.33	0.0273	12.00	50+25.00	714.52	12.00	-0.0273	-0.33	714.19															50+25.00
714.38	0.36	0.0298	12.00	50+50.00	714.02	12.00	-0.0298	-0.36	713.66															50+50.00
713.91	0.39	0.0322	12.00	50+75.00	713.52	12.00	-0.0322	-0.39	713.13															50+75.00
713.44	0.42	0.0347	12.00	51+00.00	713.02	12.00	-0.0347	-0.42	712.60															51+00.00
712.97	0.45	0.0371	12.00	51+25.00	712.52	12.00	-0.0371	-0.45	712.07															51+25.00
712.50	0.48	0.0396	12.00	51+50.00	712.02	12.00	-0.0396	-0.48	711.54															51+50.00
712.02	0.50	0.0420	12.00	51+75.00	711.52	12.00	-0.0420	-0.50	711.02															51+75.00
711.55	0.53	0.0445	12.00	52+00.00	711.02	12.00	-0.0445	-0.53	710.49															52+00.00
711.45	0.53	0.0442	12.00	52+05.22	710.92	12.00	-0.0442	-0.53	710.39													SC	52+05.22	
711.06	0.54	0.0450	12.00	52+25.00	710.52	12.00	-0.0450	-0.54	709.98															52+25.00
710.56	0.54	0.0450	12.00	52+50.00	710.02	12.00	-0.0450	-0.54	709.48															52+50.00
710.06	0.54	0.0450	12.00	52+75.00	709.52	12.00	-0.0450	-0.54	708.98															52+75.00
709.56	0.54	0.0450	12.00	53+00.00	709.02	12.00	-0.0450	-0.54	708.48															53+00.00
709.19	0.54	0.0450	12.00	53+18.40	708.65	12.00	-0.0450	-0.54	708.11													Tr Jnt	53+18.40	
709.06	0.54	0.0450	12.00	53+25.00	708.52	12.00	-0.0450	-0.54	707.98															53+25.00
708.56	0.54	0.0450	12.00	53+50.00	708.02	12.00	-0.0450	-0.54	707.48															53+50.00
708.06	0.54	0.0450	12.00	53+75.00	707.52	12.00	-0.0450	-0.54	706.98															53+75.00
707.56	0.54	0.0450	12.00	54+00.00	707.02	12.00	-0.0450	-0.54	706.48															54+00.00
707.38	0.54	0.0450	12.00	54+08.77	706.84	12.00	-0.0450	-0.54	706.30													Nose, AD	54+08.77	
707.38	0.54	0.0450	12.00	54+08.77	706.84	12.00	-0.0450	-0.54	706.30													Nose, AD	54+08.77	
707.06	0.54	0.0450	12.00	54+25.00	706.52	12.00	-0.0450	-0.54	705.98															54+25.00
706.56	0.54	0.0450	12.00	54+50.00	706.02	12.00	-0.0450	-0.54	705.48															54+50.00
706.10	0.53	0.0442	12.00	54+72.72	705.57	12.00	-0.0442	-0.53	705.04															



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				ROADWAY "A"																			
LEFT SIDE				STATION	PROF GR	RIGHT SIDE				WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	PT DESCR	STATION
ELEV	CORR	SLOPE	WIDTH			WIDTH	SLOPE	CORR	ELEV														
								RAMP CA TERMINAL AREA				RAMP CA TERMINAL AREA											
700.64	-0.17	-0.0142	12.00	58+76.72	700.81	12.00	-0.0156	-0.19	700.62	4.38	-0.0156	-0.07	700.55	12.00	-0.0156	-0.19	700.36					Nor Cr	58+76.72
700.43	-0.19	-0.0156	12.00	59+00.00	700.62	12.00	-0.0156	-0.19	700.43	3.41	-0.0156	-0.05	700.38	12.00	-0.0156	-0.19	700.19						59+00.00
700.32	-0.19	-0.0156	12.00	59+14.50	700.50	12.00	-0.0156	-0.19	700.32	2.81	-0.0156	-0.04	700.27	12.00	-0.0156	-0.19	700.09					Trns Jt	59+14.50
700.23	-0.19	-0.0156	12.00	59+25.00	700.42	12.00	-0.0156	-0.19	700.23	2.38	-0.0156	-0.04	700.20	12.00	-0.0156	-0.19	700.01						59+25.00
700.03	-0.19	-0.0156	12.00	59+50.00	700.22	12.00	-0.0156	-0.19	700.03	13.35	-0.0156	-0.21	699.82										59+50.00
699.83	-0.19	-0.0156	12.00	59+75.00	700.02	12.00	-0.0156	-0.19	699.83	12.31	-0.0156	-0.19	699.64										59+75.00
699.63	-0.19	-0.0156	12.00	60+00.00	699.82	12.00	-0.0156	-0.19	699.63	11.28	-0.0156	-0.18	699.46										60+00.00
699.43	-0.19	-0.0156	12.00	60+25.00	699.62	12.00	-0.0156	-0.19	699.43	10.25	-0.0156	-0.16	699.27										60+25.00
699.37	-0.19	-0.0156	12.00	60+32.85	699.56	12.00	-0.0158	-0.19	699.37	9.92	-0.0158	-0.16	699.21									Nor Cr	60+32.85
699.34	-0.19	-0.0156	12.00	60+37.50	699.53	12.00	-0.0146	-0.17	699.35	9.73	-0.0146	-0.14	699.21										60+37.50
699.26	-0.19	-0.0156	12.00	60+50.00	699.45	12.00	-0.0112	-0.13	699.31	9.21	-0.0112	-0.10	699.21										60+50.00
699.15	-0.19	-0.0156	12.00	60+75.00	699.33	12.00	-0.0044	-0.05	699.28	8.18	-0.0044	-0.04	699.24										60+75.00
699.10	-0.19	-0.0156	12.00	60+91.47	699.29	12.00	0.0000	0.00	699.29	7.50	0.0000	0.00	699.29									TS	60+91.47
699.09	-0.19	-0.0156	12.00	61+00.00	699.28	12.00	0.0023	0.03	699.30	7.15	0.0023	0.02	699.32										61+00.00
699.09	-0.19	-0.0156	12.00	61+25.00	699.27	12.00	0.0091	0.11	699.38	6.12	0.0091	0.06	699.44										61+25.00
699.14	-0.19	-0.0156	12.00	61+50.00	699.33	12.00	0.0158	0.19	699.52	5.08	0.0158	0.08	699.60										61+50.00
699.14	-0.19	-0.0158	12.00	61+50.09	699.33	12.00	0.0158	0.19	699.52	5.08	0.0158	0.08	699.60									Rem Cr	61+50.09
699.17	-0.27	-0.0226	12.00	61+75.00	699.44	12.00	0.0226	0.27	699.71	4.05	0.0226	0.09	699.80										61+75.00
699.26	-0.35	-0.0293	12.00	62+00.00	699.61	12.00	0.0293	0.35	699.96	3.02	0.0293	0.09	700.05										62+00.00
699.40	-0.43	-0.0361	12.00	62+25.00	699.83	12.00	0.0361	0.43	700.27	1.98	0.0361	0.07	700.34										62+25.00
699.60	-0.51	-0.0428	12.00	62+50.00	700.12	12.00	0.0428	0.51	700.63	0.95	0.0428	0.04	700.67										62+50.00
699.86	-0.59	-0.0496	12.00	62+75.00	700.45	12.00	0.0496	0.59	701.05														62+75.00
700.17	-0.68	-0.0563	12.00	63+00.00	700.85	12.00	0.0563	0.68	701.52														63+00.00
700.54	-0.76	-0.0631	12.00	63+25.00	701.30	12.00	0.0631	0.76	702.06														63+25.00
700.97	-0.84	-0.0698	12.00	63+50.00	701.81	12.00	0.0698	0.84	702.65														63+50.00
701.45	-0.92	-0.0766	12.00	63+75.00	702.37	12.00	0.0767	0.92	703.29														63+75.00
701.46	-0.92	-0.0766	12.00	63+75.20	702.38	12.00	0.0767	0.92	703.30													Rt ES	63+75.20
701.70	-0.95	-0.0792	12.00	63+86.60	702.65	12.00	0.0792	0.95	703.60													HP Shldr	63+86.60
701.79	-0.96	-0.0800	12.00	63+90.30	702.75	12.00	0.0800	0.96	703.71													Rt EP	63+90.30
702.02	-0.98	-0.0817	12.00	64+00.00	703.00	12.00	0.0817	0.98	703.98														64+00.00
702.32	-0.99	-0.0825	12.00	64+12.00	703.31	12.00	0.0825	0.99	704.30													C/L Lanes	64+12.00
702.67	-1.00	-0.0833	12.00	64+25.00	703.67	12.00	0.0833	1.00	704.67														64+25.00
703.07	-1.00	-0.0833	12.00	64+38.80	704.07	12.00	0.0833	1.00	705.07													Lt EP	64+38.80
703.15	-1.00	-0.0833	12.00	64+41.47	704.15	12.00	0.0833	1.00	705.15													SC	64+41.47
703.28	-1.00	-0.0833	12.00	64+45.77	704.28	12.00	0.0833	1.00	705.28													CS	64+45.77
703.41	-1.00	-0.0833	12.00	64+50.00	704.41	12.00	0.0833	1.00	705.41														64+50.00
703.50	-1.00	-0.0833	12.00	64+53.00	704.50	12.00	0.0833	1.00	705.50													Lt ES	64+53.00
704.25	-0.95	-0.0792	12.00	64+75.00	705.20	12.00	0.0792	0.95	706.15														64+75.00
705.18	-0.84	-0.0698	12.00	65+00.00	706.02	12.00	0.0698	0.84	706.86														65+00.00
706.14	-0.70	-0.0586	12.00	65+25.00	706.84	12.00	0.0586	0.70	707.54									</					

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				ROADWAY "A"																			
LEFT SIDE				STATION	PROF GR	RIGHT SIDE				WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV	PT DESCR	STATION
ELEV	CORR	SLOPE	WIDTH			WIDTH	SLOPE	CORR	ELEV														
														ROAD DJ-ADD LANE				ROAD DJ ADD LANE					
717.39	0.71	0.0592	12.00	68+25.00	716.68	12.00	-0.0592	-0.71	715.97					2.72	-0.0600	-0.16	715.81	12.00	-0.0600	-0.72	715.09		68+25.00
718.18	0.72	0.0600	12.00	68+50.00	717.46	12.00	-0.0600	-0.72	716.74									13.65	-0.0600	-0.82	715.92		68+50.00
718.88	0.72	0.0600	12.00	68+75.00	718.16	12.00	-0.0600	-0.72	717.44									12.88	-0.0600	-0.77	716.67		68+75.00
719.35	0.72	0.0600	12.00	68+93.26	718.63	12.00	-0.0600	-0.72	717.91													Nose, DA	68+93.26
719.51	0.72	0.0600	12.00	69+00.00	718.79	12.00	-0.0600	-0.72	718.07									12.41	-0.0600	-0.74	717.32		69+00.00
720.05	0.72	0.0600	12.00	69+25.00	719.33	12.00	-0.0600	-0.72	718.61									12.17	-0.0600	-0.73	717.88		69+25.00
720.15	0.72	0.0600	12.00	69+29.80	719.43	12.00	-0.0600	-0.72	718.71									12.17	-0.0600	-0.73	717.98		69+29.80
720.50	0.70	0.0583	12.00	69+50.00	719.80	12.00	-0.0583	-0.70	719.10									12.03	-0.0583	-0.70	718.40		69+50.00
720.77	0.68	0.0567	12.00	69+68.05	720.09	12.00	-0.0567	-0.68	719.41													CS	69+68.05
720.86	0.67	0.0558	12.00	69+75.00	720.19	12.00	-0.0558	-0.67	719.52									12.00	-0.0558	-0.67	718.85		69+75.00
721.09	0.61	0.0507	12.00	69+99.00	720.48	12.00	-0.0558	-0.67	719.81									12.00	-0.0558	-0.67	719.14		69+99.00
721.10	0.60	0.0504	12.00	70+00.00	720.50	12.00	-0.0504	-0.60	719.89									12.00	-0.0504	-0.60	719.29		70+00.00
721.08	0.44	0.0367	12.00	70+10.00	720.64	12.00	-0.0400	-0.48	720.16									12.00	-0.0517	-0.62	719.54		70+10.00
721.18	0.56	0.0467	12.00	70+12.50	720.62	12.00	-0.0467	-0.56	720.06									12.00	-0.0467	-0.56	719.50		70+12.50
721.25	0.51	0.0429	12.00	70+25.00	720.74	12.00	-0.0429	-0.51	720.23									12.00	-0.0429	-0.51	719.71		70+25.00
721.40	0.42	0.0354	12.00	70+50.00	720.98	12.00	-0.0354	-0.42	720.56									12.00	-0.0354	-0.42	720.13		70+50.00
721.44	0.26	0.0217	12.00	70+60.00	721.18	12.00	-0.0333	-0.40	720.78									12.00	-0.0367	-0.44	720.34		70+60.00
721.55	0.33	0.0279	12.00	70+75.00	721.22	12.00	-0.0279	-0.33	720.89									12.00	-0.0279	-0.33	720.55		70+75.00
721.70	0.24	0.0204	12.00	71+00.00	721.46	12.00	-0.0204	-0.24	721.22									12.00	-0.0204	-0.24	720.97		71+00.00
721.75	0.12	0.0100	12.00	71+09.00	721.63	12.00	-0.0283	-0.34	721.29									12.00	-0.0267	-0.32	720.97		71+09.00
721.80	0.19	0.0156	12.00	71+16.05	721.61	12.00	-0.0156	-0.19	721.43									12.00	-0.0156	-0.19	721.24	Rem Cr	71+16.05
721.89	0.19	0.0156	12.00	71+25.00	721.70	12.00	-0.0156	-0.19	721.51									12.00	-0.0156	-0.19	721.33		71+25.00
722.13	0.19	0.0156	12.00	71+50.00	721.94	12.00	-0.0156	-0.19	721.75									12.00	-0.0156	-0.19	721.57		71+50.00
722.21	0.11	0.0092	12.00	71+61.10	722.10	12.00	-0.0217	-0.26	721.84									12.00	-0.0208	-0.25	721.59		71+61.10
722.30	0.19	0.0156	12.00	71+68.05	722.11	12.00	-0.0156	-0.19	721.93									12.00	-0.0183	-0.22	721.71	ST	71+68.05
722.37	0.19	0.0156	12.00	71+75.00	722.18	12.00	-0.0156	-0.19	721.99									12.00	-0.0192	-0.23	721.76		71+75.00
722.61	0.19	0.0156	12.00	72+00.00	722.42	12.00	-0.0156	-0.19	722.23									12.00	-0.0225	-0.27	721.96		72+00.00
722.63	0.20	0.0167	12.00	72+00.40	722.43	12.00	-0.0150	-0.18	722.25									12.00	-0.0225	-0.27	721.98	App Sl	72+00.40
722.73	0.15	0.0125	12.00	72+26.80	722.58	12.00	-0.0156	-0.19	722.39									12.00	-0.0267	-0.32	722.07	Bridge	72+26.80

CALCULATED  
ESP

CHECKED  
TJH

ROADWAY "A" SUPERELEVATION TABLE

FRA-IR71-14.39  
FRA-315-0.00

24B  
89



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ROADWAY "B"		ROAD BD-ADD LANE				GORE BETWEEN BD & B				ROADWAY B-2 LANES						GORE BETWEEN B & BC				ROAD BC-ADD LANE			
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR.	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV
	44+00.00									717.34	-0.37	-0.0156	24.00	44+00.00	717.54								
	44+25.00									717.27	-0.37	-0.0156	24.00	44+25.00	717.52								
	44+50.00									717.19	-0.37	-0.0156	24.00	44+50.00	717.51								
	44+75.00									717.12	-0.37	-0.0156	24.00	44+75.00	717.49								
	45+00.00									717.04	-0.37	-0.0156	24.00	45+00.00	717.41								
	45+25.00									716.96	-0.37	-0.0156	24.00	45+25.00	717.33								
	45+50.00									716.88	-0.37	-0.0156	24.00	45+50.00	717.25								
	45+75.00									716.80	-0.37	-0.0156	24.00	45+75.00	717.17								
PVC	45+81.00									716.77	-0.37	-0.0156	24.00	45+81.00	717.14								
Norm	46+00.00									716.70	-0.37	-0.0156	24.00	46+00.00	717.07								
	46+25.00									716.59	-0.37	-0.0156	24.00	46+25.00	716.97								
Begin	46+44.59									716.49	-0.37	-0.0156	24.00	46+44.59	716.87								
(match ex)	46+50.00									716.46	-0.38	-0.0158	24.00	46+50.00	716.84								
	46+75.00									716.28	-0.41	-0.0171	24.00	46+75.00	716.69								
PC	46+76.00									716.27	-0.41	-0.0172	24.00	46+76.00	716.68								
	47+00.00									716.07	-0.45	-0.0188	24.00	47+00.00	716.52								
	47+25.00									715.84	-0.49	-0.0204	24.00	47+25.00	716.33								
	47+50.00									715.58	-0.53	-0.0221	24.00	47+50.00	716.11								
	47+75.00									715.31	-0.57	-0.0238	24.00	47+75.00	715.88								
	48+00.00									715.01	-0.61	-0.0254	24.00	48+00.00	715.62								
Full	48+25.00									714.71	-0.64	-0.0267	24.00	48+25.00	715.35								
	48+50.00									714.40	-0.65	-0.0270	24.00	48+50.00	715.05								
	48+75.00									714.08	-0.65	-0.0270	24.00	48+75.00	714.73								
	49+00.00									713.74	-0.65	-0.0270	24.00	49+00.00	714.39								
	49+25.00									713.38	-0.65	-0.0270	24.00	49+25.00	714.03								
	49+50.00									713.00	-0.65	-0.0270	24.00	49+50.00	713.64								
	49+75.00									712.59	-0.65	-0.0270	24.00	49+75.00	713.24								
PVT	49+81.00									712.49	-0.65	-0.0270	24.00	49+81.00	713.14								
	50+00.00									712.17	-0.65	-0.0270	24.00	50+00.00	712.82								
	50+25.00									711.75	-0.65	-0.0270	24.00	50+25.00	712.40								
	50+50.00									711.33	-0.65	-0.0270	24.00	50+50.00	711.98								
	50+75.00									710.91	-0.65	-0.0270	24.00	50+75.00	711.56								
	51+00.00									710.49	-0.65	-0.0270	24.00	51+00.00	711.14								
	51+25.00									710.07	-0.65	-0.0270	24.00	51+25.00	710.72								
	51+50.00									709.68	-0.62	-0.0258	24.00	51+50.00	710.30								
	51+51.51									709.66	-0.62	-0.0258	24.00	51+51.51	710.28								
	51+75.00									709.33	-0.55	-0.0230	24.00	51+75.00	709.88								
	52+00.00									709.01	-0.45	-0.0188	24.00	52+00.00	709.46								
Norm	52+18.57									708.77	-0.37	-0.0156	24.00	52+18.57	709.15								
	52+25.00									708.69	-0.35	-0.0145	24.00	52+25.00	709.04								
PT	52+40.00									708.50	-0.29	-0.0120	24.00	52+40.00	708.79								
	52+50.00									708.37	-0.25	-0.0103	24.00	52+50.00	708.62								
	52+75.00									708.06	-0.14	-0.0060	24.00	52+75.00	708.20								
	53+00.00									707.74	-0.04	-0.0018	24.00	53+00.00	707.78								
TS	53+10.33									707.61	0.00	0.0000	24.00	53+10.33	707.61								
	53+25.00									707.42	0.06	0.0025	24.00	53+25.00	707.36								
	53+50.00									707.10	0.16	0.0067	24.00	53+50.00	706.94								
	53+75.00									706.78	0.26	0.0110	24.00	53+75.00	706.52								
	54+00.00									706.47	0.37	0.0152	24.00	54+00.00	706.10								
	54+25.00									706.15	0.47	0.0195	24.00	54+25.00	705.68								
	54+50.00									705.83	0.57	0.0237	24.00	54+50.00	705.26								
	54+75.00									705.51	0.67	0.0280	24.00	54+75.00	704.84								
	55+00.00									705.19	0.77	0.0322	24.00	55+00.00	704.42								
	55+25.00									704.88	0.88	0.0365	24.00	55+25.00	704.00								
	55+50.00									704.56	0.98	0.0407	24.00	55+50.00	703.58								
	55+75.00									704.24	1.08	0.0450	24.00	55+75.00	703.16								
	56+00.00									703.89	1.15	0.0479	24.00	56+00.00	702.74								
SC	56+10.33									703.75	1.18	0.0492	24.00	56+10.33	702.57								
	56+25.00									703.52	1.20	0.0500	24.00	56+25.00	702.32								
	56+50.00									703.12	1.22	0.0510	24.00	56+50.00	701.90								
	56+75.00									702.70	1.22	0.0510	24.00	56+75.00	701.48								
	56+87.50									702.50	1.22	0.0510	24.00	56+87.50	701.28								

CALCULATED  
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ROADWAY "B" SUPERELEVATION TABLE

FRA-IR71-14.39  
FRA-315-0.00

240  
89



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ROADWAY "B"		ROAD BD-ADD LANE				GORE BETWEEN BD & B				ROADWAY B-2 LANES						GORE BETWEEN B & BC				ROAD BC-ADD LANE			
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR.	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV
	57+00.00									702.31	1.22	0.0510	24.00	57+00.00	701.09								
	57+25.00									701.99	1.22	0.0510	24.00	57+25.00	700.76								
	57+50.00									701.72	1.22	0.0510	24.00	57+50.00	700.49								
	57+75.00									701.51	1.22	0.0510	24.00	57+75.00	700.28								
	58+00.00									701.36	1.22	0.0510	24.00	58+00.00	700.14								
	58+25.00									701.27	1.22	0.0510	24.00	58+25.00	700.05								
	58+50.00									701.25	1.22	0.0510	24.00	58+50.00	700.02								
	58+75.00									701.28	1.22	0.0510	24.00	58+75.00	700.06								
	59+00.00									701.37	1.22	0.0510	24.00	59+00.00	700.15								
	59+25.00									701.53	1.22	0.0510	24.00	59+25.00	700.31								
R† ES	59+28.20									701.55	1.22	0.0510	24.00	59+28.20	700.33								
R† EP	59+37.80									701.63	1.22	0.0510	24.00	59+37.80	700.41								
	59+50.00									701.74	1.22	0.0510	24.00	59+50.00	700.52								
C/L Lanes	59+55.90									701.80	1.22	0.0510	24.00	59+55.90	700.58								
L† EP	59+72.60									701.99	1.22	0.0510	24.00	59+72.60	700.77								
	59+75.00									702.02	1.22	0.0510	24.00	59+75.00	700.80								
HP Shldr	59+76.20									702.03	1.22	0.0510	24.00	59+76.20	700.81								
L† ES	59+85.70									702.16	1.22	0.0510	24.00	59+85.70	700.93								
	60+00.00									702.36	1.22	0.0510	24.00	60+00.00	701.13								
	60+25.00									702.75	1.22	0.0510	24.00	60+25.00	701.53								
	60+50.00									703.21	1.22	0.0510	24.00	60+50.00	701.99								
	60+75.00									703.73	1.22	0.0510	24.00	60+75.00	702.50								
	61+00.00									704.31	1.22	0.0510	24.00	61+00.00	703.08								
	61+25.00									704.95	1.22	0.0510	24.00	61+25.00	703.72								
	61+50.00									705.64	1.22	0.0510	24.00	61+50.00	704.42								
	61+75.00									706.40	1.22	0.0510	24.00	61+75.00	705.18								
	62+00.00									707.19	1.22	0.0510	24.00	62+00.00	705.97								
	62+12.50									707.59	1.22	0.0510	24.00	62+12.50	706.37								
	62+25.00									707.98	1.22	0.0510	24.00	62+25.00	706.76								
CS	62+50.00									708.74	1.19	0.0496	24.00	62+50.00	707.55								
	62+54.21									708.86	1.18	0.0492	24.00	62+54.21	707.68								
	62+75.00									709.47	1.13	0.0471	24.00	62+75.00	708.34								
	63+00.00									710.17	1.04	0.0432	24.00	63+00.00	709.13								
	63+25.00									710.86	0.94	0.0390	24.00	63+25.00	709.92								
	63+50.00									711.54	0.83	0.0347	24.00	63+50.00	710.71								
	63+75.00									712.23	0.73	0.0305	24.00	63+75.00	711.50								
	64+00.00									712.92	0.63	0.0262	24.00	64+00.00	712.29								
	64+25.00									713.61	0.53	0.0220	24.00	64+25.00	713.08								
	64+50.00									714.30	0.43	0.0177	24.00	64+50.00	713.87								
	64+75.00									714.98	0.32	0.0135	24.00	64+75.00	714.66								
	64+87.50									715.33	0.27	0.0113	24.00	64+87.50	715.06								
	65+00.00									715.67	0.22	0.0092	24.00	65+00.00	715.45								
	65+12.50									716.02	0.17	0.0071	24.00	65+12.50	715.85								
	65+25.00									716.36	0.12	0.0050	24.00	65+25.00	716.24								
ST	65+50.00									716.98	0.02	0.0007	24.00	65+50.00	716.96								
	65+54.21									717.07	0.00	0.0000	24.00	65+54.21	717.07								
	65+75.00									717.53	-0.08	-0.0035	24.00	65+75.00	717.61								
	66+00.00									718.01	-0.19	-0.0078	24.00	66+00.00	718.20								
	66+25.00									718.44	-0.28	-0.0117	24.00	66+25.00	718.72								
	66+45.97									718.76	-0.34	-0.0142	24.00	66+45.97	719.10								
	66+50.00									718.83	-0.34	-0.0142	24.00	66+50.00	719.17	27.24							
Shldr, BC	66+52.69									718.84	-0.37	-0.0156	24.00	66+52.69	719.21	26.68	0.0367	0.98	720.19	16.00	-0.0338	-0.54	719.65
Shldr, BD	66+54.81	718.46	-0.96	-0.0600	16.00	719.42	0.55	0.0190	28.85	718.87	-0.37	-0.0156	24.00	66+54.81	719.25								
	66+75.00	718.50	-0.90	-0.0560	16.00	719.40	0.23	0.0100	22.66	719.17	-0.37	-0.0156	24.00	66+75.00	719.55	22.86	0.0300	0.69	720.23	16.00	-0.0400	-0.64	719.59
Nose, BD	66+84.71				16.00					719.30	-0.37	-0.0156	24.00	66+84.71	719.68					16.00			
Nose, BC	66+94.24				16.00					719.42	-0.37	-0.0156	24.00	66+94.24	719.80					16.00			
	67+00.00	718.70	-0.78	-0.0490	16.00	719.49	0.00	0.0000	15.94	719.49	-0.37	-0.0156	24.00	67+00.00	719.86	19.23	0.0200	0.38	720.25	16.00	-0.0400	-0.64	719.61
	67+25.00	718.96	-0.67	-0.0420	16.00	719.63	-0.10	-0.0100	10.34	719.73	-0.37	-0.0156	24.00	67+25.00	720.11	16.15	0.0100	0.16	720.27	16.00	-0.0400	-0.64	719.63
	67+50.00	719.23	-0.56	-0.0350	16.00	719.79	-0.12	-0.0200	5.89	719.91	-0.37	-0.0156	24.00	67+50.00	720.29	13.53	0.0000	0.00	720.29	16.00	-0.0400	-0.64	719.65
	67+62.50				16.00					719.98	-0.37	-0.0156	24.00	67+62.50	720.35					16.00			
	67+75.00	719.51	-0.45	-0.0280	16.00	719.96	-0.07	-0.0280	2.45	720.03	-0.37	-0.0156	24.00	67+75.00	720.41	11.19	-0.0100	-0.11	720.29	16.00	-0.0360	-0.58	719.72
	68+00.00	719.81	-0.34	-0.0210	15.98					720.14	-0.37	-0.0156	24.00	68+00.00	720.52	9.05	-0.0156	-0.14	720.37	16.00	-0.0310	-0.50	719.88

ROADWAY "B" SUPERELEVATION TABLE

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FRA-315-0.00

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ROADWAY "B"		ROAD BD-ADD LANE				GORE BETWEEN BD & B				ROADWAY B-2 LANES						GORE BETWEEN B & BC				ROAD BC-ADD LANE			
PT DESCR	STATION	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	ELEV	CORR	SLOPE	WIDTH	STATION	PROF GR.	WIDTH	SLOPE	CORR	ELEV	WIDTH	SLOPE	CORR	ELEV
	68+25.00	720.03	-0.22	-0.0156	14.17					720.25	-0.37	-0.0156	24.00	68+25.00	720.63	7.06	-0.0156	-0.11	720.51	16.00	-0.0250	-0.40	720.11
	68+50.00	720.16	-0.20	-0.0156	13.03					720.36	-0.37	-0.0156	24.00	68+50.00	720.74	5.23	-0.0200	-0.10	720.63	16.00	-0.0200	-0.32	720.31
	68+75.00	720.28	-0.19	-0.0156	12.37					720.47	-0.37	-0.0156	24.00	68+75.00	720.85	3.58	-0.0156	-0.06	720.79	16.00	-0.0156	-0.25	720.54
	69+00.00	720.39	-0.19	-0.0156	12.08					720.58	-0.37	-0.0156	24.00	69+00.00	720.96	2.10	-0.0156	-0.03	720.92	16.00	-0.0156	-0.25	720.67
TS, BD	69+25.00	720.50	-0.19	-0.0156	12.00					720.69	-0.37	-0.0156	24.00	69+25.00	721.07					16.79	-0.0156	-0.26	720.80
	69+38.66	720.56	-0.19	-0.0156	12.00					720.75	-0.37	-0.0156	24.00	69+38.66	721.13								
	69+50.00	720.61	-0.19	-0.0156	12.00					720.80	-0.37	-0.0156	24.00	69+50.00	721.18					15.62	-0.0156	-0.24	720.93
	69+75.00	720.72	-0.19	-0.0156	12.00					720.91	-0.37	-0.0156	24.00	69+75.00	721.29					14.61	-0.0156	-0.23	721.06
	70+00.00	720.83	-0.19	-0.0156	12.00					721.02	-0.37	-0.0156	24.00	70+00.00	721.40					13.77	-0.0156	-0.21	721.18
	70+25.00	720.95	-0.19	-0.0156	12.00					721.14	-0.37	-0.0156	24.00	70+25.00	721.52					13.09	-0.0156	-0.20	721.31
	70+50.00	721.07	-0.19	-0.0156	12.00					721.26	-0.37	-0.0156	24.00	70+50.00	721.63					12.58	-0.0156	-0.20	721.43
	70+54.20		-0.19	-0.0156	12.00					721.09	-0.50	-0.0208	24.00	70+54.20	721.59								
	70+75.00	721.18	-0.19	-0.0156	12.00					721.37	-0.37	-0.0156	24.00	70+75.00	721.75					12.22	-0.0156	-0.19	721.55
	71+00.00	721.30	-0.19	-0.0156	12.00					721.49	-0.37	-0.0156	24.00	71+00.00	721.86					12.03	-0.0156	-0.19	721.67
	71+00.30	721.23	-0.16	-0.0133	12.00					721.39	-0.30	-0.0125	24.00	71+00.30	721.69					12.00	-0.0108	-0.13	721.56
	71+25.00	721.41	-0.19	-0.0156	12.00					721.60	-0.37	-0.0156	24.00	71+25.00	721.98					12.00	-0.0156	-0.19	721.79
	71+50.00	721.53	-0.19	-0.0156	12.00					721.72	-0.37	-0.0156	24.00	71+50.00	722.09					12.00	-0.0156	-0.19	721.90
	71+53.70	721.45	-0.21	-0.0175	12.00					721.66	-0.16	-0.0067	24.00	71+53.70	721.82					12.00	-0.0008	-0.01	721.81
	71+75.00	721.64	-0.19	-0.0156	12.00					721.83	-0.37	-0.0156	24.00	71+75.00	722.21					12.00	-0.0156	-0.19	722.02
	72+00.00	721.68	-0.27	-0.0221	12.00					721.95	-0.37	-0.0156	24.00	72+00.00	722.32					12.00	-0.0120	-0.14	722.18
	72+01.20	721.82	-0.15	-0.0125	12.00					721.97	-0.22	-0.0092	24.00	72+01.20	722.19					12.00	-0.0042	-0.05	722.14
POT	72+07.89	721.71	-0.29	-0.0242	12.00					722.00	-0.36	-0.0150	24.00	72+07.89	722.36					12.00	-0.0111	-0.13	722.22
	72+25.00	721.75	-0.34	-0.0286	12.00					722.10	-0.34	-0.0142	24.00	72+25.00	722.44					12.00	-0.0090	-0.11	722.33
	72+36.90	721.78	-0.38	-0.0317	12.00					722.16	-0.33	-0.0138	24.00	72+36.90	722.49					12.00	-0.0058	-0.07	722.42
App SI	72+36.90	721.78	-0.38	-0.0317	12.00					722.16	-0.33	-0.0138	24.00	72+36.90	722.49					12.00	-0.0058	-0.07	722.42
Bridge	72+63.20	721.97	-0.37	-0.0308	12.00					722.34	-0.35	-0.0146	24.00	72+63.20	722.69					12.00	-0.0108	-0.13	722.56

CALCULATED  
ESP

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ROADWAY "B" SUPERELEVATION TABLE

FRA-IR71-14.39  
FRA-315-0.00

24E  
89

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Roadway	From	To	Side		202 Guardrail Removed Lin Ft	622 Type D Conc Bar Lin Ft	605 4" SPUD Lin Ft	605 6" SPUD Lin Ft	605 4" Ty F Cond Lin Ft	605 6" Ty F Cond Lin Ft	605 4" Bends, Branches	605 6" Bends, Branches		
A	16+52.48	19+00.00	Lt. & Rt.		272.00									
A	19+00.00	31+00.00	Lt. & Rt.		319.10									
A	31+00.00	43+00.00	Lt. & Rt.		323.00	36.00								
A	43+00.00	46+30.00	Lt. & Rt.		0.00	36.00								
A	46+30.00	50+50.00	Lt. & Rt.		73.00	36.00	1,655.00							
A	50+50.00	55+50.00	Lt. & Rt.		385.79		1,571.00		20		2-45 Bends; 1-90 Bend; 1-Tee; 1-Wye			
A	55+50.00	60+50.00	Lt. & Rt.		458.35	285	1,425.00		23		2-45 Bends; 3-Wye			
A	60+50.00	65+50.00	Lt. & Rt.		326.00	555	1,387.00		17		3-45Bends; 1-Wye			
A	65+50.00	70+50.00	Lt. & Rt.		371.34	116	1,142.00	111.00	24	5	2-45 Bends; 1-Wye			
A	70+50.00	72+00.20	Lt. & Rt.		151.94	24	283.00	63.00						
B	16+95.89	19+00.00	Lt. & Rt.		207.00									
B	19+00.00	31+00.00	Lt. & Rt.		849.00									
B	31+00.00	43+00.00	Lt. & Rt.		934.00	36								
B	43+00.00	46+44.59	Lt. & Rt.		100.00									
B	46+44.59	50+50.00	Lt. & Rt.		0.00	95	1,598.00		5		1-45 Bend; 1-Wye			
B	50+50.00	55+50.00	Lt. & Rt.		235.00		1,706.00		23		2-45 Bends			
B	55+50.00	60+50.00	Lt. & Rt.		550.46	293	1,470.00		18		4-45 Bends; 2- Wye			
B	60+50.00	65+50.00	Lt. & Rt.		401.49	7	1,466.00		8	3	2-45 Bends; 1-Wye			
B	65+50.00	70+50.00	Lt. & Rt.		0.00	40	998.00	119.00			2-45 Bends	1-45 Bend		
B	70+50.00	72+36.82	Lt. & Rt.		0.00		178.00	120.00						
TOTAL					7,950	1,559	14,879	413	138	8				

SUMMARY OF GUARDRAIL REMOVAL,  
BARRIER AND UNDERDRAIN QUANTITIES

FRA-IR71-14.39,  
FRA-315-0.00



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Ref. No.	Roadway	From	To	Side	Area Pvt & Shldr Sq Ft	609 Type 2A Curb Lin Ft	451 10" Reinf Conc SY		Area 304	304 6" Agg Base CY	203 Subgrade ompact SY	202 Pavement Removed SY	202 Curb Removed Lin Ft
14-P	A	46+30.00	50+50.00	Lt & Rt	23,520.00	420.00	2,636.67		24,150.00	447.22	2,683.33	2,613.33	420.00
16-P	A	50+50.00	55+50.00	Lt & Rt	29,498.14	911.97	3,328.24		30,866.10	571.59	3,429.57	3,277.57	643.00
18-P	A	55+50.00	60+50.00	Lt & Rt	26,036.85	882.00	2,941.98		27,359.85	506.66	3,039.98	2,892.98	882.00
20-P	A	60+50.00	65+50.00	Lt & Rt	20,254.43	1,000.00	2,306.05		21,754.43	402.86	2,417.16	2,250.49	1,000.00
22-P	A	65+50.00	70+50.00	Lt & Rt	28,452.85	661.94	3,198.20		29,445.76	545.29	3,271.75	3,161.43	662.70
24-P	A	70+50.00	72+00.12	Lt & Rt	9,344.97	0.00	1,038.33		9,344.97	173.06	1,038.33	1,038.33	152.00
	A	Subtotals			137,107.24	3,875.91	15,449.47		142,921.11	2,646.69	15,880.12	15,234.14	3,759.70
15-P	B	44+40.00	50+50.00	Lt & Rt	17,838.04	405.41	2,004.53		18,446.16	341.60	2,049.57	1,982.00	405.41
17-P	B	50+50.00	55+50.00	Lt & Rt	21,507.19	950.00	2,442.47		22,932.19	424.67	2,548.02	2,389.69	950.00
19-P	B	55+50.00	60+50.00	Lt & Rt	19,625.00	1,000.00	2,236.11		21,125.00	391.20	2,347.22	2,180.56	1,000.00
21-P	B	60+50.00	65+50.00	Lt & Rt	19,661.80	1,000.00	2,240.20		21,161.80	391.89	2,351.31	2,184.64	1,000.00
23-P	B	65+50.00	70+50.00	Lt & Rt	34,723.89	656.87	3,894.70		35,709.20	661.28	3,967.69	3,858.21	394.40
25-P	B	70+50.00	72+36.82	Lt & Rt	11,141.13	0.00	1,237.90		11,141.13	206.32	1,237.90	1,237.90	0.00
	B	Subtotals			124,497.05	4,012.28	14,055.91		130,515.47	2,416.95	14,501.72	13,833.01	3,749.81

SUMMARY OF PAVEMENT QUANTITIES

FRA-IR71-14.39,  
FRA-315-0.00

DATE: 05/16/2002 09:33:31 AM  
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REF. NO.	SHEET. NO.				606 GUARDRAIL, TYPE 5	606 ANCHOR ASSEMBLY, TYPE E-98	606 ANCHOR ASSEMBLY, TYPE T	606 BRIDGE TERMINAL ASSEMBLY, TYPE I												
		ALIGNMENT	FROM STA TO STA	SIDE					LIN FT	EACH	EACH	EACH								
1-GR	29, 30	ROAD "A"	16+50 TO 21+00	RT	437.5		1	1												
2-GR	29, 30	RAMP " H"	3+22 TO 6+22	RT	237.5	1	1													
3-GR	30	ROAD "A"	33+78 TO 34+66	RT	38	1		1												
4-GR	31	RAMP "AC"	488+00 TO 488+88	RT	38	1		1												
5-GR	31	ROAD "A"	50+18 TO 50+93	RT	25	1		1												
6-GR	35	ROAD "A"	57+70 TO 58+45	RT	25	1		1												
7-GR	35, 36	ROAD "A"	59+31 TO 61+56	RT	225			2												
8-GR	35	ROAD "A"	57+10 TO 57+97	LT	37.5	1		1												
9-GR	35, 36	ROAD "A"	59+80 TO 62+42	LT	262			2												
10-GR	37, 38	RAMP "DJ"	14+05 TO 19+30	RT	525			1												
11-GR	29, 30	ROAD "B"	16+95 TO 22+95	LT	550	1		1												
12-GR	29, 30	RAMP "G"	2+50 TO 12+37.5	LT	925	1	1													
13-GR	30	ROAD "B"	33+60 TO 37+47	LT	375		1	1												
14-GR	30, 31	ROAD "B"	37+87 TO 44+00	LT	613			1												
15-GR	31	ROAD "B"	51+20 TO 52+08	RT	37.5	1		1												
16-GR	35	ROAD "B"	56+46 TO 57+34	RT	37.5	1		1												
17-GR	35	ROAD "B"	55+74 TO 57+12	LT	88	1		1												
18-GR	35, 36	ROAD "B"	60+10 TO 65+47	RT	487	1		1												
19-GR	36	ROAD "B"	60+58 TO 64+46	LT	338	1		1												
20-GR	37	RAMP "BD"	73+00 TO 72+00	LT	50	1														
TOTAL					5352	14	4	19												

SUMMARY OF GUARDRAIL QUANTITIES

FRA-IR71-14.39  
FRA-315-0.00

DATE: 05/16/2002 09:44:47 AM  
FILENAME: T:\DRAWING\06\06120\306\support\Quantities.dwg

REF. NO.	SHEET. NO.							603	604	604	604	604	604	604	604	604	604						CALCULATED BBB	CHECKED LJS	
								15" CONDUIT TYPE B	CURB INLET NO. 3E	CURB INLET NO. 2-A-6	CURB INLET NO. 2-A-8	CURB INLET NO. 2-A-10	CURB INLET NO. 2-A-12	CATCH BASIN NO. 3	CATCH BASIN NO. 3A	CATCH BASIN NO. 5	CATCH BASIN NO. 6								MANHOLE RECONSTRUCTED TO GRADE
								ALIGNMENT	STATION	SIDE	LIN FT	EACH	EACH	EACH	EACH	EACH	EACH								EACH
D-1	53	ROAD "B"	47+00	LT. CURB						/															
D-2	54	ROAD "B"	53+00	LT. CURB										/											
D-3	54	ROAD "B"	53+00	RT. CURB							/														
D-4	54	ROAD "A"	53+00	RT. CURB									/												
D-5	54	ROAD "A"	55+32	RT. CURB						/															
D-6	55	ROAD "B"	58+50	LT. CURB										/											
D-7	55	ROAD "B"	58+50	RT. CURB										/											
D-8	55	ROAD "A"	56+60	RT. CURB													/								
D-9	55	ROAD "A"	57+74	RT. CURB						/															
D-10	55	ROAD "A"	60+45	RT. CURB						/															
D-11	56	ROAD "B"	61+00	RT. CURB							/														
D-12	56	ROAD "B"	61+00	LT. CURB						/															
D-13	56	ROAD "A"	60+77	LT. CURB						/															
D-14	56	ROAD "A"	61+12	LT. CURB										/											
D-15	56	ROAD "A"	62+20	LT. CURB						/															
D-16	56	ROAD "A"	60+75	RT. CURB										/											
D-17	57	ROAD "DJ"	15+00	RT. CURB								/													
D-18	57	ROAD "DJ"	16+65	RT. CURB					/																
D-19	57	ROAD "BC"	66+50	LT. CURB													/								
D-20	53	ROAD "B"	46+90	RT. CURB									/												
D-21	53	ROAD "A"	47+10	LT. CURB						/															
D-22	57	ROAD "B"	68+46	41' LT.			74	/																	
D-23	57	ROAD "B"	70+25	40' LT.			74	/																	
D-24	54	ROAD "B"	71+00	42' LT.				/																	
D-25	55	ROAD "A"	57+95	33' LT.																					
D-26	55	ROAD "A"	58+70	34' LT.												/					/				
D-27	55	ROAD "A"	58+84	57' LT.																	/				
D-28	55	ROAD "B"	58+50	17' RT.												/									
D-29	55	ROAD "B"	58+50	53' RT.												/									
D-30	56	ROAD "B"	61+00	47' LT.												/									
D-32	56	ROAD "B"	61+00	23' RT												/									
D-33	56	ROAD "A"	60+75	45' RT												/									
D-34	56	ROAD "A"	62+03	7' RT																	/				
D-35	56	ROAD "B"	64+00	23' RT												/									
D-36	56	ROAD "B"	64+00	50' LT												/									
D-37	57	ROAD "BD"	73+00	RT. CURB										/											
D-38	57	ROAD "BD"	73+00	LT. CURB											/										
D-39	57	ROAD "A"	65+50	LT. CURB											/										
D-40	57	ROAD "DJ"	13+40	23' LT.			56														/				
D-41	57	ROAD "DJ"	13+51	RT. CURB										/								/			
D-42	57	ROAD "A"	68+40	46' RT.																					
D-43	57	ROAD "B"	68+89	RT. CURB										/											
D-44	57	ROAD "B"	69+33	LT. CURB			94								/										
D-45	55	ROAD "B"	57+50	RT. CURB			28		/																
D-46	55	ROAD "B"	57+50	LT. CURB			40		/																
D-47	55	ROAD "A"	56+50	RT. SHOULDER														/							
D-48	53	ROAD "B"	47+00	14' RT.																	/				
D-49	54	ROAD "AD"	53+00	LT. CURB					/																
D-50	55	ROAD "B"	60+54	125' RT.										/											
TOTAL								366	3	//	3	/	2	8	4	8	3	6							

SUMMARY OF DRAINAGE QUANTITIES

FRA-IR71-14.39  
FRA-315-0.00

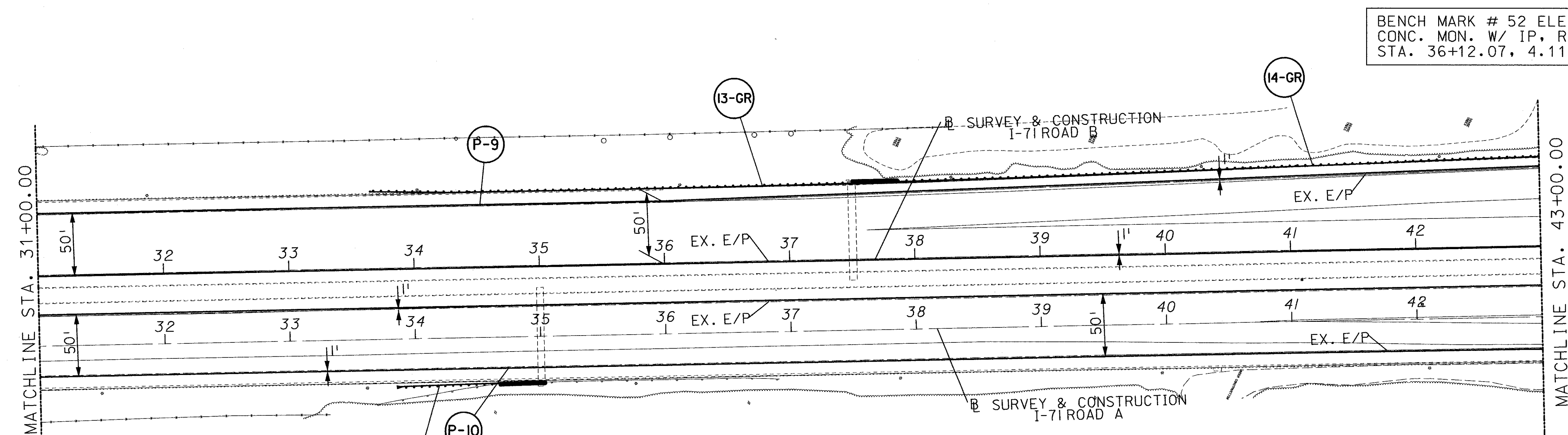
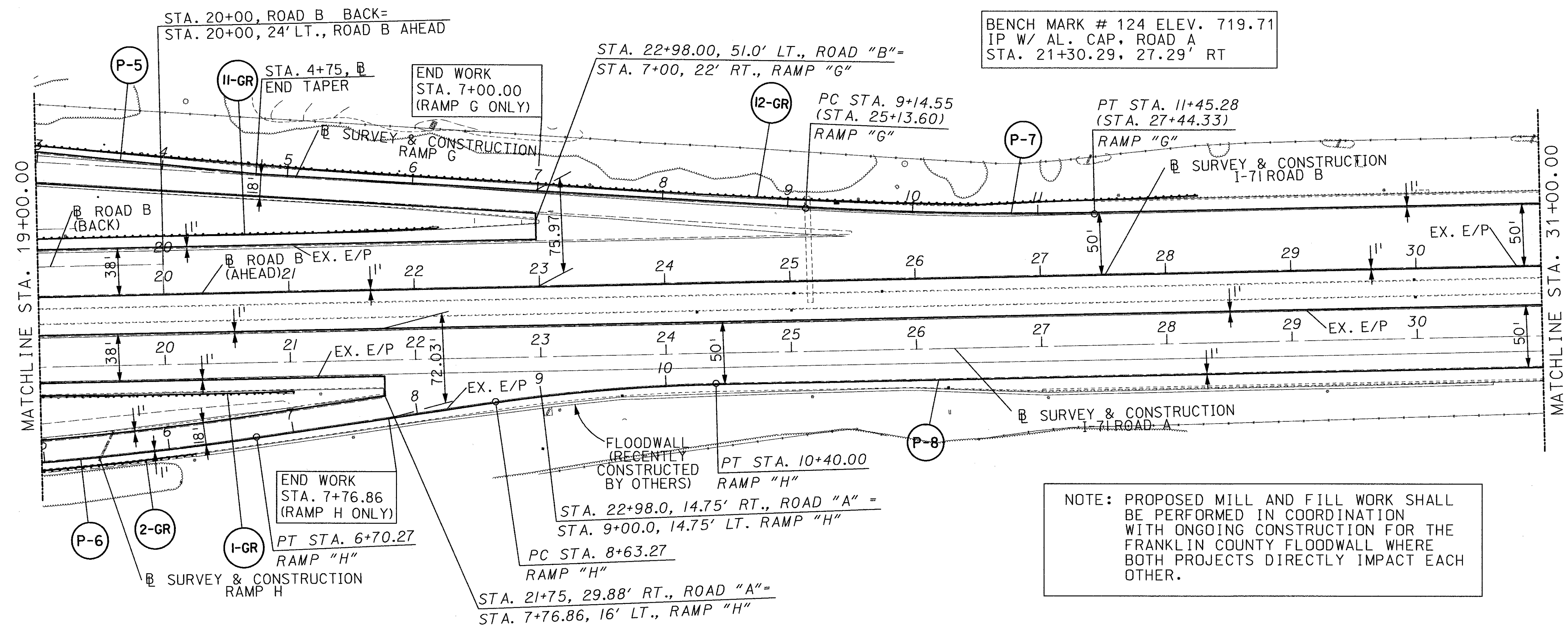
28  
89

SUMMARY OF DRAINAGE QUANTITIES

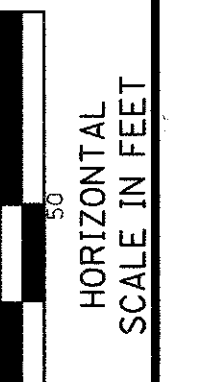
FRA-IR71-14.39  
FRA-315-0.00







CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY



CALCULATED  
BBB  
CHECKED  
LJS

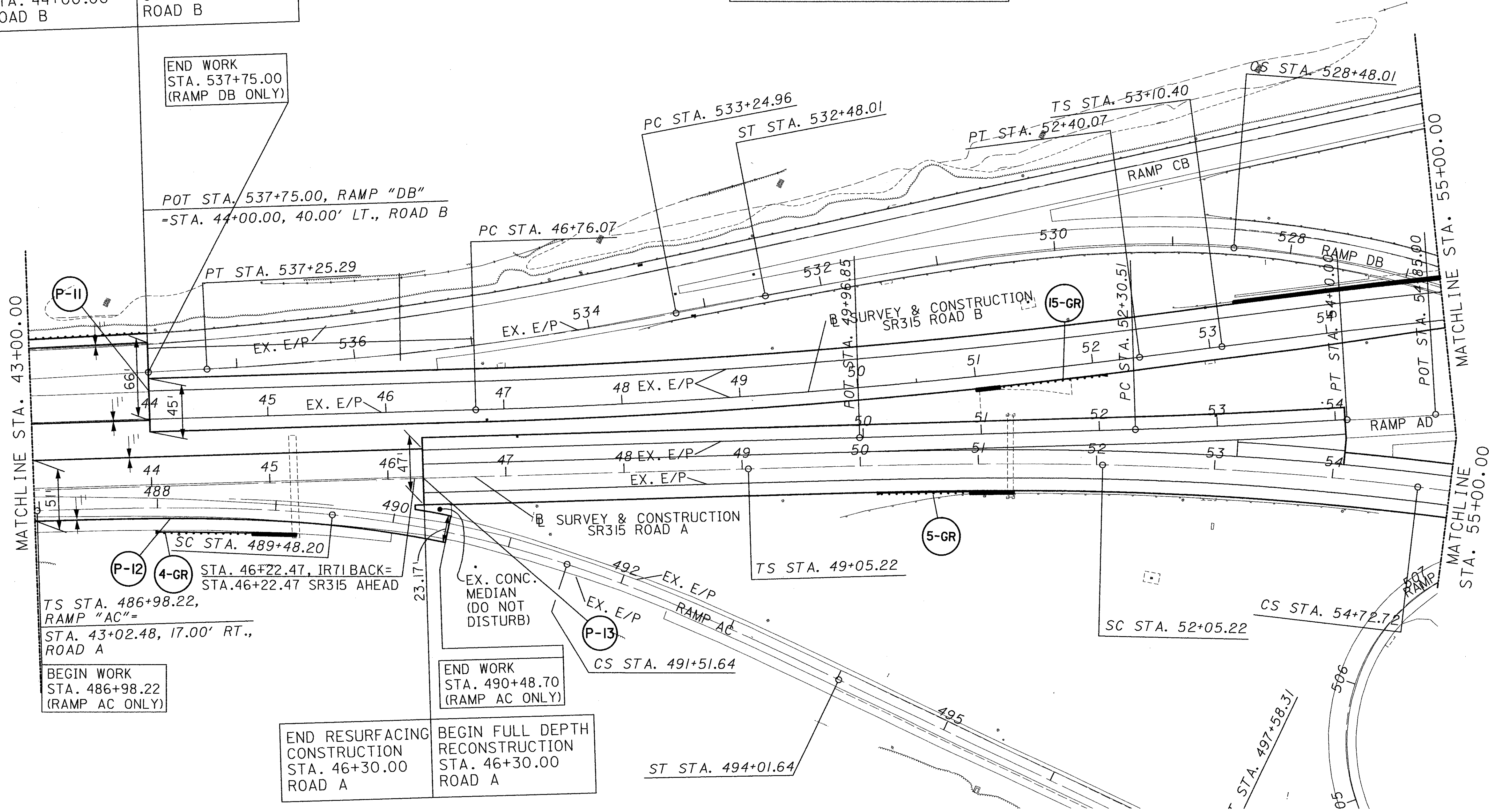
CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
46 - 48	PAVEMENT DETAIL

END RESURFACING  
CONSTRUCTION  
STA. 44+00.00  
ROAD B

BEGIN FULL DEPTH  
RECONSTRUCTION  
STA. 44+00.00  
ROAD B

BENCH MARK # 120 ELEV. 720.14  
PK W/ SHINER, ROAD A  
STA. 44+15.31, 35.36' RT

END WORK  
STA. 537+75.00  
(RAMP DB ONLY)

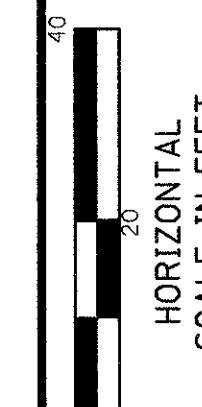


NOTE: PROPOSED MILL AND FILL WORK SHALL BE PERFORMED IN COORDINATION WITH ONGOING CONSTRUCTION FOR THE FRANKLIN COUNTY FLOODWALL WHERE BOTH PROJECTS DIRECTLY IMPACT EACH OTHER.

PLAN - IR71, SR315  
STA. 43+00.00 TO STA. 55+00.00

FRA-171-14.39,  
FRA-315-0.00





CALCULATED  
BBB  
CHECKED  
LJS

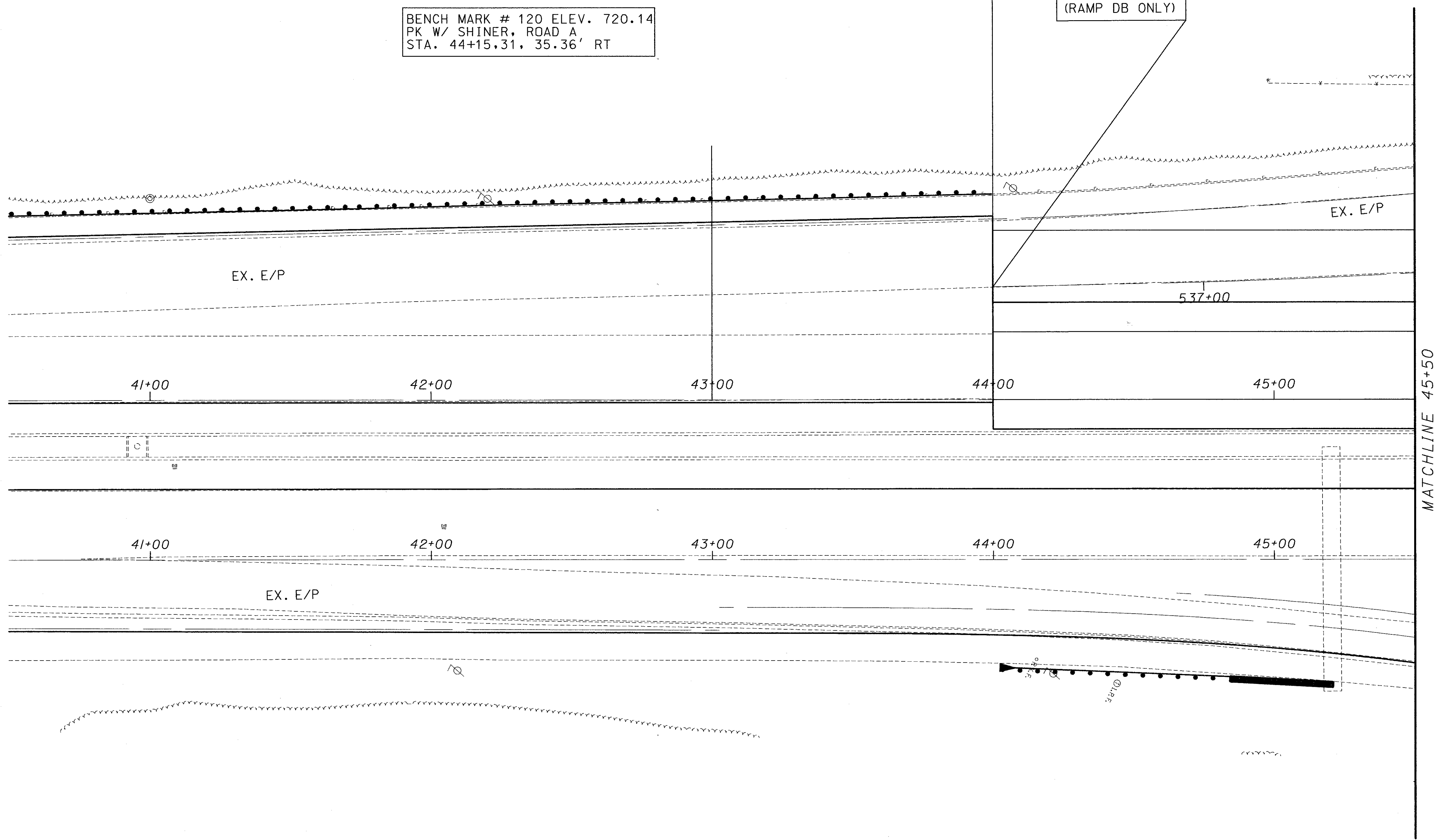
CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
46	PAVEMENT DETAIL

END RESURFACING  
CONSTRUCTION  
STA. 44+00.00  
ROAD B

BEGIN FULL DEPTH  
RECONSTRUCTION  
STA. 44+00.00  
ROAD B

END WORK  
STA. 537+75.00  
(RAMP DB ONLY)

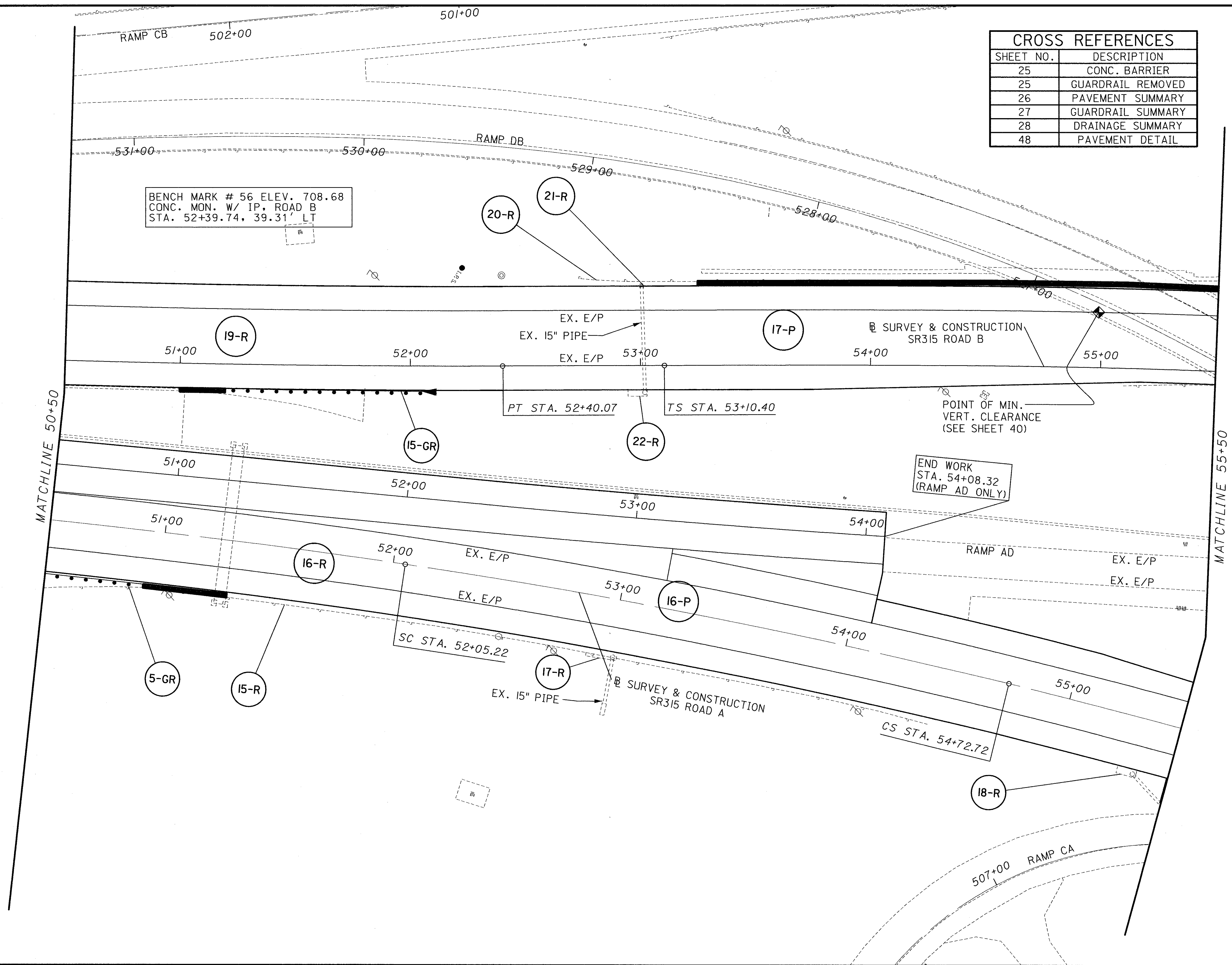
BENCH MARK # 120 ELEV. 720.14  
PK W/ SHINER, ROAD A  
STA. 44+15.31, 35.36' RT




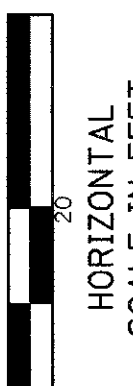
PLAN  
S.R. 315 - STA. 40+50.00 TO STA. 45+50.00

FRA-171-14.39,  
FRA-315-0.00 (A/B)





CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
48	PAVEMENT DETAIL

  
  
HORIZONTAL  
SCALE IN FEET

CALCULATED  
BBB

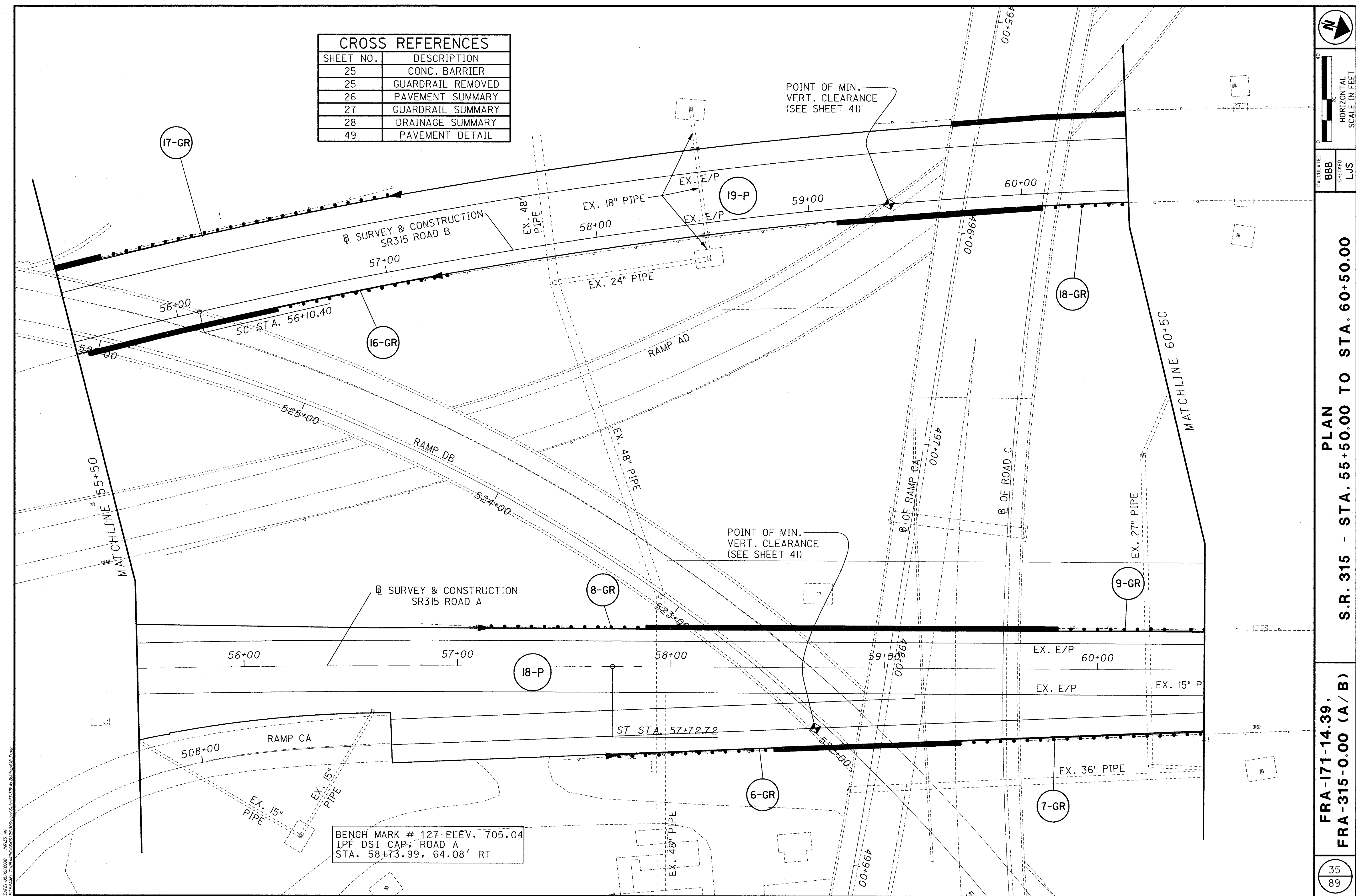
CHECKED  
LUS

FRA-171-14.39,  
FRA-315-0.00 (A/B)

PLAN  
S.R. 315 - STA. 50+50.00 TO STA. 55+50.00



CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
49	PAVEMENT DETAIL



CALCULATED  
BBB

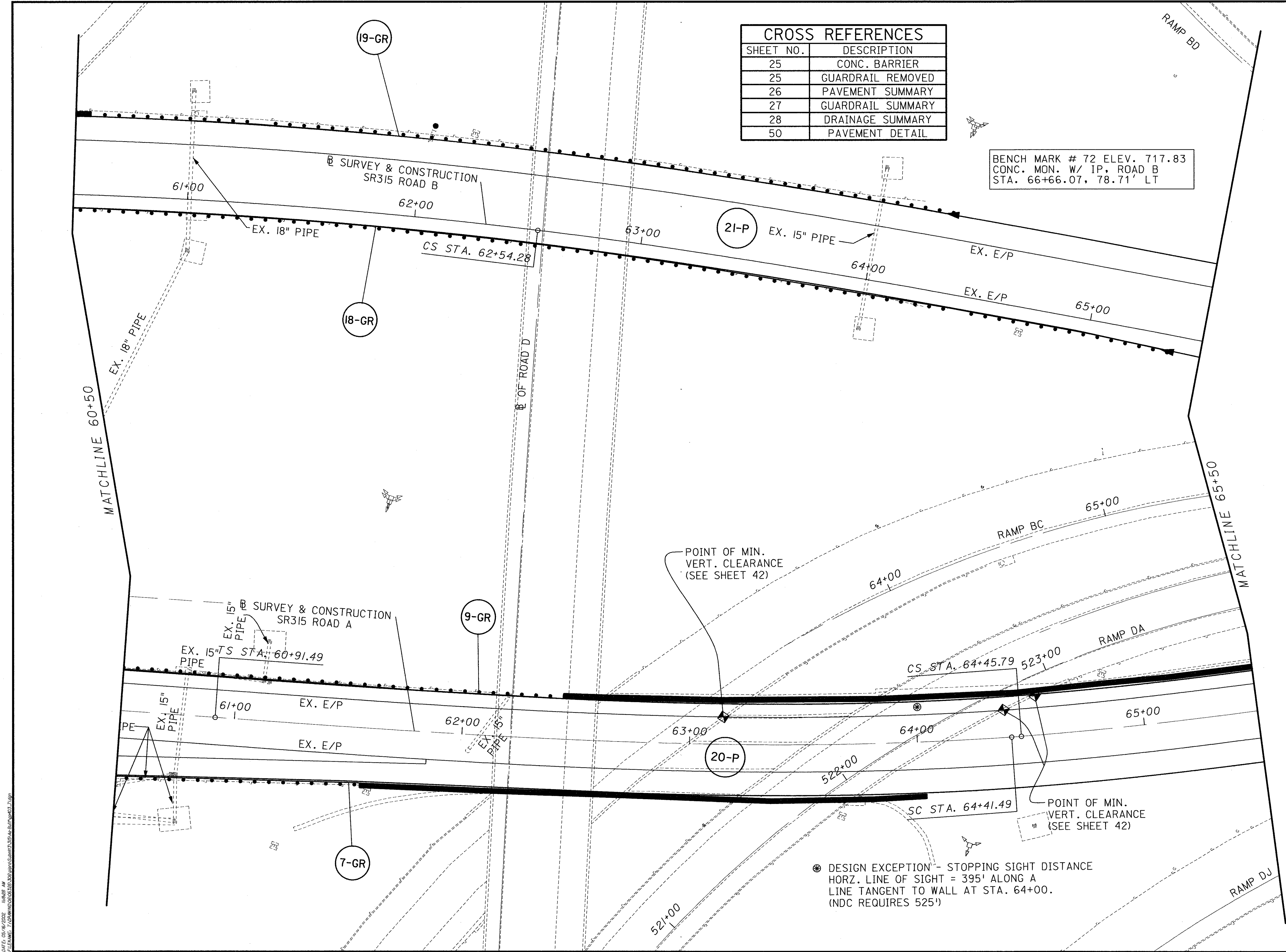
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LJS

FRA-171-14.39,  
FRA-315-0.00 (A/B)

PLAN  
S.R. 315 - STA. 55+50.00 TO STA. 60+50.00

35  
89

DATE: 07/16/2020 10:08 AM  
DRAWN: J. BROWN  
CHECKED: J. BROWN  
DESIGNED: J. BROWN



CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
50	PAVEMENT DETAIL

BENCH MARK # 72 ELEV. 717.83  
CONC. MON. W/ IP, ROAD B  
STA. 66+66.07, 78.71' LT



CALCULATED  
BBB  
CHECKED  
LJS

PLAN  
S.R. 315 - STA. 60+50.00 TO STA. 65+50.00

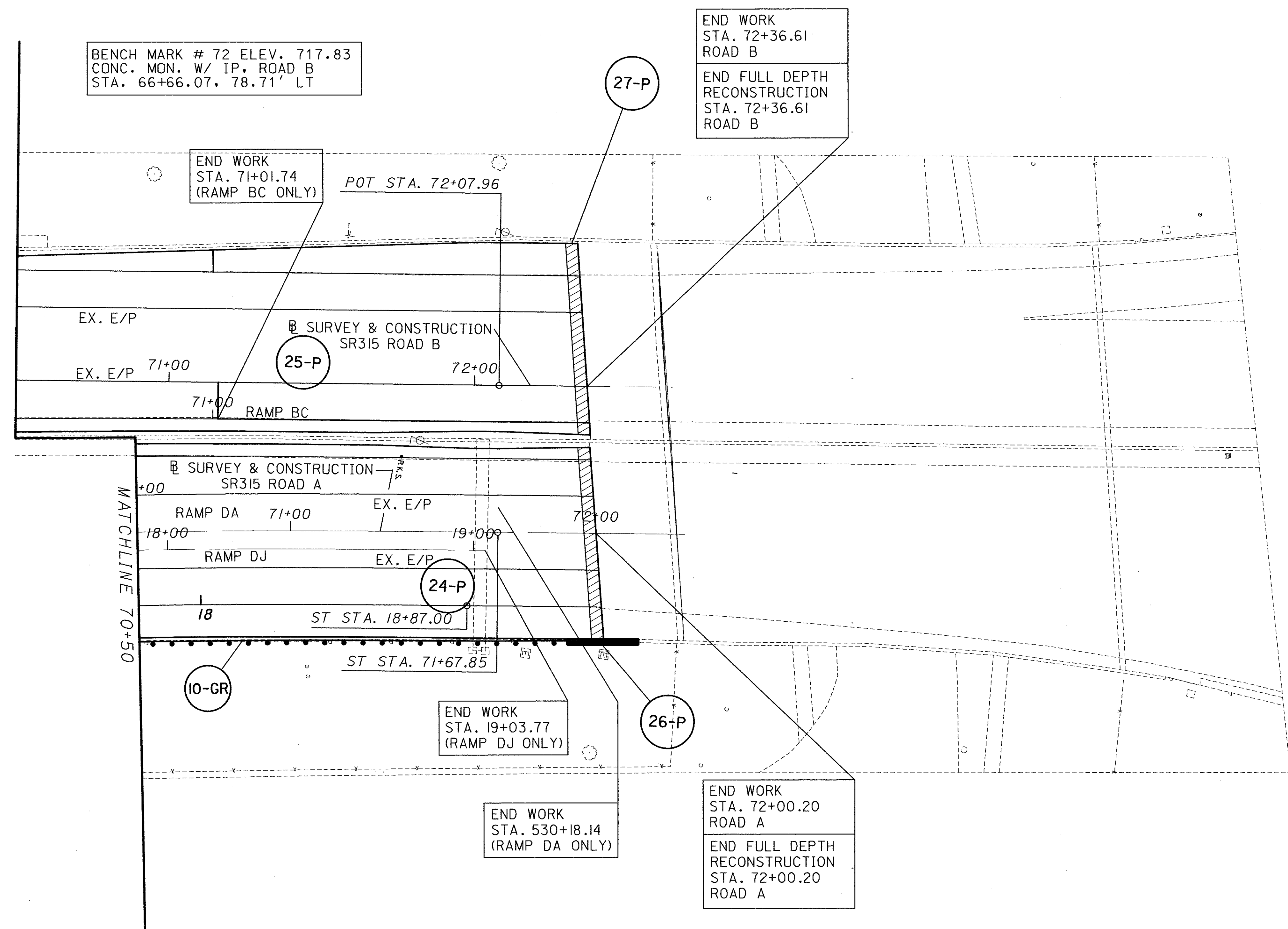
FRA-171-14.39,  
FRA-315-0.00 (A/B)

36  
89





DATE: 01/16/2002 10:00:00 AM  
PROJECT: FRA-171-14.39, FRA-315-0.00 (A/B)



CROSS REFERENCES	
SHEET NO.	DESCRIPTION
25	CONC. BARRIER
25	GUARDRAIL REMOVED
26	PAVEMENT SUMMARY
27	GUARDRAIL SUMMARY
28	DRAINAGE SUMMARY
52	PAVEMENT DETAIL

CALCULATED  
BBB

CHECKED  
LJS

FRA-171-14.39,  
FRA-315-0.00 (A/B)

PLAN  
S.R. 315 - STA. 70+50.00 TO STA. 72+00.12

38  
89

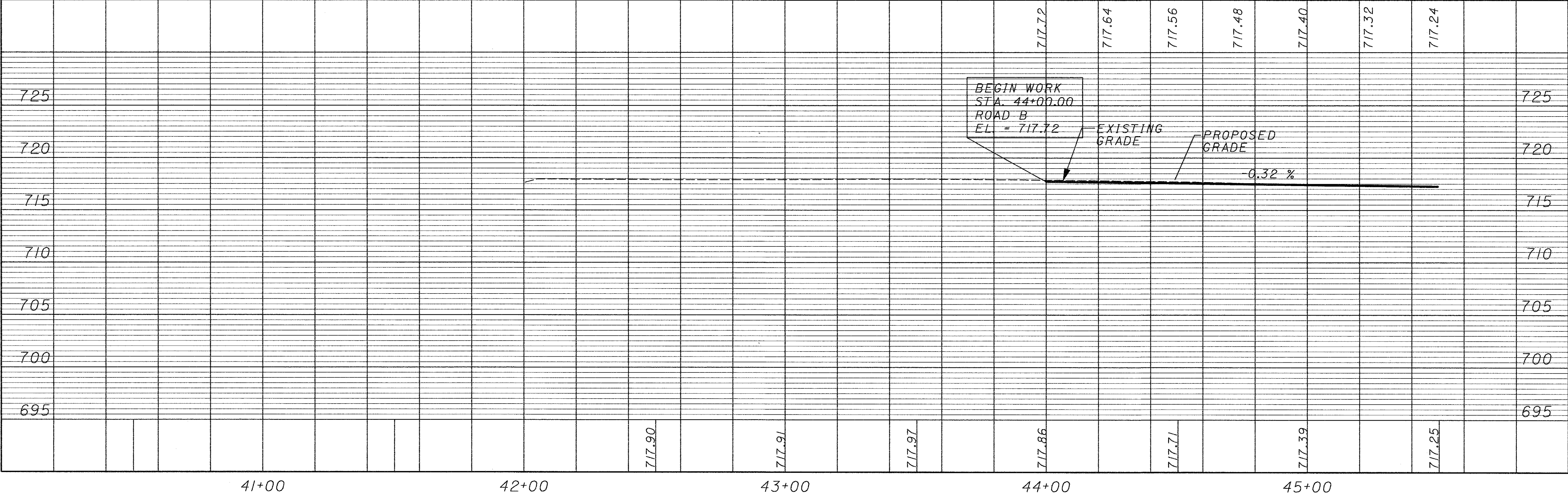
N

0 20 40

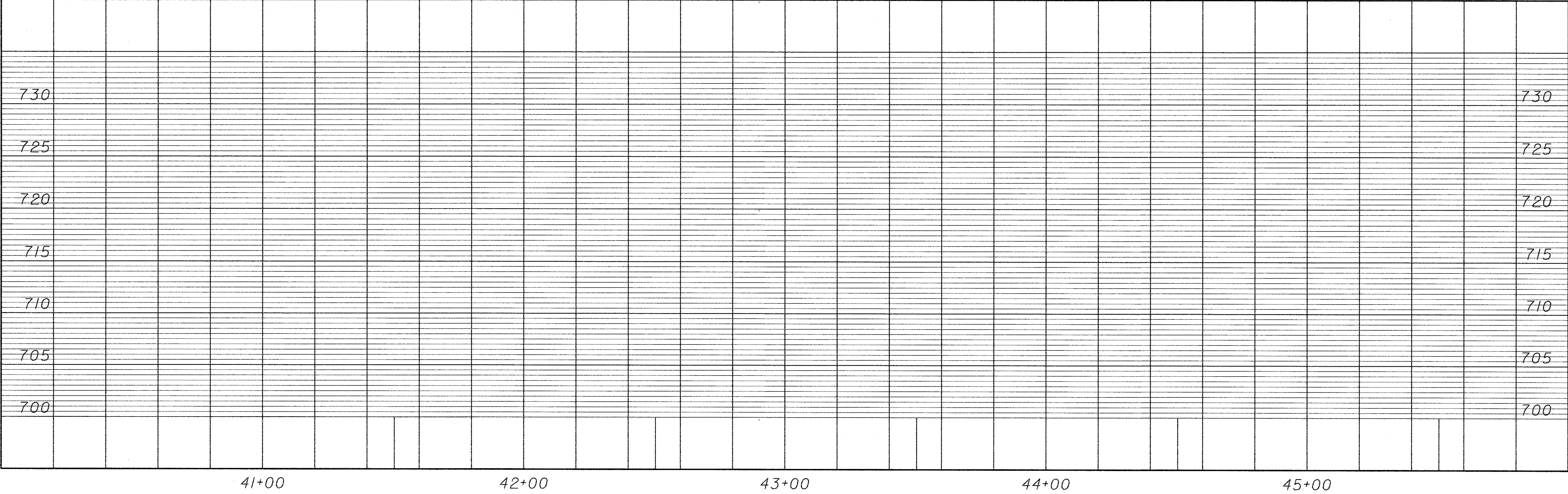
HORIZONTAL  
SCALE IN FEET

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ROAD B



ROAD A



CALCULATED  
BBB

CHECKED  
LJS

PROFILES - ROAD A & B  
STA. 40+50.00 TO STA. 45+50.00

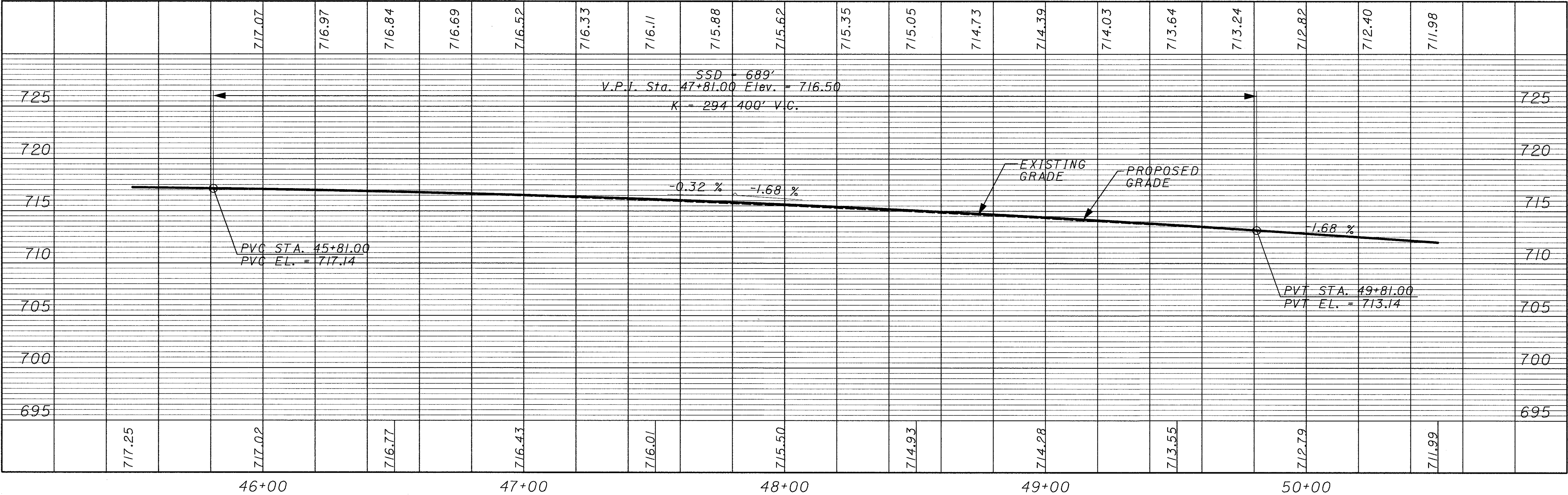
FRA-IR71-14.39,  
FRA-315-0.00

39  
89

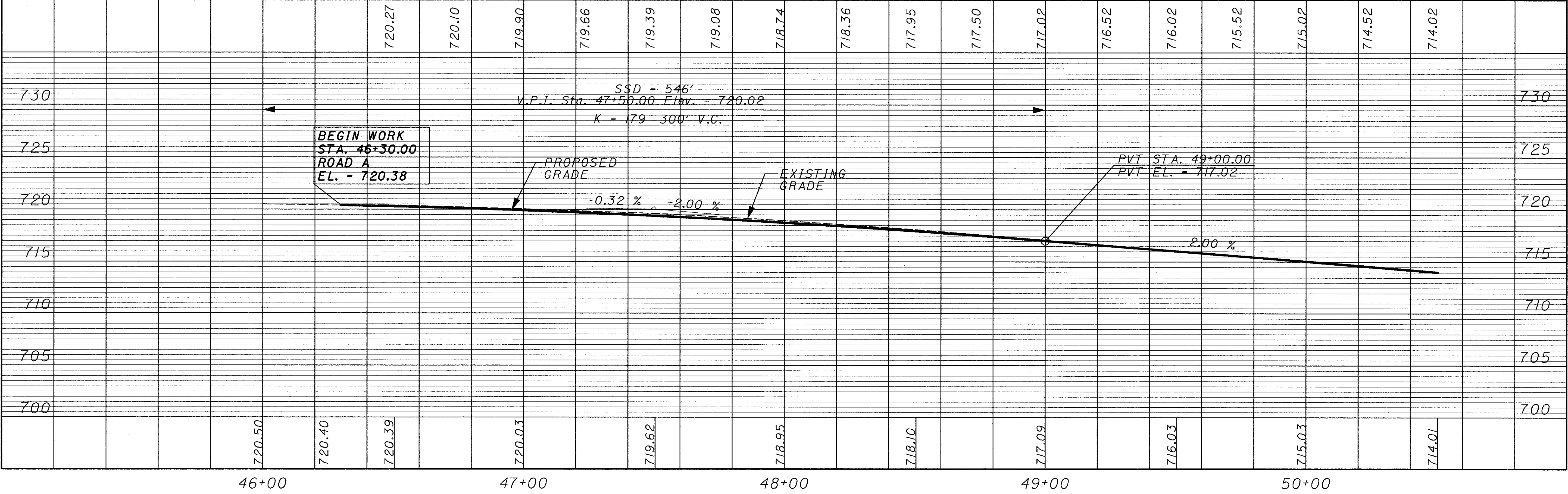


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ROAD B



ROAD A



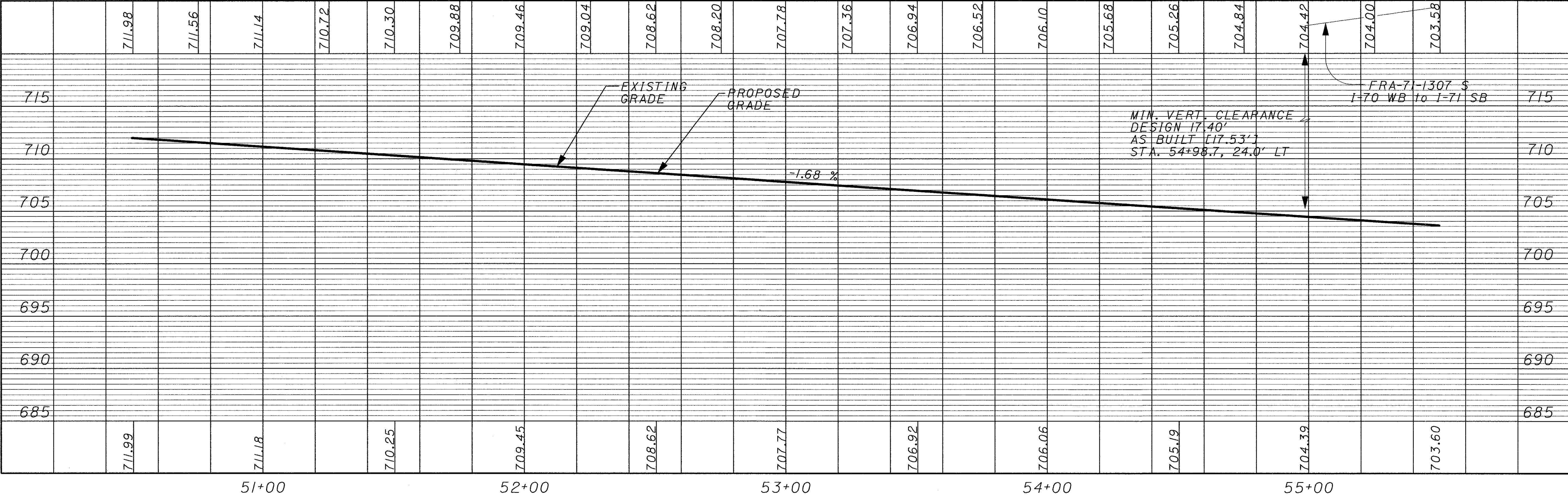
PROFILES - ROAD A & B  
STA. 45+50.00 TO STA. 50+50.00

FRA-IR71-14.39,  
FRA-315-0.00

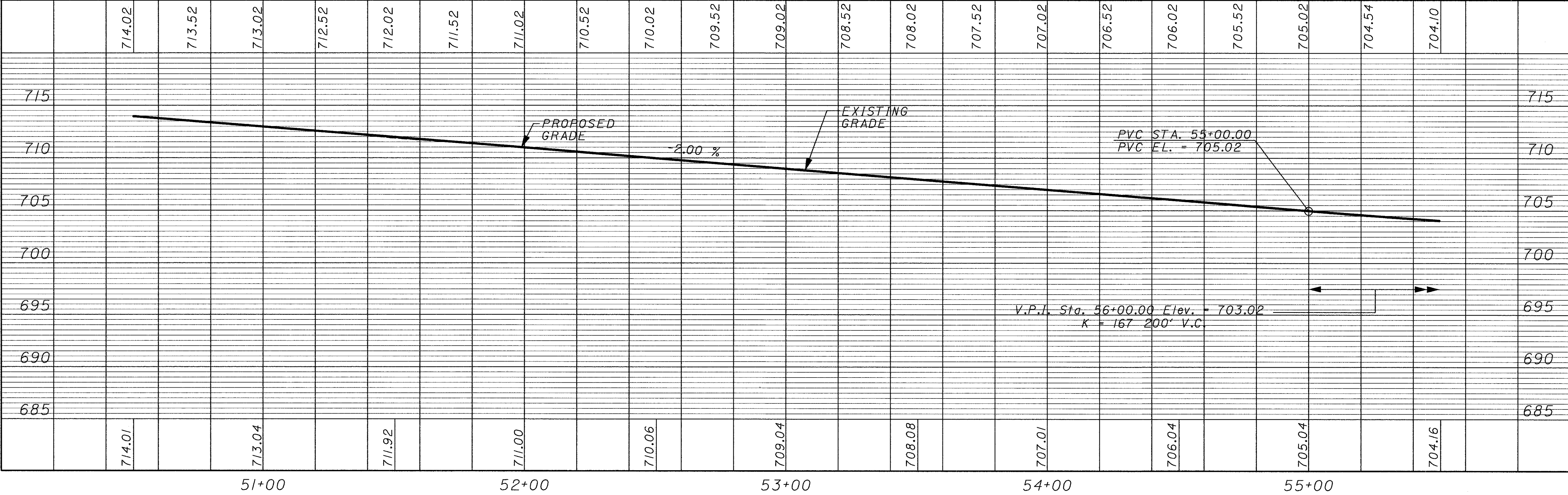
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BBB  
CHECKED  
LJS



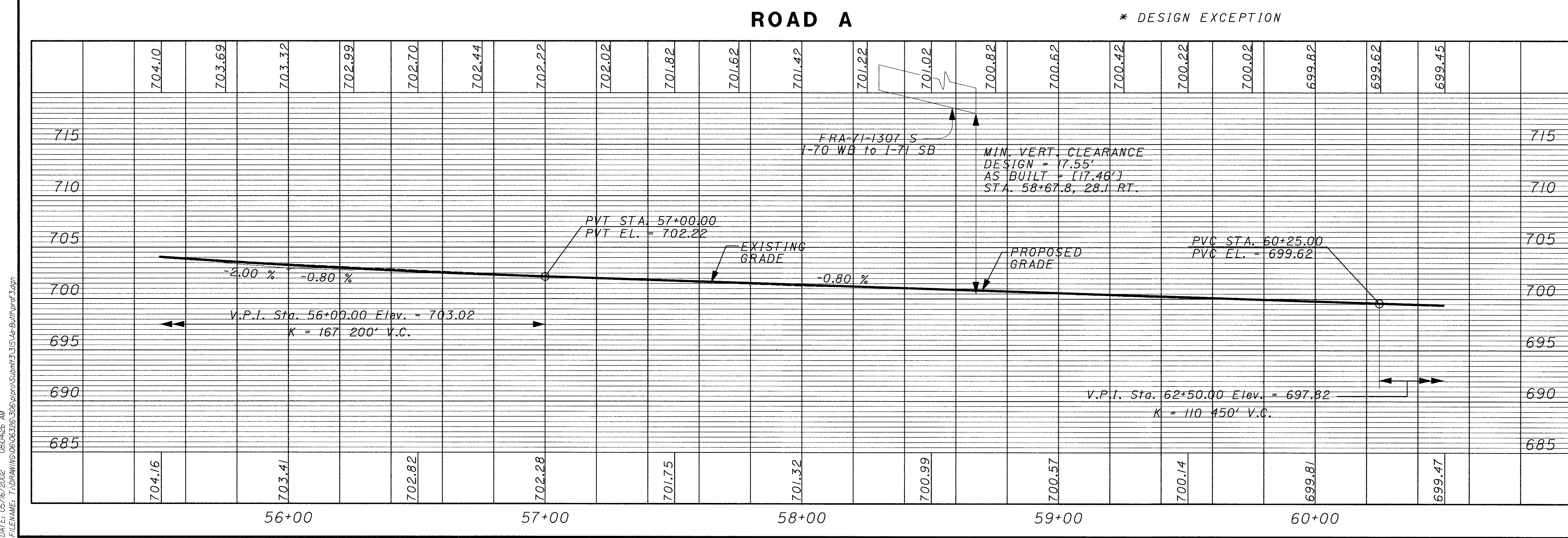
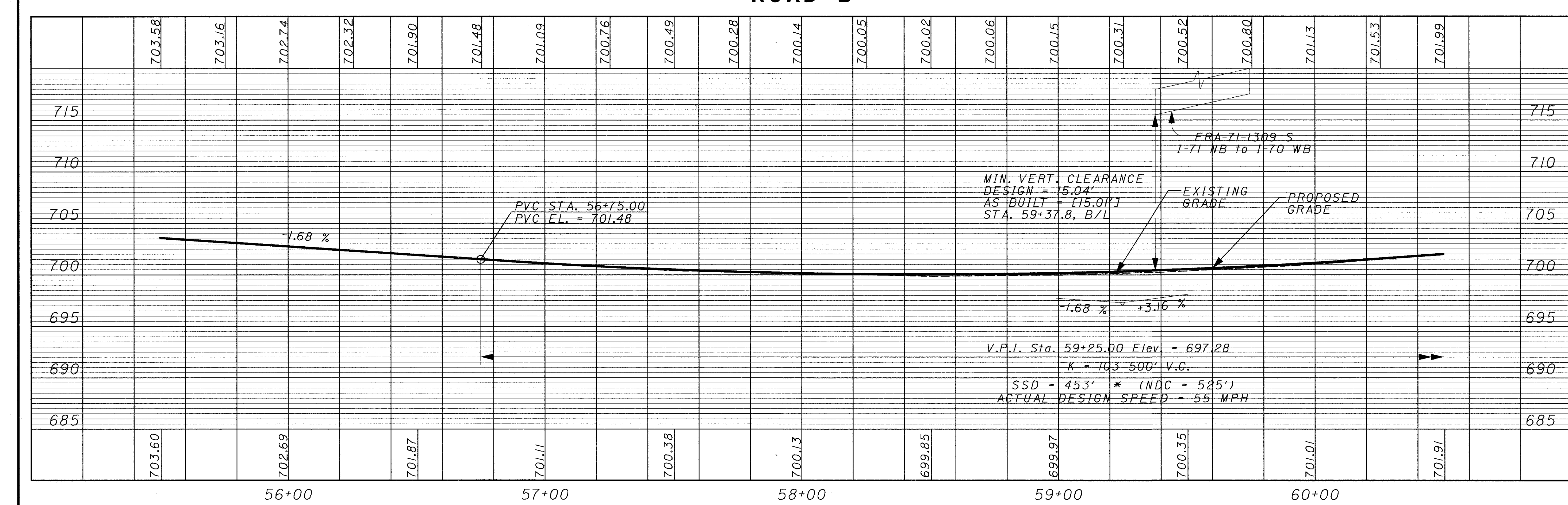
ROAD B



ROAD A



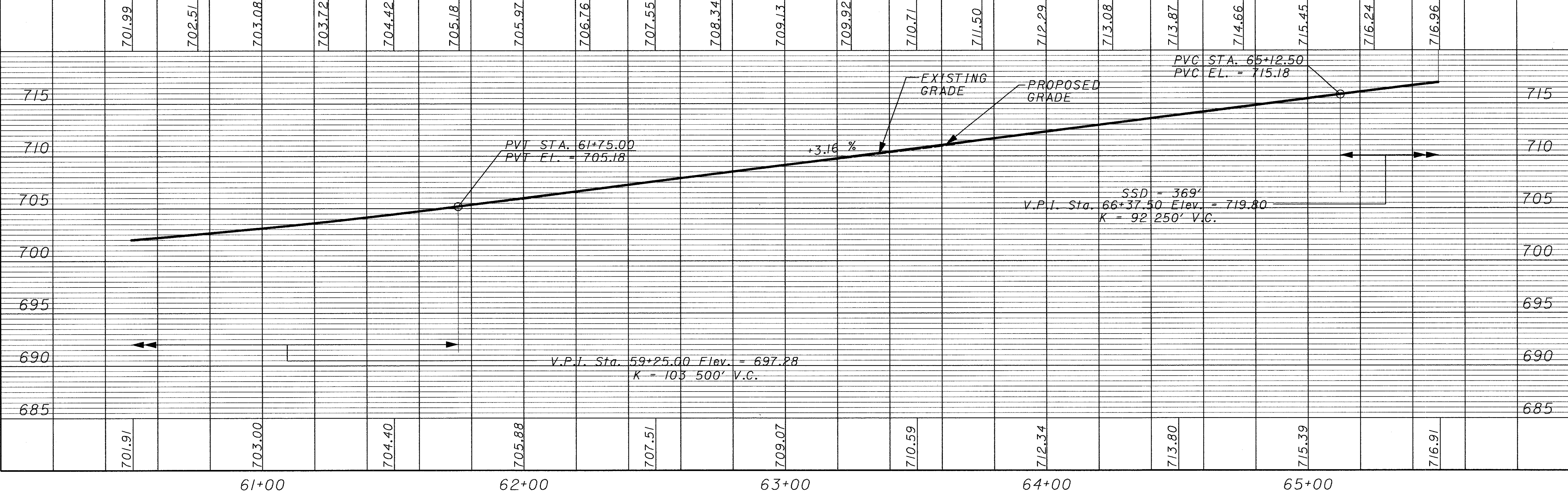
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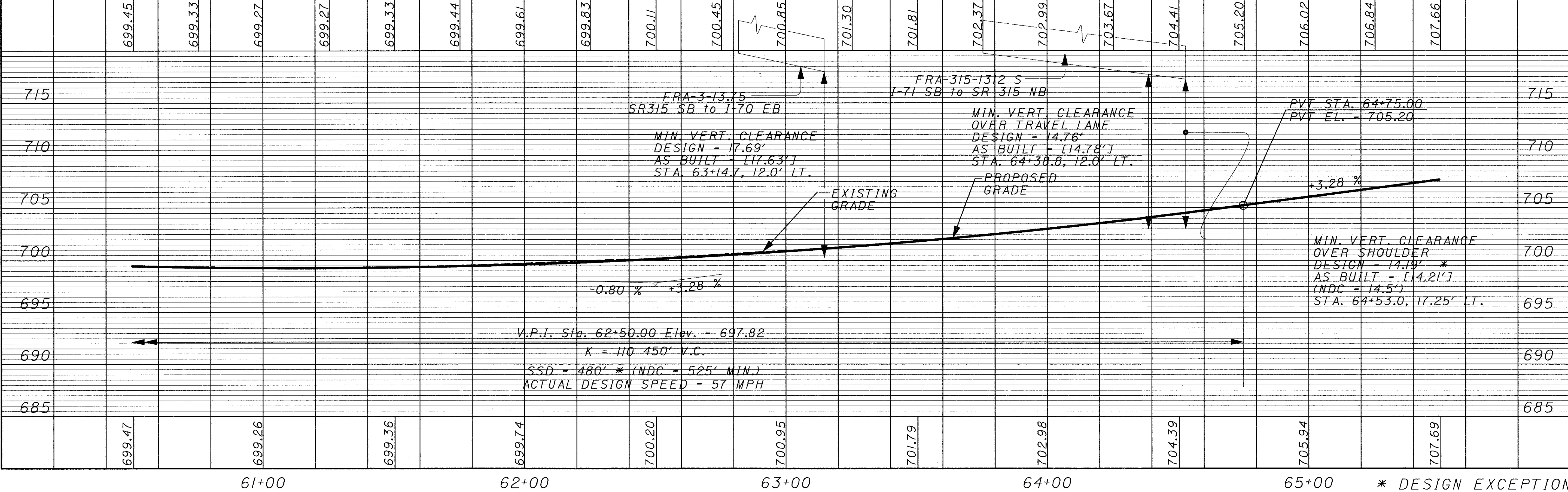


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ROAD B



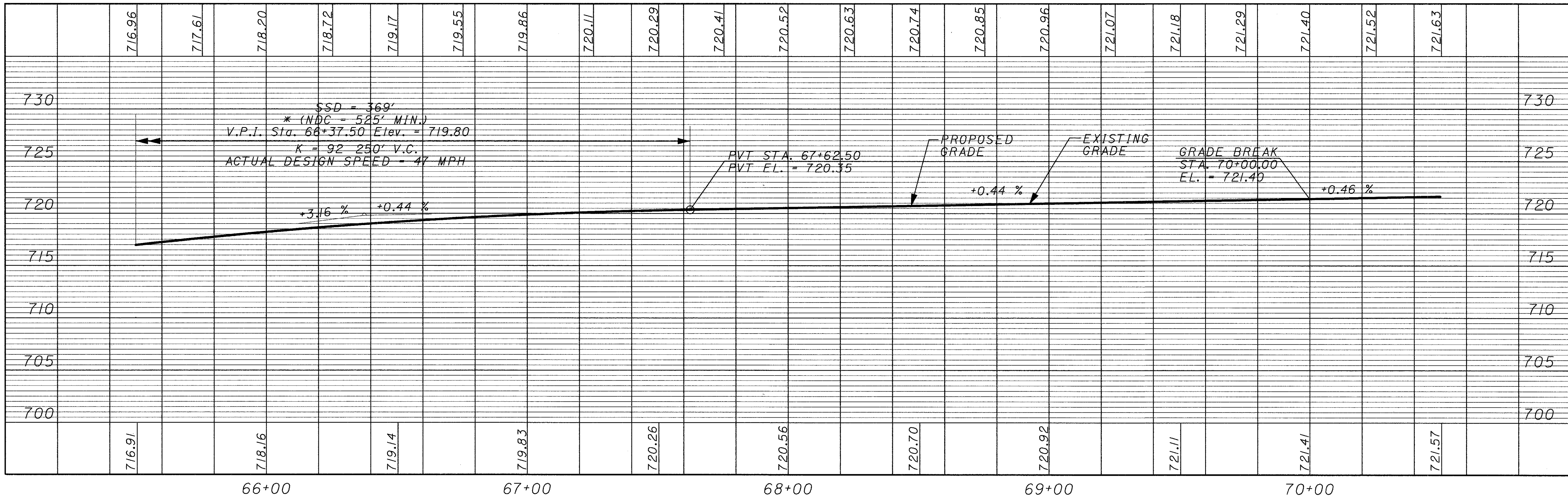
ROAD A



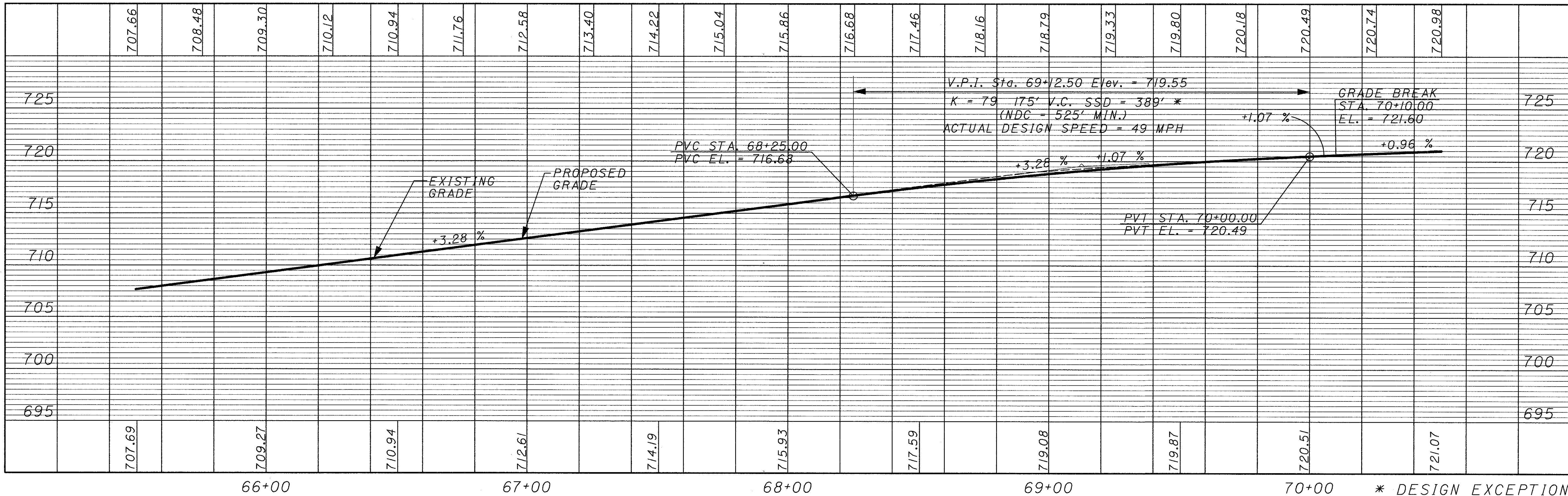


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ROAD B



ROAD A

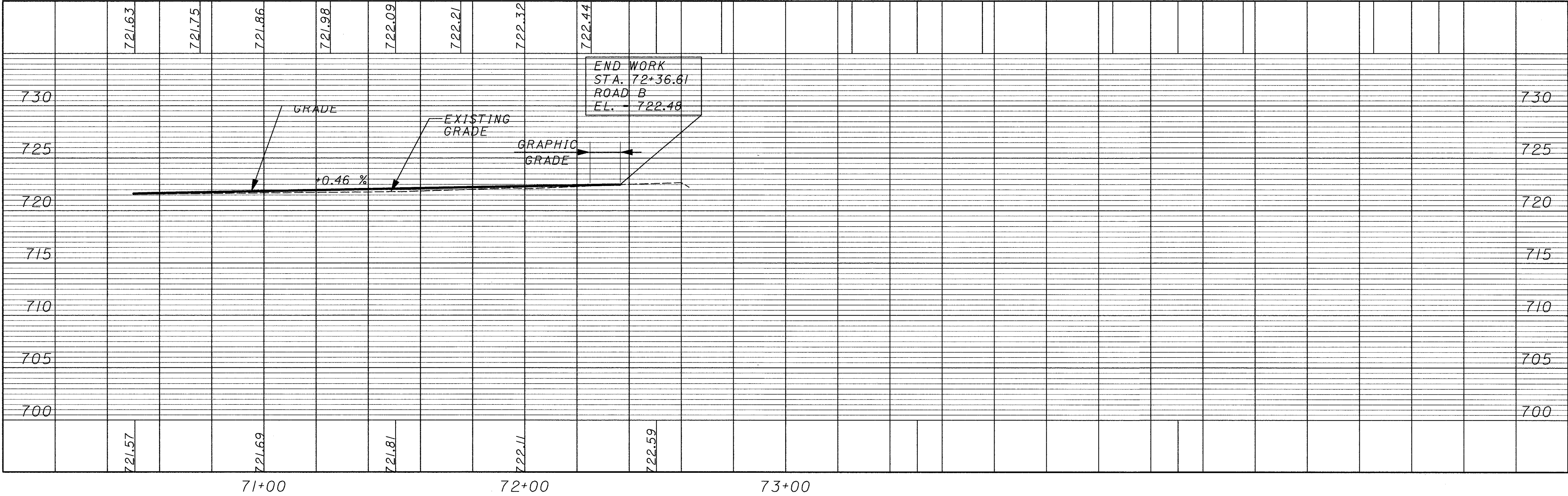


\* DESIGN EXCEPTION

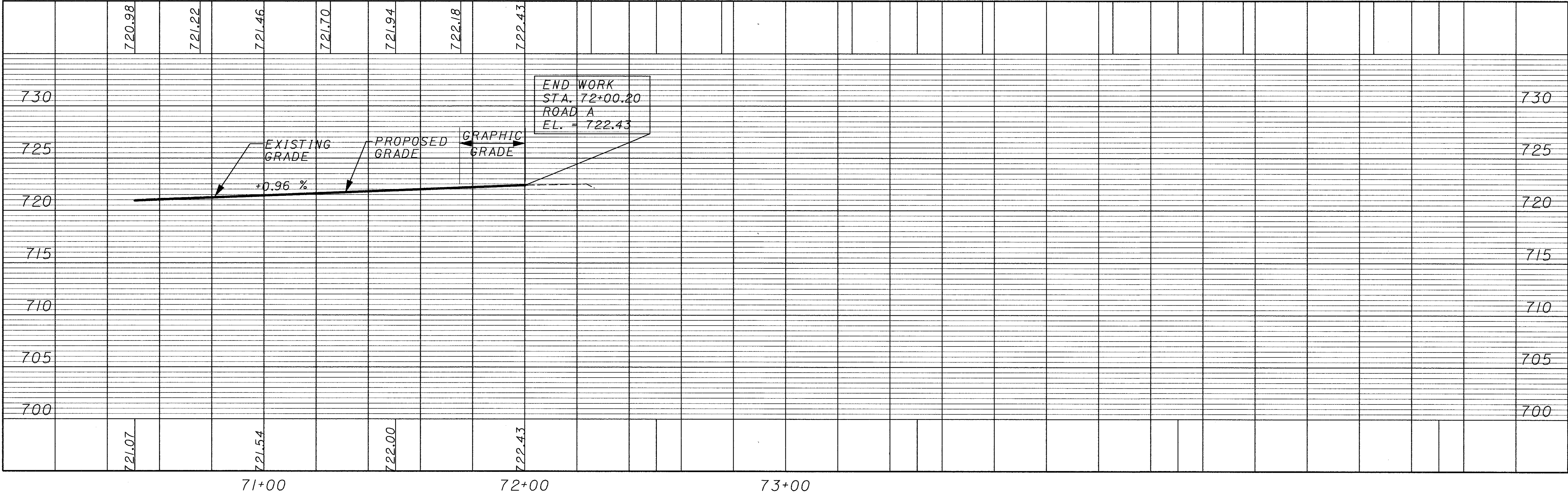
PROFILES - ROAD A & B  
STA. 65+50.00 TO STA. 70+50.00

FRA-IR71-14.39,  
FRA-315-0.00

ROAD B



ROAD A



CALCULATED  
BBB  
CHECKED  
LJS

PROFILES - ROAD A & B  
STA. 70+50.00 TO STA. 72+00.35

FRA-IR71-14.39,  
FRA-315-0.00

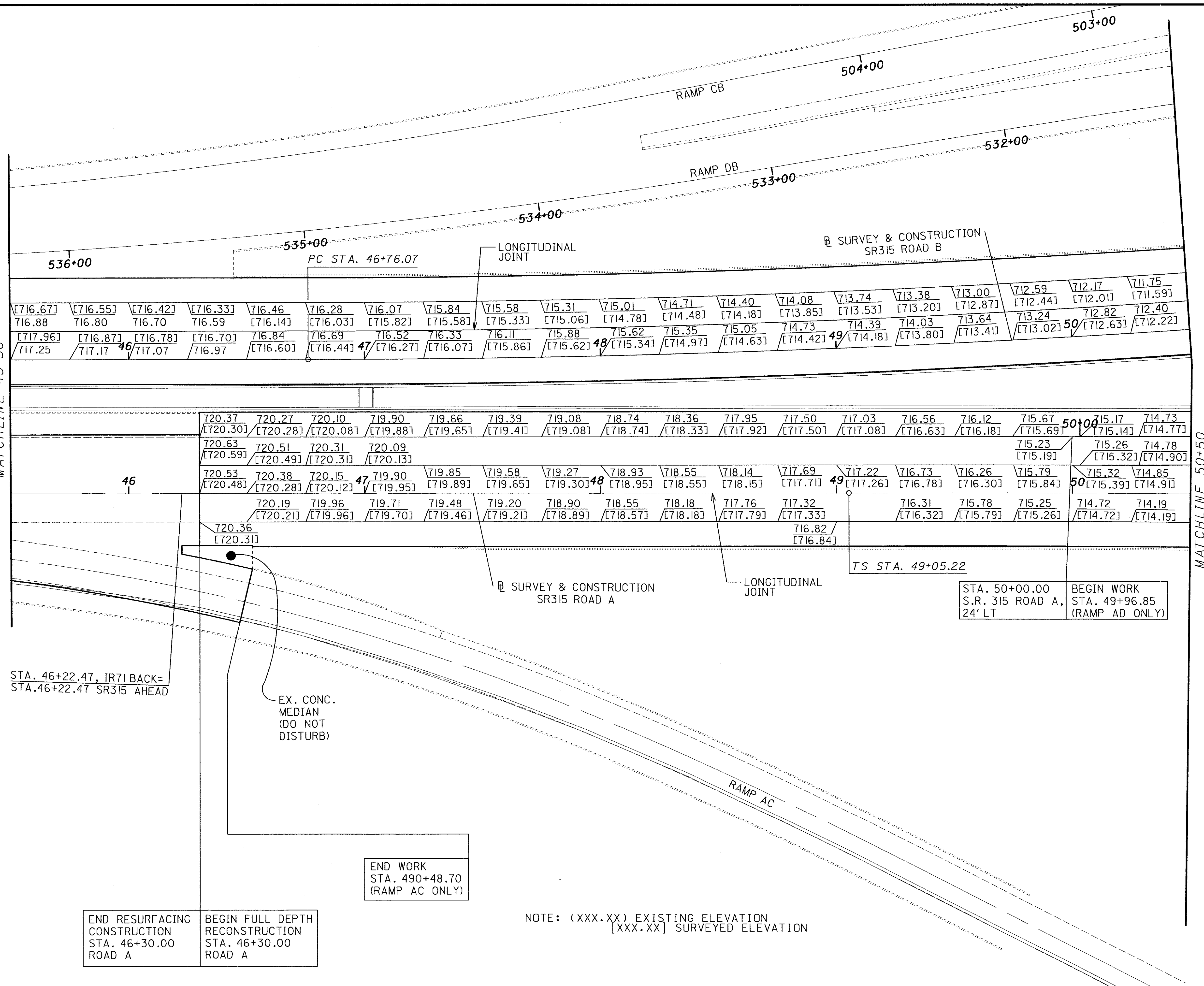






DATE: 05/16/2019 09:05:46 AM  
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MATCHLINE 45+50



NOTE: (XXX.XX) EXISTING ELEVATION  
[XXX.XX] SURVEYED ELEVATION



40  
20  
0  
HORIZONTAL  
SCALE IN FEET

CALCULATED  
BBB  
CHECKED  
LJS

PAVEMENT DETAILS  
S.R. 315 - STA. 45+50.00 TO STA. 50+50.00

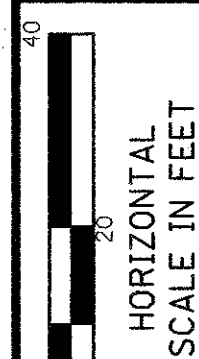
FRA-171-14.39,  
FRA-315-0.00 (A/B)

47  
89







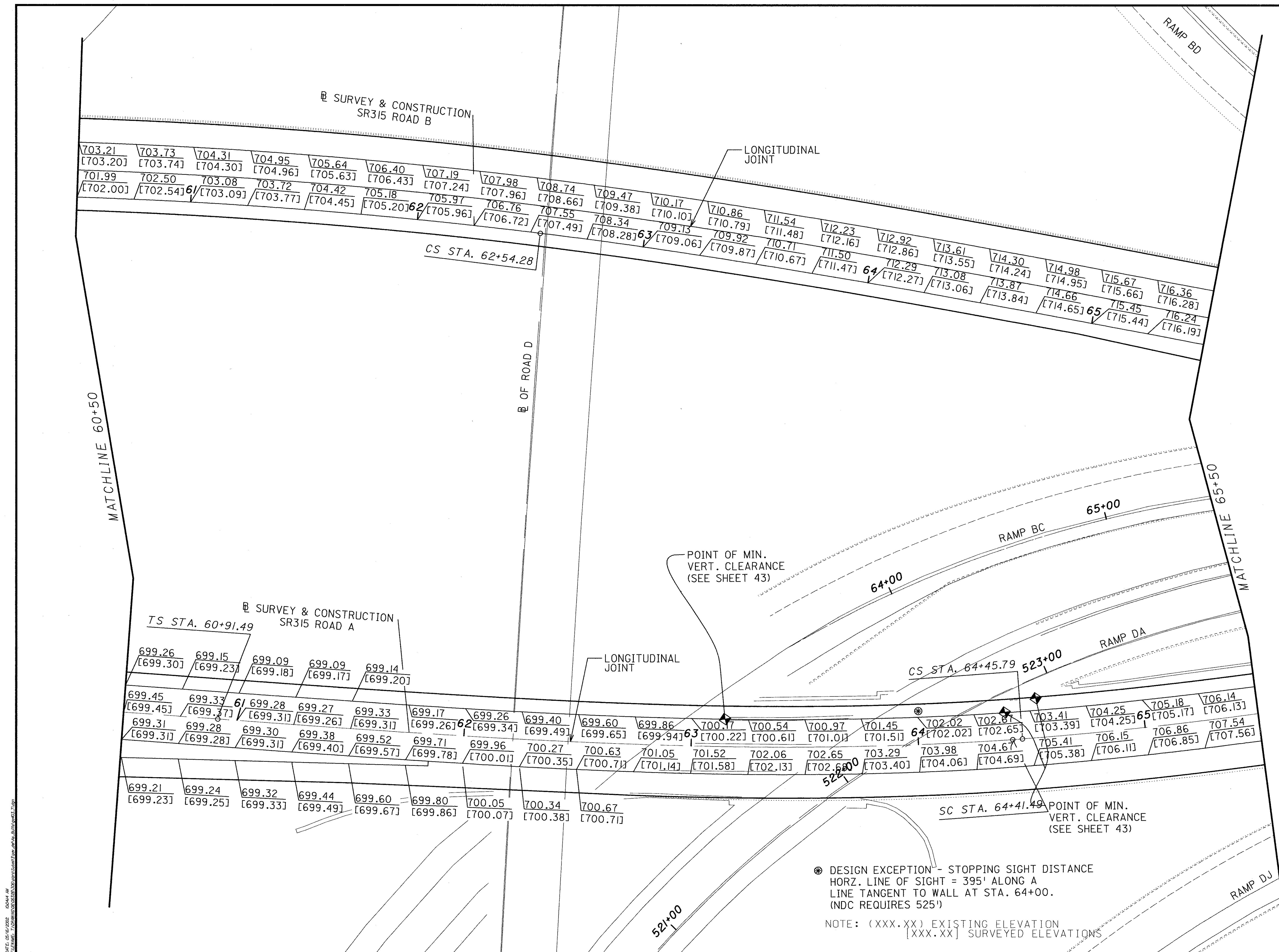


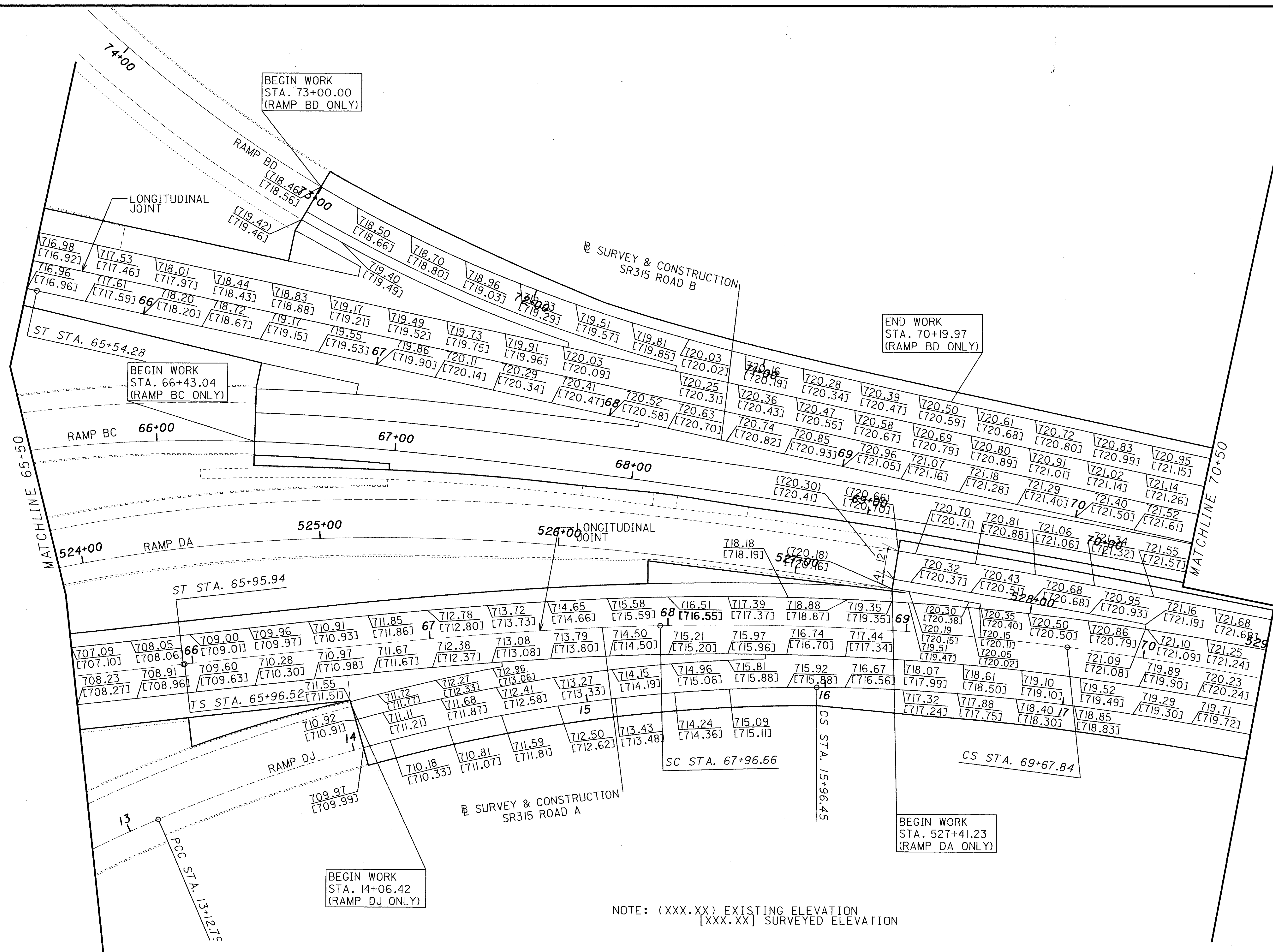
ALCULATED	CHECKED
BBB	LJS

**PAVEMENT DETAILS**  
**S.R. 315 - STA. 60+50.00 TO STA. 65+50.00**

**FRA-171-14.39,**  
**FRA-315-0.00 (A/B)**

50  
89





NOTE: (XXX.XX) EXISTING ELEVATION  
[XXX.XX] SURVEYED ELEVATION

HORIZONTAL  
SCALE IN FEET

CALCULATED	BBB
CHECKED	LJS

**PAVEMENT DETAILS**  
**S.R. 315 - STA. 65+50.00 TO STA. 70+50.00**

**FRA-171-14.39,**  
**FRA-315-0.00 (A/B)**

$$\frac{51}{89}$$



HORIZONTAL  
SCALE IN FEET

CALCULATED <b>BBB</b>	CHECKED <b>LJS</b>
--------------------------	-----------------------

**PAVEMENT DETAILS**

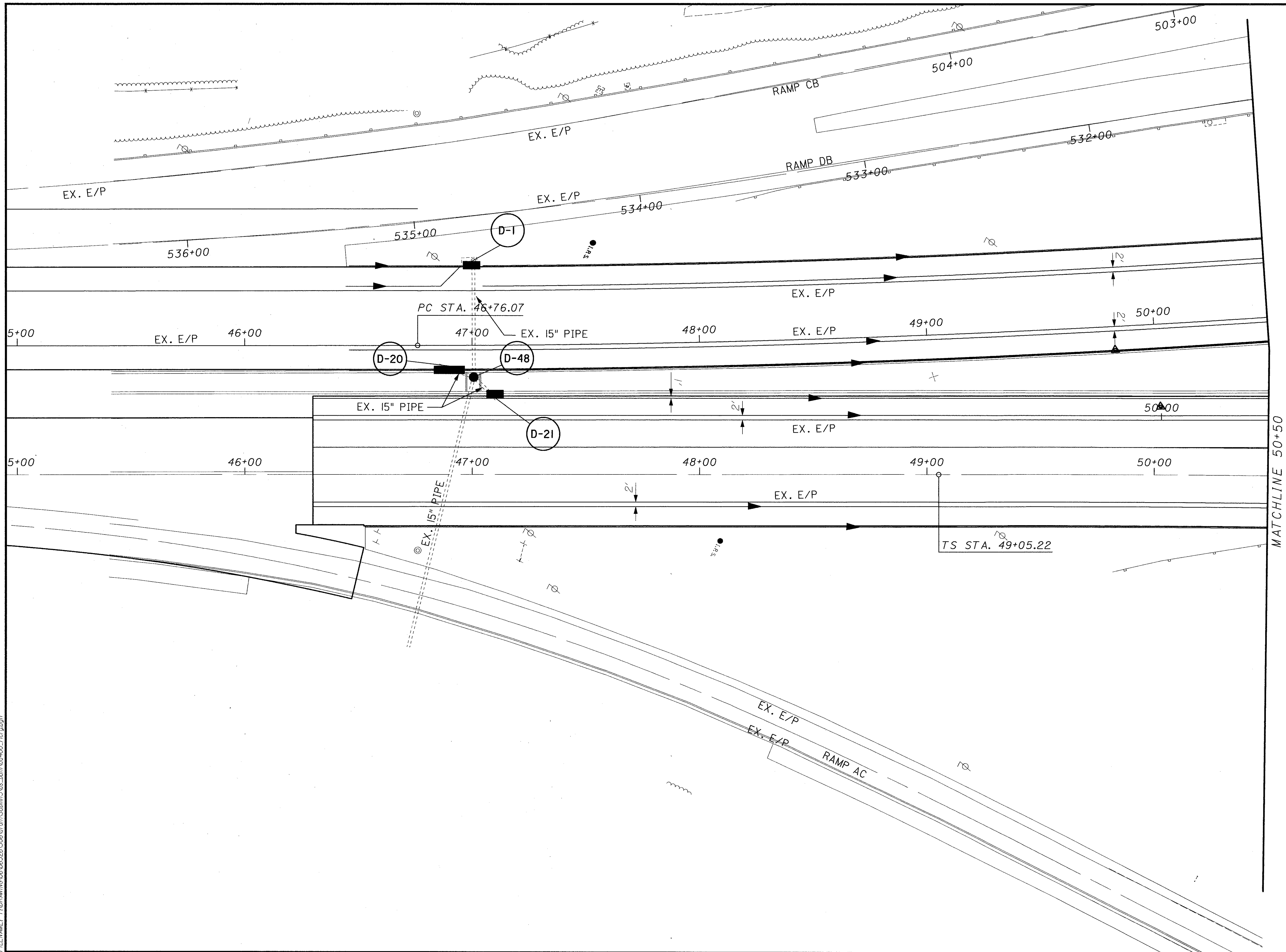
**S.R. 315 - STA. 70+50.00 TO STA. 72+00.12**

FRA -I71-14.39,  
FRA -315-0.00 (A / B)

52  
89



DATE: 05/16/2002 06:31:51 AM  
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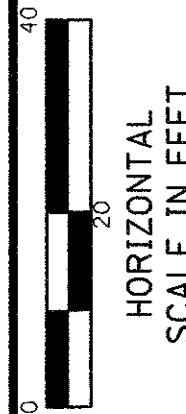
MATCHLINE 50+50

FRA-171-14.39,  
FRA-315-0.00 (A/B)

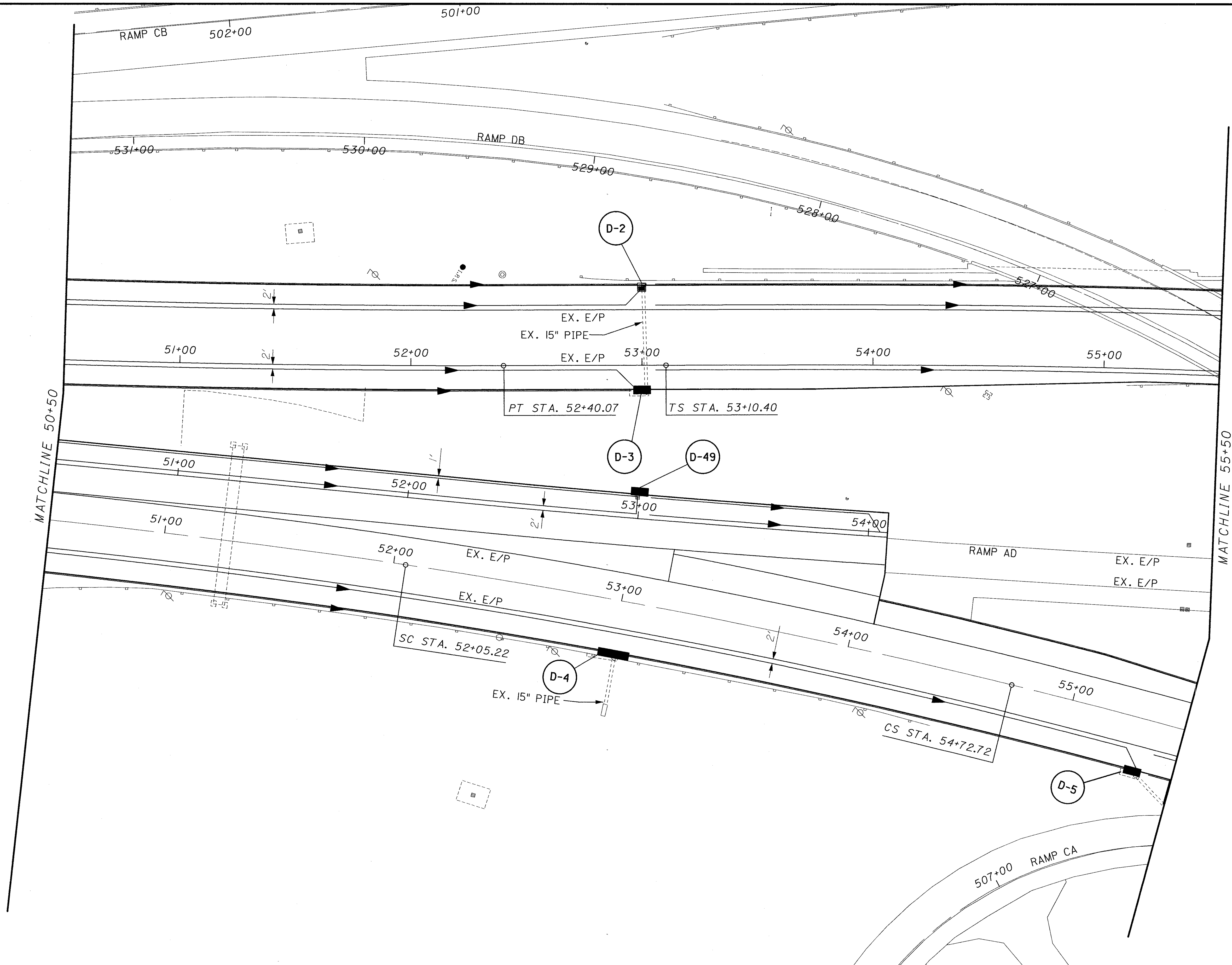
S.R. 315 - STA. 45+50.00 TO STA. 50+50.00

DRAINAGE DETAIL

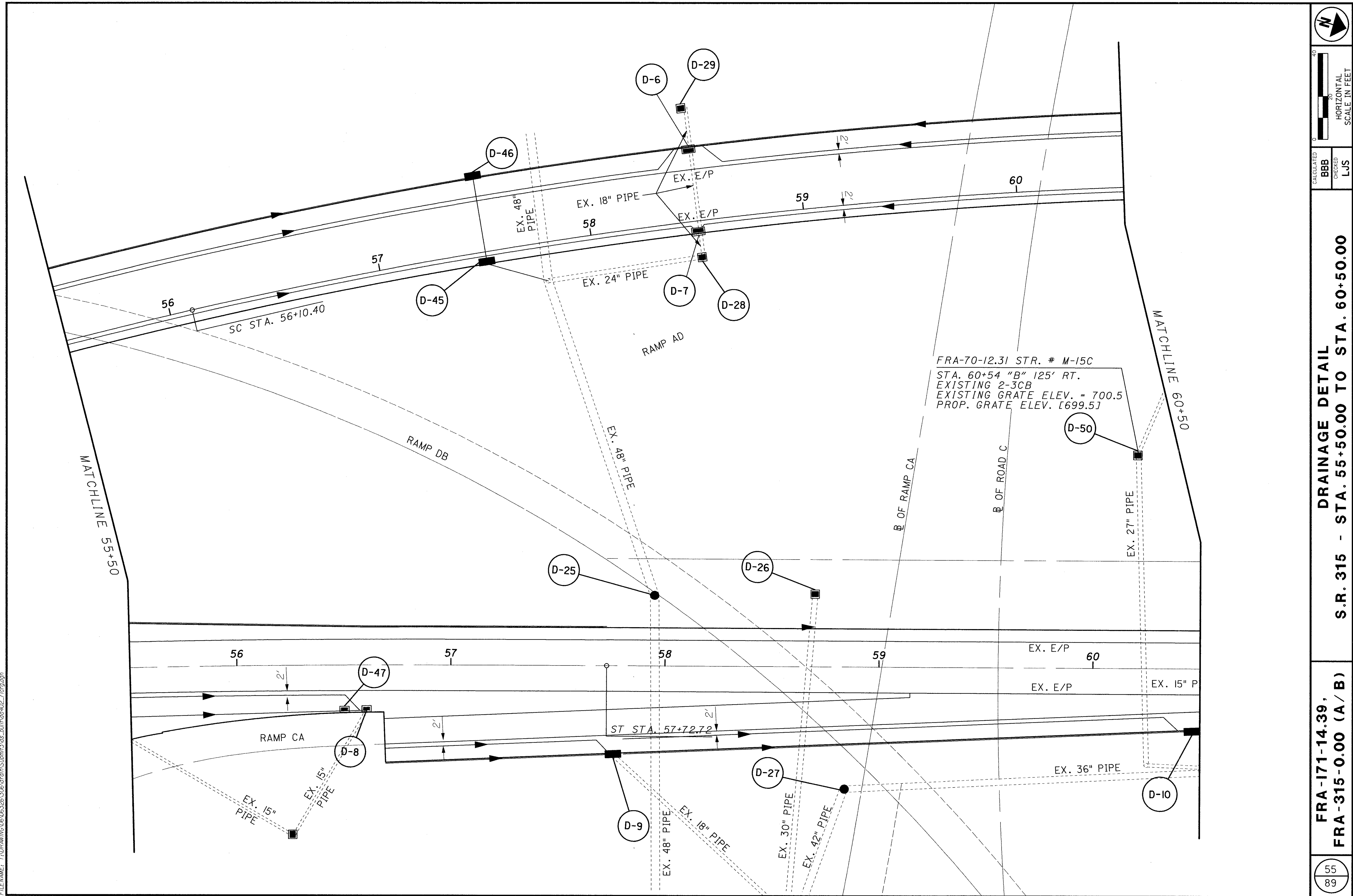
CALCULATED  
BBB  
CHECKED  
LJS



53  
89

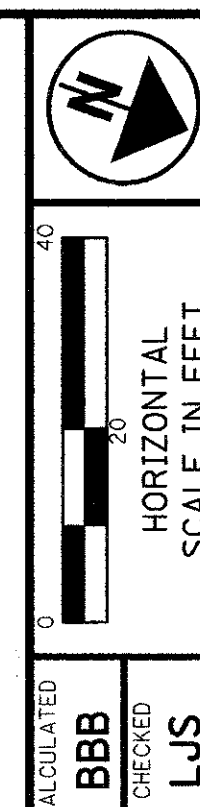


CALCULATED BBB	CHECKED LJS	FRA-171-14.39, FRA-315-0.00 (A/B)	S.R. 315 - STA. 50+50.00 TO STA. 55+50.00	DRAINAGE DETAIL	N 0 20 40 HORIZONTAL SCALE IN FEET
					54 89



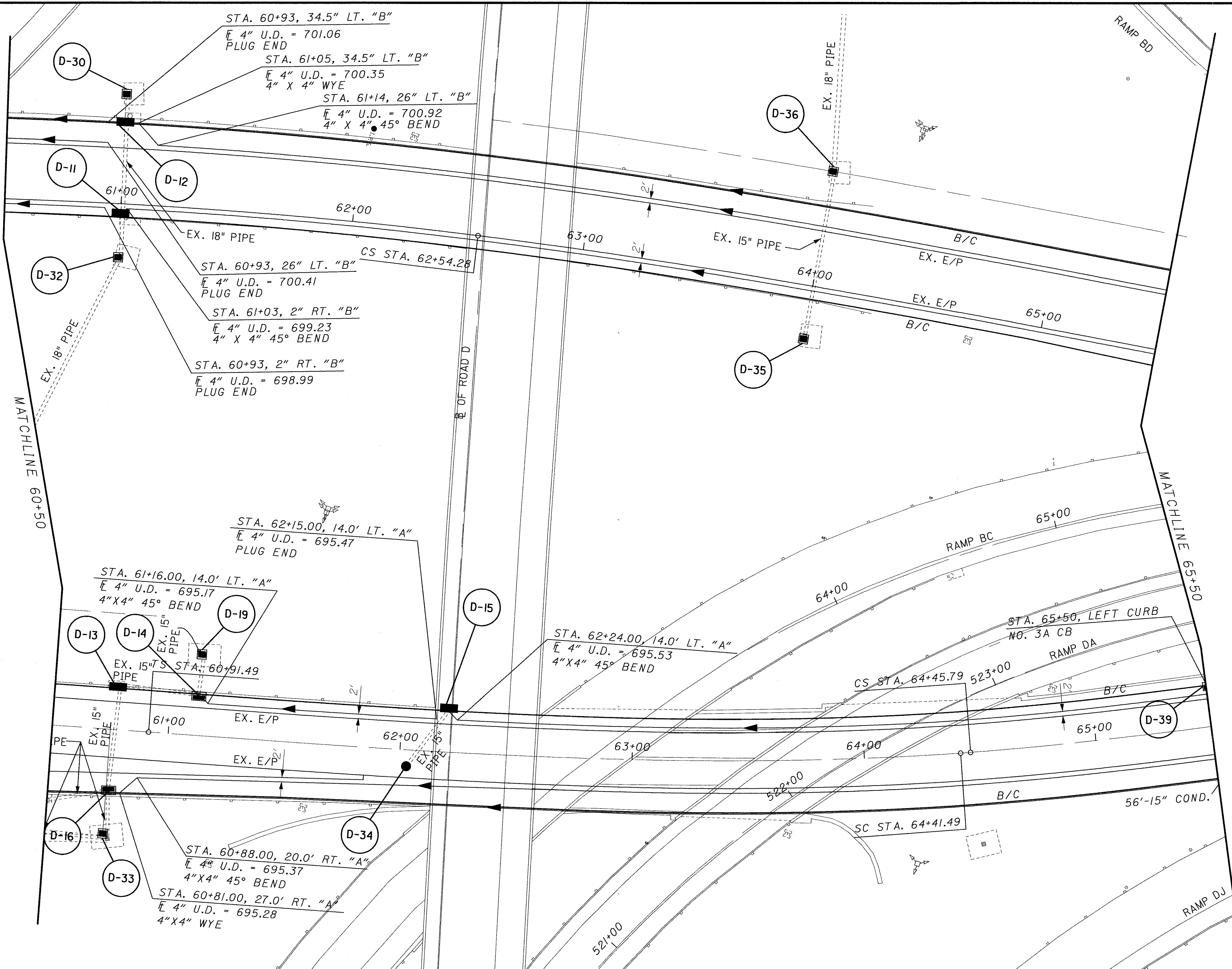

HORIZONTAL SCALE IN FEET	
CALCULATED BBB	CHECKED LJS
<b>FRA-171-14.39,          FRA-315-0.00 (A/B)</b>	
<b>S.R. 315 - STA. 55+50.00 TO STA. 60+50.00</b>	
<b>DRAINAGE DETAIL</b>	



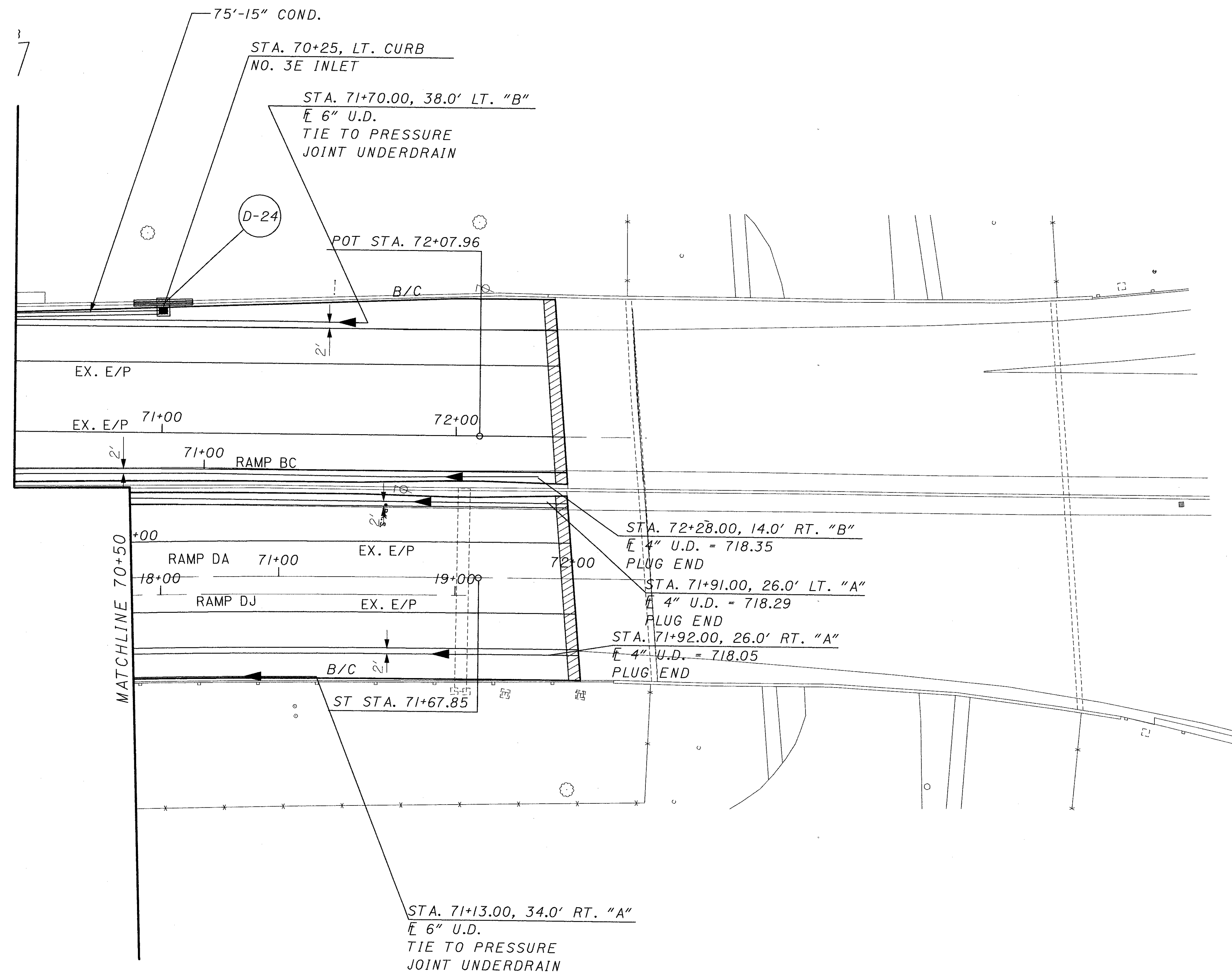


**DRAINAGE DETAIL**  
S.R. 315 - STA. 60+50.00 TO STA. 65+50.00

FRA -171-14.39,  
FRA -315-0.00 (A / B)



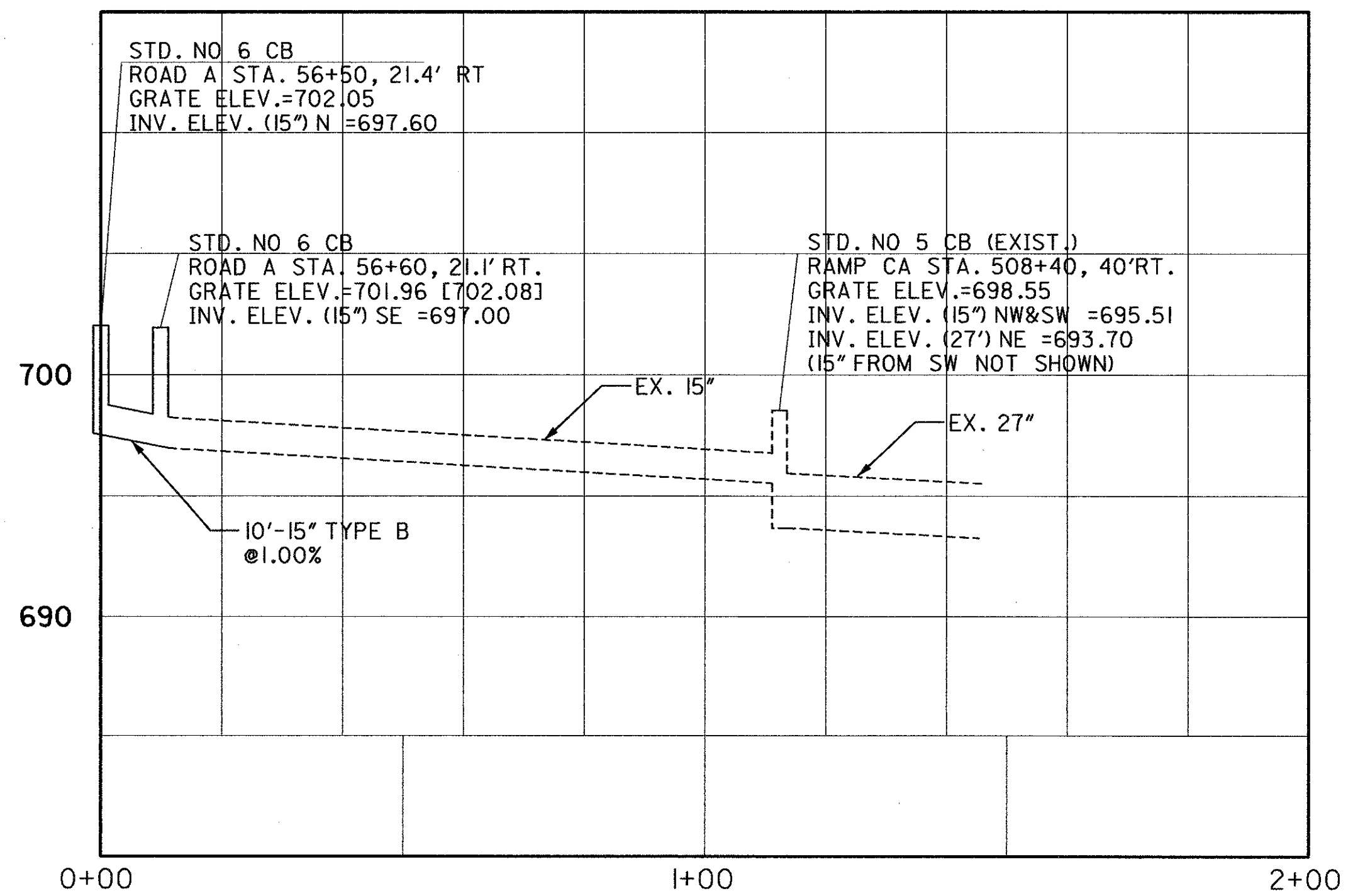
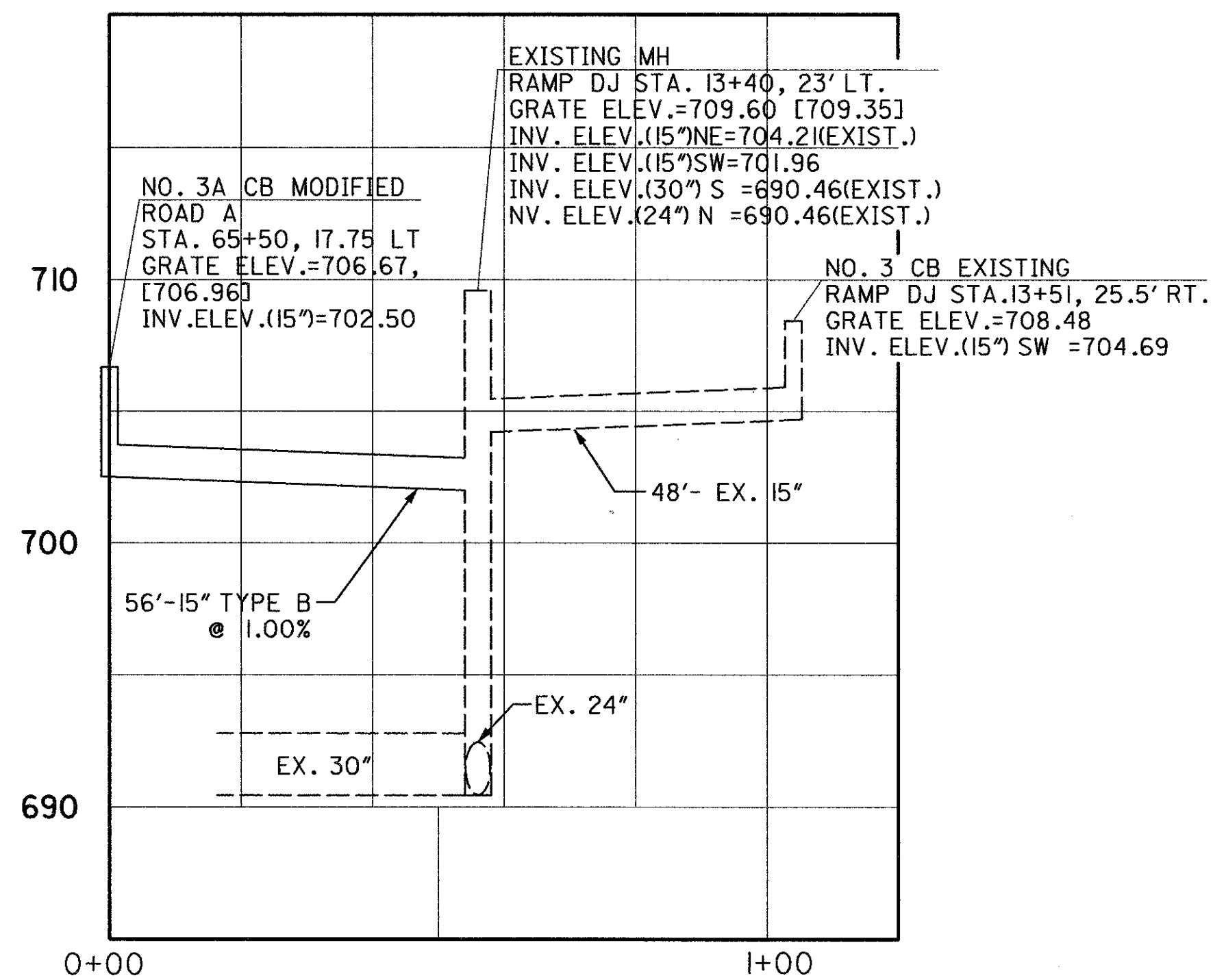
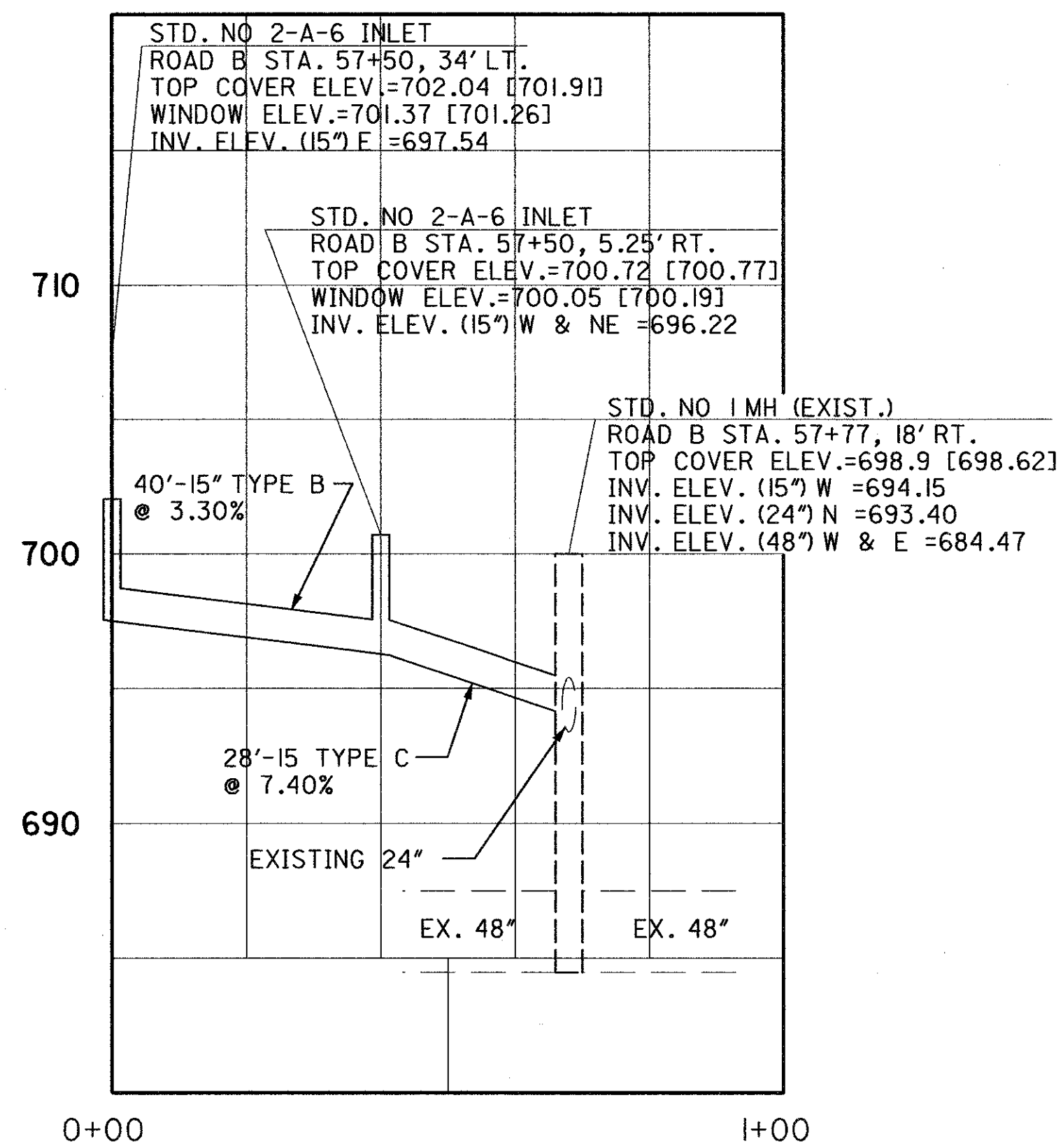




AS BUILTS, REPLACED DRAINAGE STRUCTURES			
ROAD	STATION	SIDE	STRUCTURE
ROAD A	53+00.00	RT.	I-2A
ROAD A	55+32.00	RT.	I-2A
ROAD A	56+60.00	RT.	CB-6
ROAD A	57+74.00	RT.	I-2A
ROAD A	53+00.00	RT.	CB-3
ROAD A	60+45.00	RT.	I-2A
ROAD A	60+77.77	LT.	I-2A
ROAD A	61+12.00	LT.	CB-3
ROAD A	62+20.00	LT.	I-2A
ROAD A	67+55.00	RT.	I-2A
ROAD A	16+65.00	RT.	I-2A
ROAD B	47+00.00	LT.	I-2A
ROAD B	53+00.00	LT.	CB-3A
ROAD B	53+00.00	RT.	I-2A
ROAD B	58+50.00	LT.	CB-3
ROAD B	58+50.00	RT.	CB-3
ROAD B	61+00.00	LT.	I-2A
ROAD B	61+00.00	RT.	I-2A



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[XXX.XX] = SURVEYED ELEVATION

# STORM PROFILES

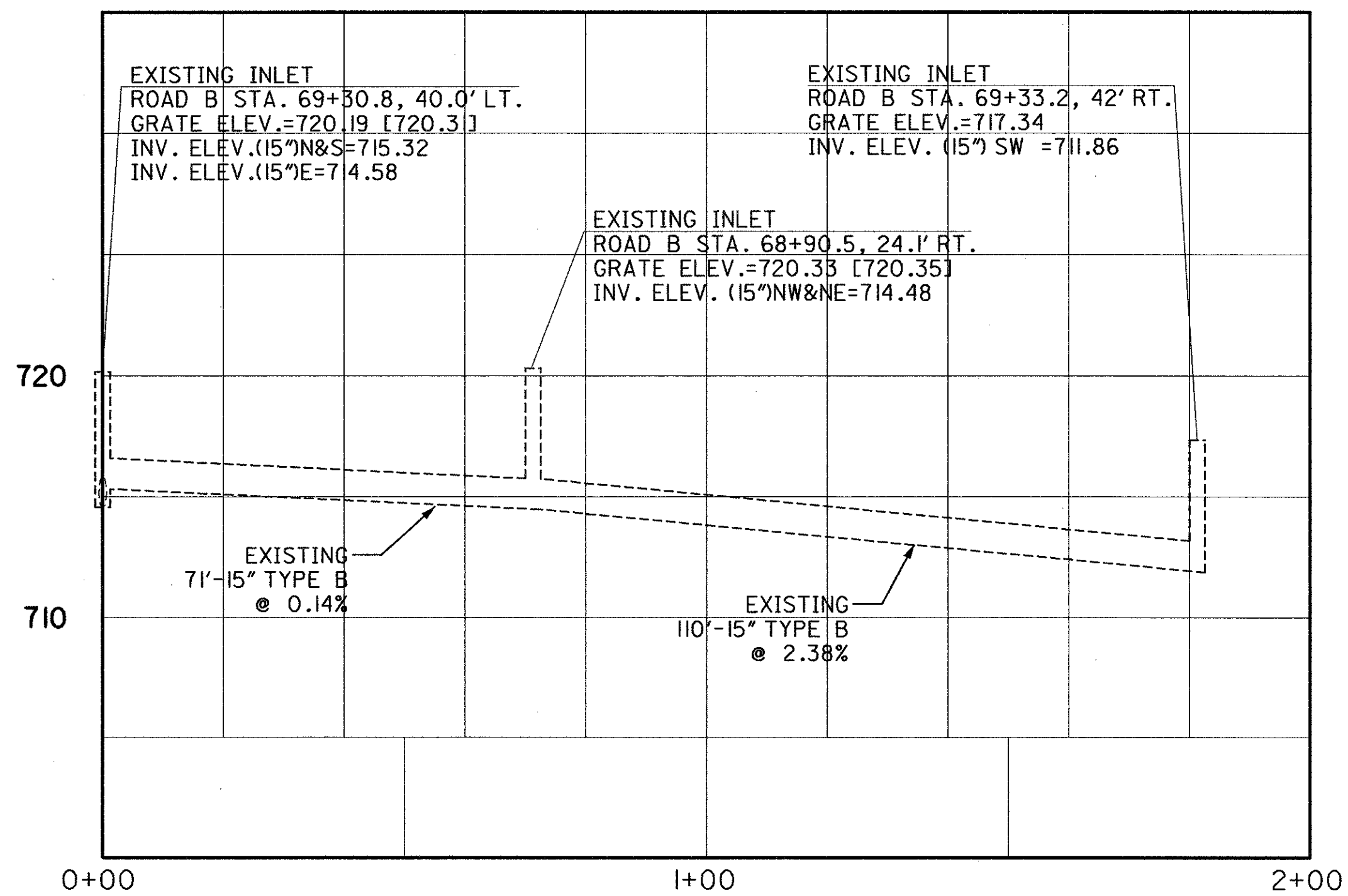
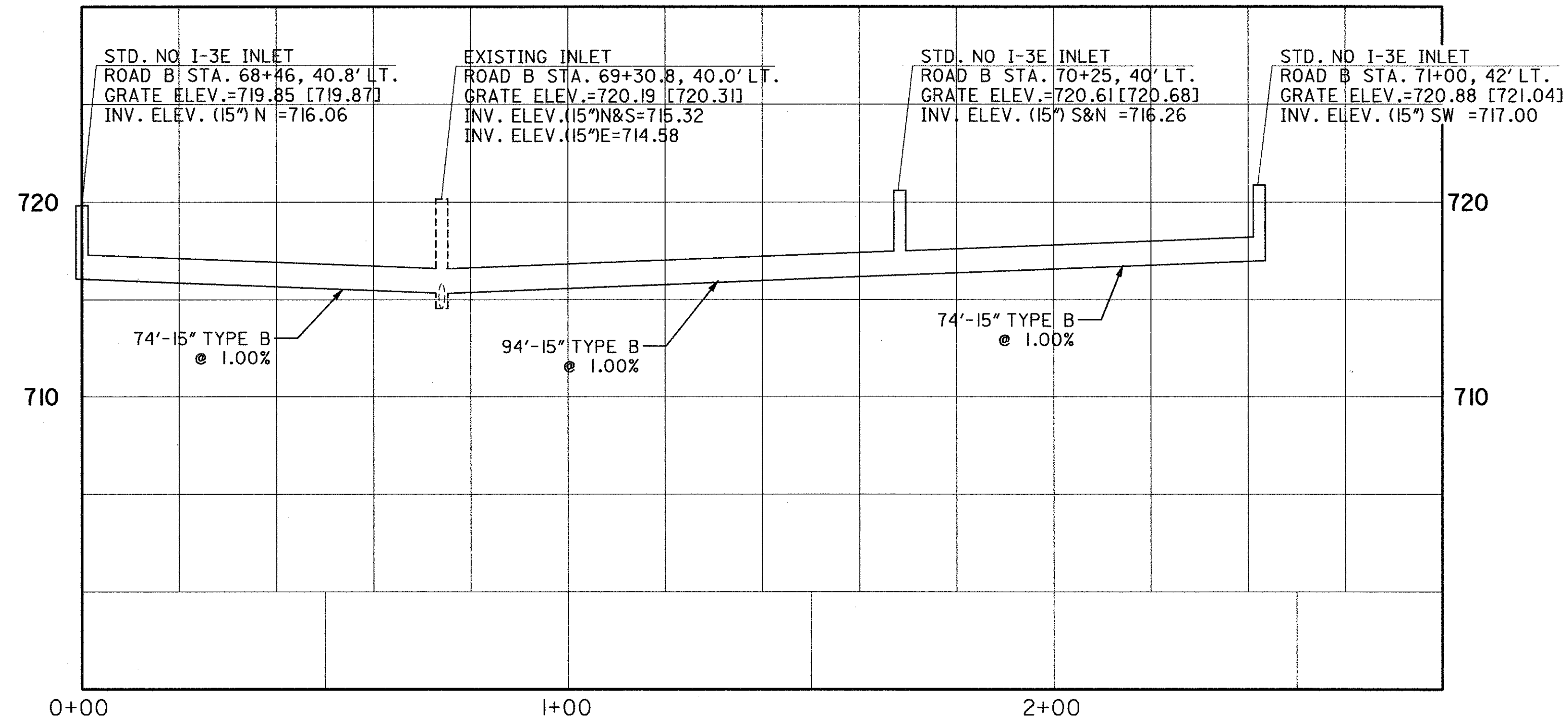
FRA-171-14.93,  
FRA-315-0.00 (A / B)

58A  
89

CALCULATED  
DAS  
CHECKED  
LJS

0 20 40  
HORIZONTAL  
SCALE IN FEET

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[XXX.XX] = SURVEYED ELEVATION



# STORM PROFILES

FRA-171-14.93,  
FRA-315-0.00 (A / B)

TOTAL FROM SHEET NUMBER																								ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SHT. NO.	CALCULATED KVM	CHECKED GSC																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																		



TOTAL FROM SHEET NUMBER	
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ITEM

ITEM  
EXT.

GRAND	
TOTAL	

UNIT

DESCRIPTION
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**SHT.**  
**NO.**

LCULATED

LCULATED

<div style="display: flex; justify-content: space-between; align-items: center;"> <div style="border: 1px solid black; border-radius: 50%; padding: 5px; text-align: center;"> <div style="display: flex; justify-content: space-between;"> <span>89</span> <span>60</span> </div> </div> <div style="text-align: center;"> <p><b>FRA -71-14.39 ,</b> <b>FRA -315-0.00</b></p> </div> </div>	<p><b>LIGHTING GENERAL SUMMARY</b></p>		<div style="display: flex; justify-content: space-between;"> <div> <p>CALCULATED KYM</p> </div> <div> <p>CHECKED GSC</p> </div> </div>

60 89	<b>FRA-71-14.39, FRA-315-0.00</b>
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ITEM 621, RAISED PAVEMENT MARKER, INSTALLATION ONLY

THE DEPARTMENT WILL SUPPLY THE RPM CASTINGS WITH THE ONE WAY WHITE, YELLOW/YELLOW, WHITE/RED, AND YELLOW/RED RETRO-REFLECTORS INSTALLED IN THE CASTINGS FOR ITEM 621 RAISED PAVEMENT MARKER, INSTALLATION ONLY. THE CONTRACTOR SHALL FURNISH ALL OTHER MATERIAL AND LABOR TO COMPLETE THE ITEM.

THE CONTRACTOR WILL BE INFORMED AT THE PRE-CONSTRUCTION CONFERENCE AS TO THE LOCATION IN COLUMBUS OF THE DEPARTMENT SUPPLIED RPM MATERIALS. WHEN SPECIFIED, ADDITIONAL RPM MATERIALS WILL BE STORED WITHIN THE DISTRICT FOR USE ON THIS PROJECT. THE CONTRACTOR SHALL PICK UP DEPARTMENT SUPPLIED MATERIALS AT THE SPECIFIED LOCATION(S) FOR TRANSPORT TO THE WORK SITE OR TO THE CONTRACTOR'S STORAGE FACILITY. AN AUTHORIZATION FOR PICK-UP FORM IS GIVEN IN SUPPLEMENTAL SPECIFICATION 1082 DATED JANUARY 11, 2000. THE CONTRACTOR SHALL NOTIFY THE DISTRICT AND/OR THE PARTIES LISTED ON THE AUTHORIZATION FORM IN WRITING AT LEAST 5 WORKING DAYS PRIOR TO THE PICK-UP OF DEPARTMENT SUPPLIED MATERIALS. THE MATERIALS SHALL BE STORED WITHOUT DAMAGE OR CONTAMINATION WITH FOREIGN MATTER. A DEDUCTION IN THE AMOUNT OF THE ACTUAL COST TO THE DEPARTMENT SHALL BE MADE FOR THE MATERIALS DAMAGED BY THE CONTRACTOR OR FOR CASTINGS RECEIVED BY THE CONTRACTOR WHICH WERE NOT INSTALLED OR WERE NOT RETURNED TO THE DEPARTMENT.

LOADING OF MATERIAL SUPPLIED BY THE DEPARTMENT AT THE RECYCLER'S WAREHOUSE SHALL BE DONE IN ACCORDANCE WITH SUPPLEMENTAL SPECIFICATION 1082.

ALL CASTINGS SHALL BE PLACED THE SAME WORKING DAY THAT THE RPM SLOTS ARE CUT INTO THE PAVEMENT.

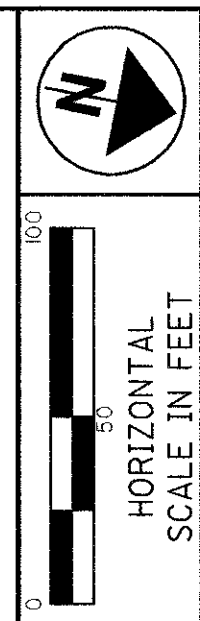
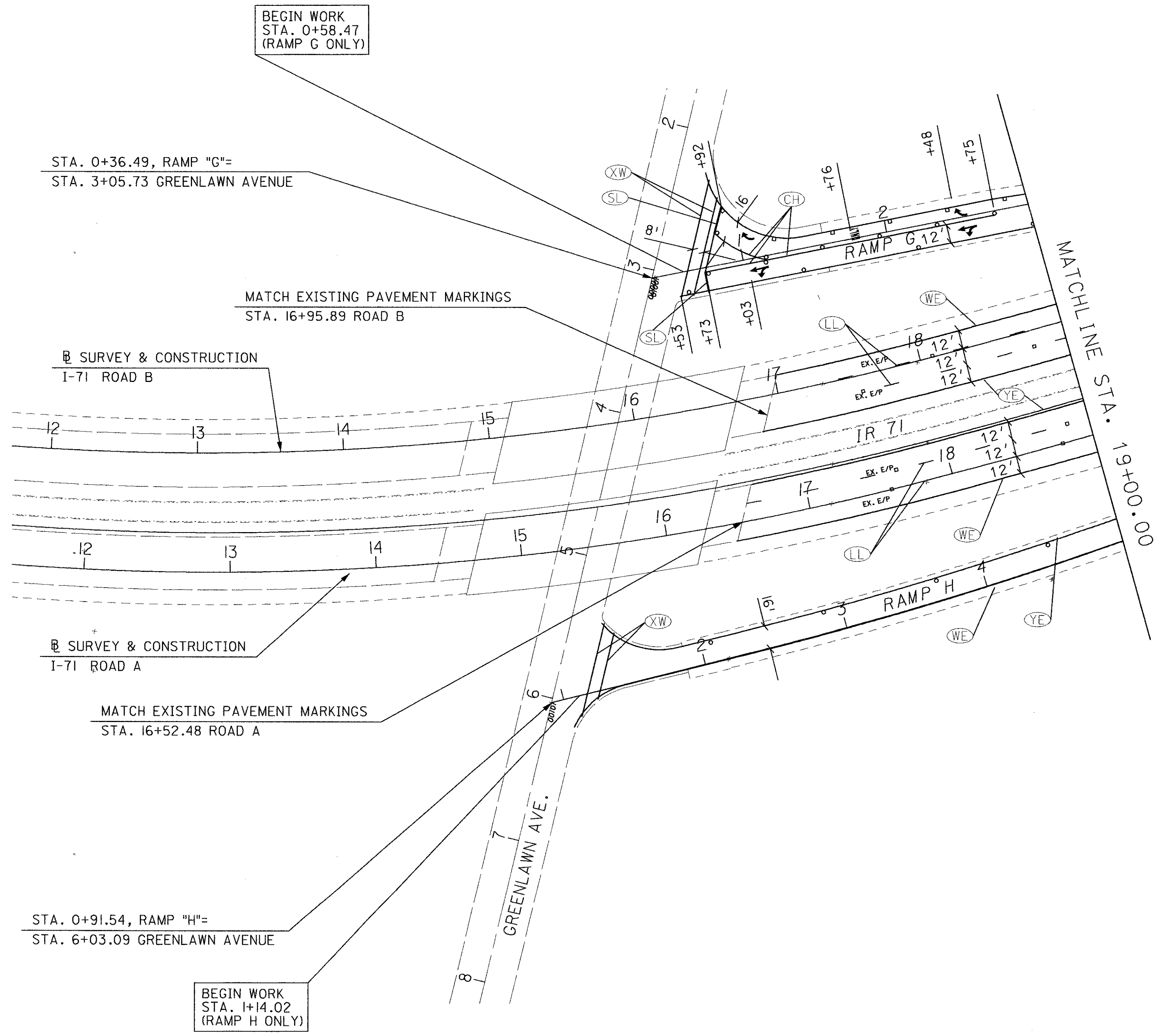
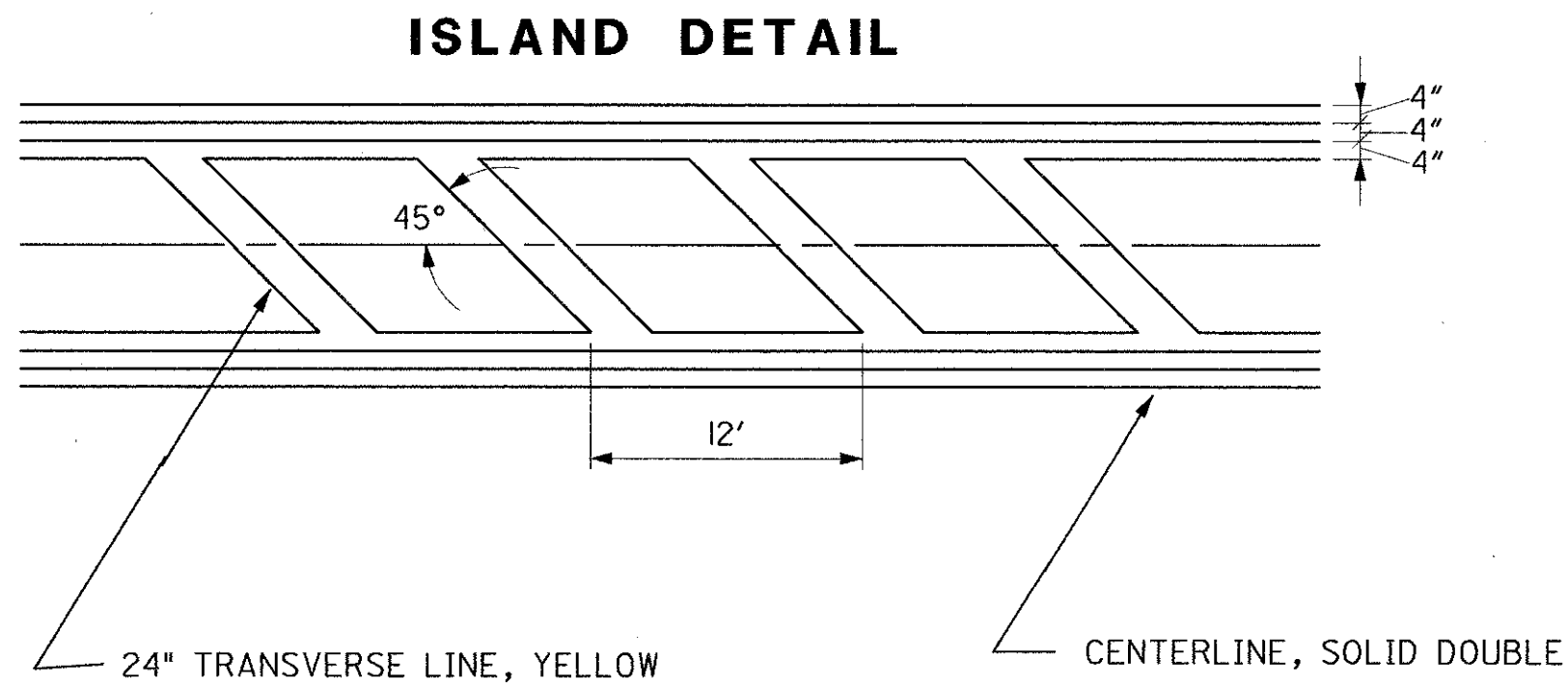
CONVERSION OF METRIC STANDARD DRAWINGS

THE METRIC STANDARD DRAWINGS REFERENCED IN THIS PLAN SHALL BE CONVERTED TO ENGLISH UNITS USING THE SI (METRIC) TO ENGLISH CONVERSION FACTORS PROVIDED IN SECTION 109.011 OF THE 1997 CONSTRUCTION AND MATERIALS SPECIFICATIONS. THE APPENDIX OF ASTM E 380 SHALL BE UTILIZED FOR ANY ADDITIONAL CONVERSION FACTORS REQUIRED. CONVERSIONS SHALL BE APPROPRIATELY PRECISE AND SHALL REFLECT STANDARD INDUSTRY ENGLISH VALUES WHERE SUITABLE.

CALCULATED	PAVEMENT MARKING GENERAL NOTES		FRA-71-14.39 FRA-315-0.00	<div>61 89</div>
CHECKED				

ITEM	ODOT LINE SPECIFICATIONS
644	
(WE)	EDGE LINE, WHITE
(YE)	EDGE LINE, YELLOW
(LL)	LANE LINE, 4"
(WD)	DOTTED LINE, 4" WHITE
(YD)	DOTTED LINE, 4" YELLOW
(DY)	CENTER LINE, SOLID DOUBLE
(BC)	CENTER LINE, DASHED SINGLE
(CL)	CENTER LINE, DASHED & SOLID DOUBLE
(CH)	CHANNELIZING LINE
(SL)	STOP LINE
(XW)	CROSSWALK LINE
(WT)	TRANSVERSE LINE, WHITE
(YT)	TRANSVERSE LINE, YELLOW
(WI)	ISLAND MARKING, WHITE
(YI)	ISLAND MARKING, YELLOW
(A)	LANE ARROW
(W)	WORD ON PAVEMENT, 72" ONLY
(S)	SCHOOL SYMBOL MARKING, 72"

RAISED PAVEMENT MARKER (RPM) LEGEND
□ RPM, WHITE, (ONE WAY)
○ RPM, WHITE/RED, (TWO WAY)
■ RPM, YELLOW, (ONE WAY)
● RPM, YELLOW/YELLOW, (TWO WAY)

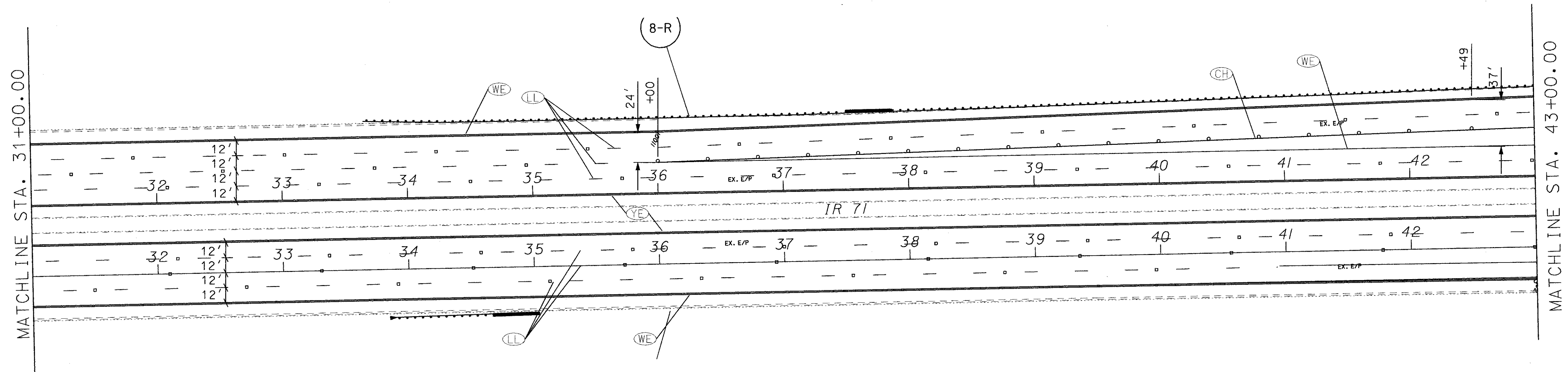
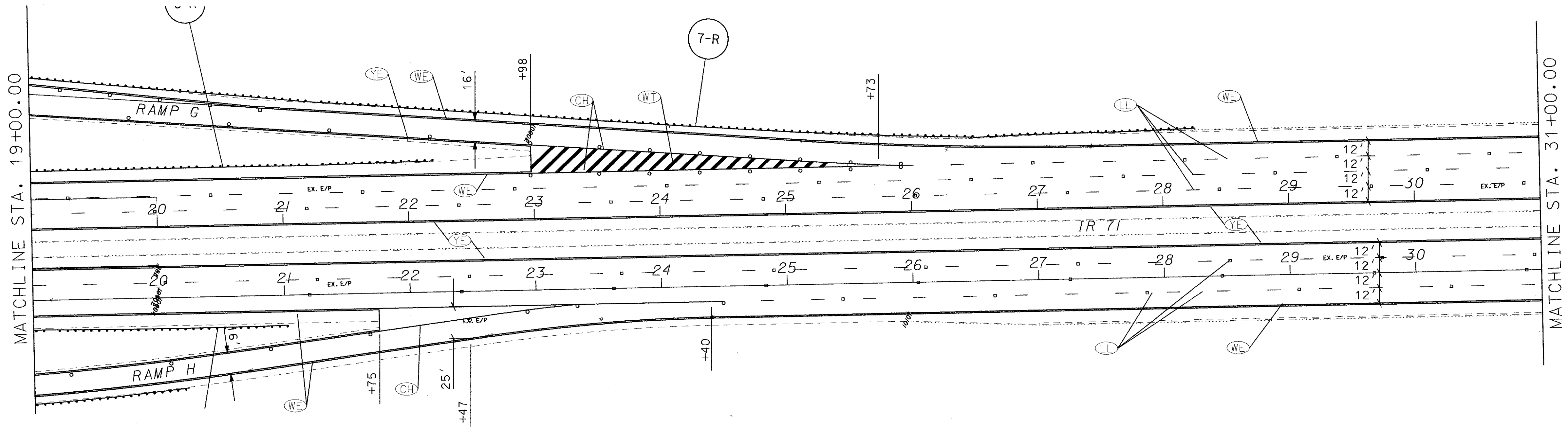


**PAVEMENT MARKING PLAN**  
**IR71 STA. 16+62.75 TO STA. 19+00.00**

**FRA-171-14.39,**  
**FRA-315-0.00**



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FOR PAVEMENT MARKING LEGEND  
SEE SHEET 62



HORIZONTAL  
SCALE IN FEET

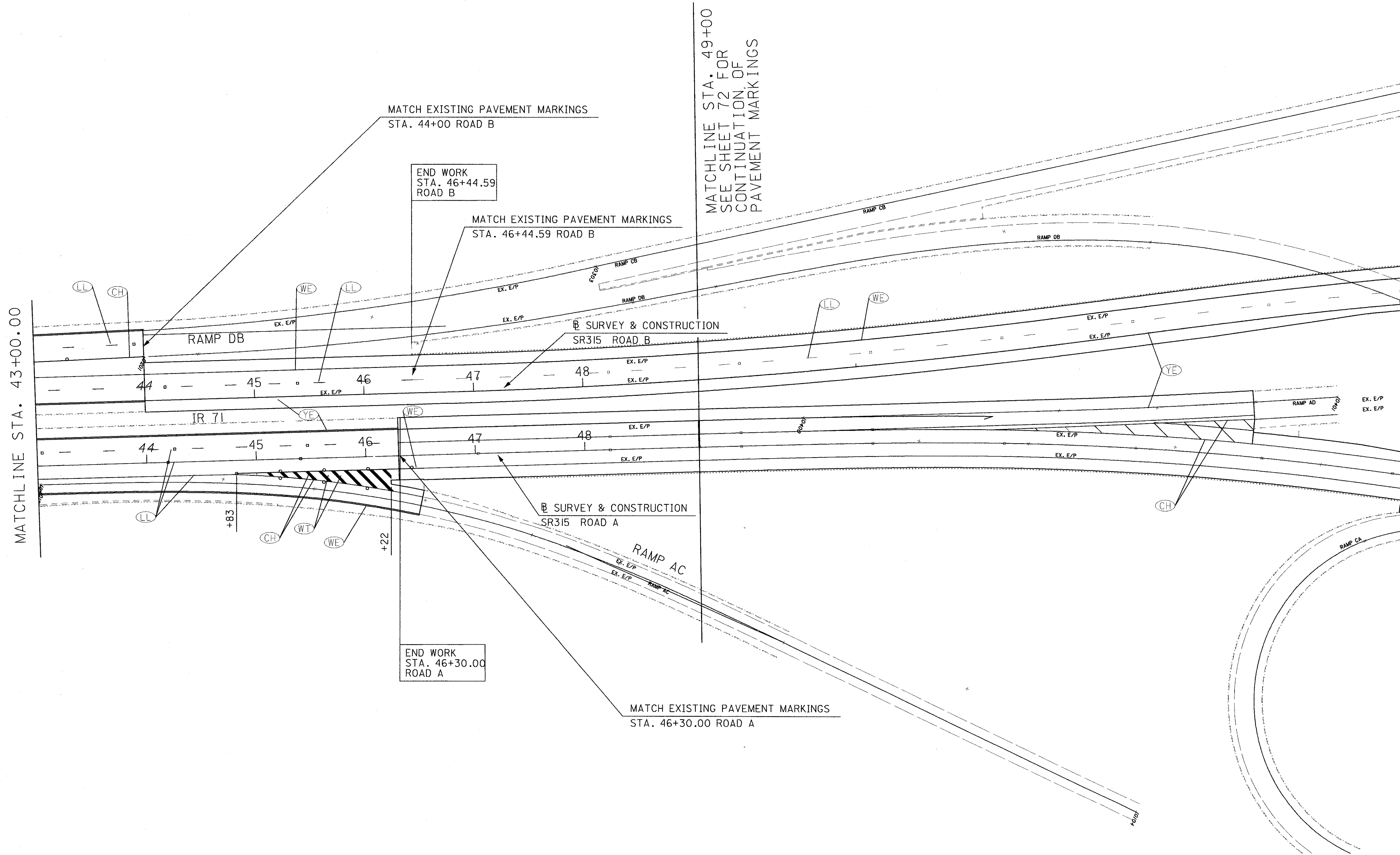
CALCULATED  
CHECKED

PAVEMENT MARKING PLAN  
IR 71 STA. 19+00.00 TO STA. 43+00.00

FRA-I71-14.39,  
FRA-315-0.00

63  
89

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FOR PAVEMENT MARKING LEGEND  
SEE SHEET 62

FRA-171-14.39,  
FRA-315-0.00  
IR71, SR315 STA. 43+00.00 TO STA. 46+52.35

64  
89



CALCULATED  
CHECKED

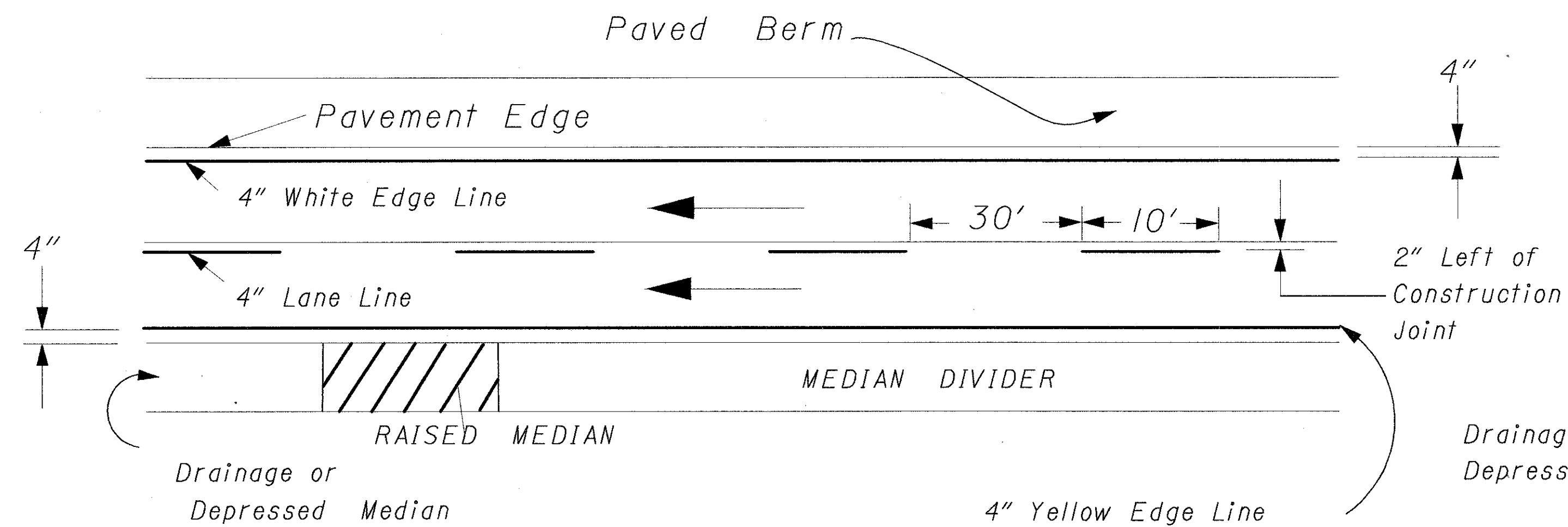
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SHEET NO.	COLUMN A		SIDE	<div><div>WE</div><div>YE</div><div>LL</div><div>CH</div><div>WT</div><div>YT</div><div>SL</div><div>XW</div><div>A</div><div>W</div></div>												62I	62I	62I
				EDGE LINE, WHITE	EDGE LINE, YELLOW	LANE LINE	CHANNELIZING LINE	TRANSVERSE LINE, WHITE	TRANSVERSE LINE, YELLOW	STOP LINE	CROSSWALK LINE	LANE ARROW	WORD ON PAVEMENT, 96"					
	FROM	TO		L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	EA	EA.					
	RAMP G															RAISED PAVEMENT MARKERS	RAISED PAVEMENT MARKERS	RAISED PAVEMENT MARKERS
62	0+53		LT/RT								90							
	0+61		LT/RT								70							
	0+73		RT							13								
	0+92		LT/RT				27			18								
	0+73	2+75	RT				202								6			
↓	0+92	I+20	LT/RT				40								2			
62	I+03		LT/RT								2							
	I+76		LT									1						
	I+48		LT/RT								2							
	0+75	2+98	RT		223													
↓	0+90	2+98	LT		208											3		
62	0+90	2+98	LT												6			
RAMP H																		
62	I+18		LT/RT								60							
↓	I+27										44							
	I+30	4+97	RT	367														
62	I+45	4+97	LT		352											4		
MAINLINE																		
62	16+46	19+00	RT	254														
	16+52	19+00	RT			248								2				
↓	16+57	19+00	RT			243								2				
62	16+63	19+00	RT		237													
62	16+86	19+00			214													
	16+91	19+00				209								2				
↓	16+95	19+00				205								2				
62	17+02	19+00		198														
RAMP G																		
63	2+98	6+98	LT/RT	400	400										5	5		
RAMP H																		
63	4+97	7+75	LT/RT	278	278											4		
MAINLINE																		
63	19+00	31+00	RT	1200	1200	2400								20				
	21+75	24+40	RT				265								5			
↓	21+75	23+10	RT	135														
63	24+40	31+00	RT			660								5				
63	19+00	31+00	LT	1200	1200	2400								20				
↓	22+98	25+73	LT				550	326							15			
63	25+73	31+00	LT			527								4				
63	31+00	43+00	RT	1200	1200	3600								30				
63	31+00	43+00	LT	1200	1200	2400								20				
↓	31+00	36+00	LT			500								4				
63	36+00	43+00	LT	700			700								17			
TOTAL COLUMN A				7132	6712	13392	1784	326	-	31	264	4	1	-	111	56	16	

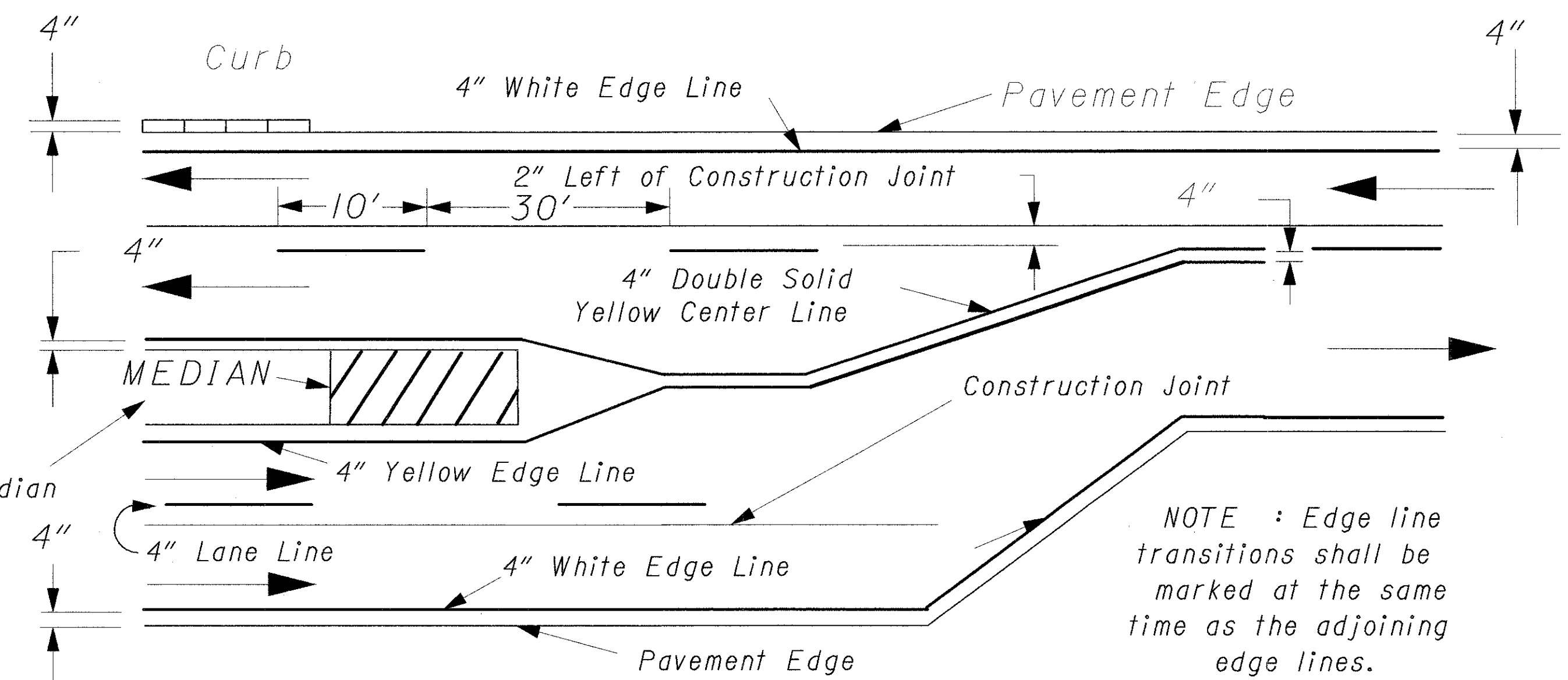
SHEET NO.	COLUMN B		STATION	SIDE																		
	FROM				TO		EDGE LINE, WHITE L.F.	EDGE LINE, YELLOW L.F.	LANE LINE L.F.	CHANNELIZING LINE L.F.	TRANSVERSE LINE, WHITE L.F.	TRANSVERSE LINE, YELLOW L.F.	STOP LINE L.F.	CROSSWALK LINE L.F.	LANE ARROW EA	WORD ON PAVEMENT, 96" EA.	RAISED PAVEMENT MARKERS W		RAISED PAVEMENT MARKERS W/R		RAISED PAVEMENT MARKERS Y/R	
																	EACH		EACH		EACH	
MAINLINE																						
64	43+00	67+75	RT	2153																		
	43+00	72+00	RT		2578																	
	43+00	72+00	RT			2578										22						
	43+00	51+73	RT			551										4						
	43+00	44+83	RT			183										1						
	44+83	46+22	RT				278	155										7				
	46+22	46+30	RT		8																	
	43+00	44+00			100		100	100								1			1			
	43+00	72+37	LT		2593																	
	43+00	72+37	LT			2593																
▼	43+00	72+37	LT			2593										22						
72	51+73	54+09	RT				472	250										12				
72	55+50	58+30	RT				280											8				
72	56+68	58+30	RT		162																	
72	58+30	59+60	RT			130										2						
73	66+43	69+55	LT				590	300										15				
	67+90	71+00	RT				572											16				
	68+02	72+37	LT			435										3						
	68+60	72+00	RT			340										2						
▼	69+55	72+37	LT			282										2						
73	71+00	72+00	RT			100																
RAMP BD																						
73	71+55	73+00	LT				296	160										8				
RAMP DJ																						
73	14+06	16+00	RT				194											7				
73	14+06	72+00	RT		530																	
TOTAL COLUMN B					5546	5171	7292	2782	865	-	-	-	-	-	-	59	73	1				
FROM COLUMN A					7132	6712	13392	1784	326	-	31	264	4	1	-	111	56	16				
TOTAL (FT)/(EACH)					12678	11883	20684	4566	1191	-	31	264	4	1	-	170	129	17				
COMBINED TOTAL (FT)/(EACH)					24561		20684	4566	1191	-	31	264	4	1	-	316						
TOTAL (MILES)					4.652		3.917															
FRA-IR71-14.39, FRA-315-0.00																						
65 89																						



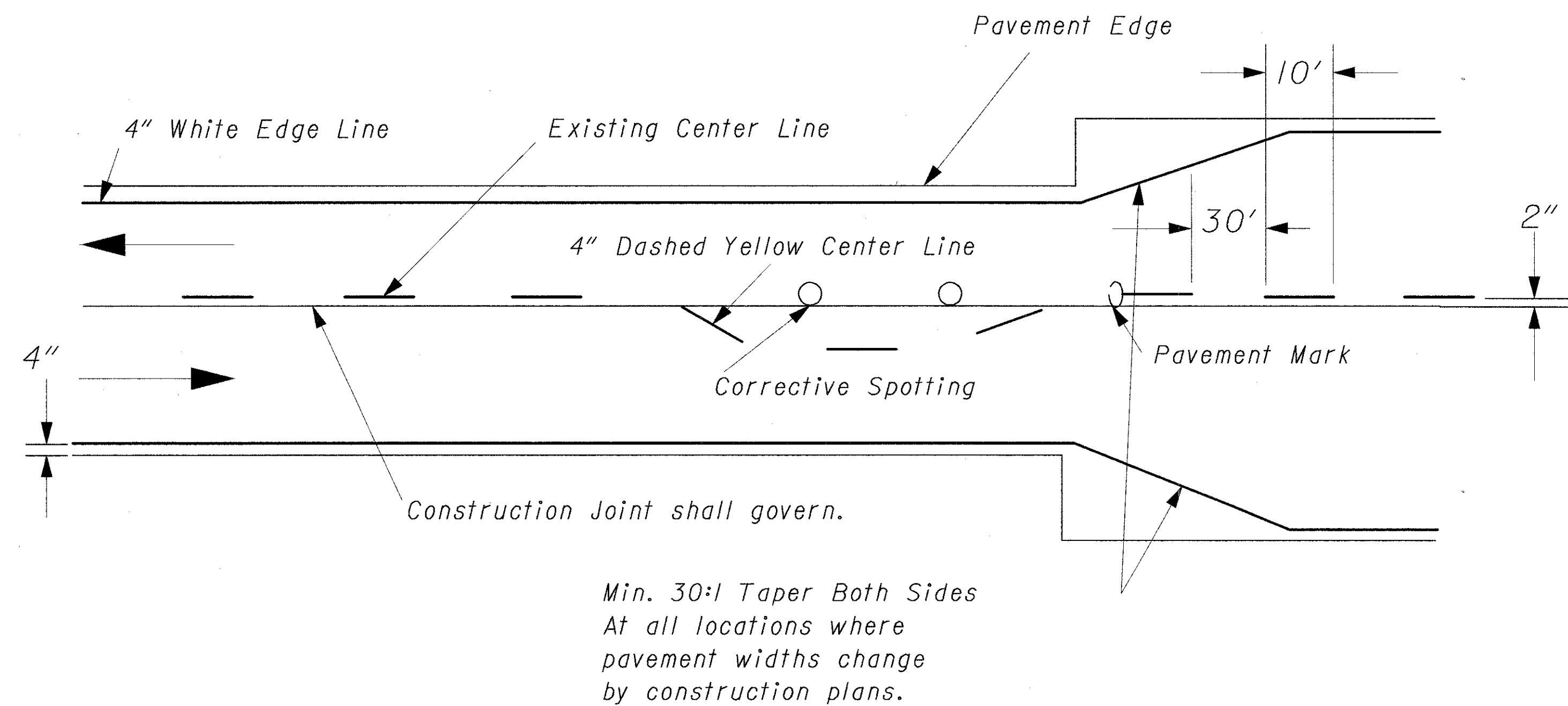
## FREEWAY & EXPRESSWAY MAINLINE MARKINGS



## MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS

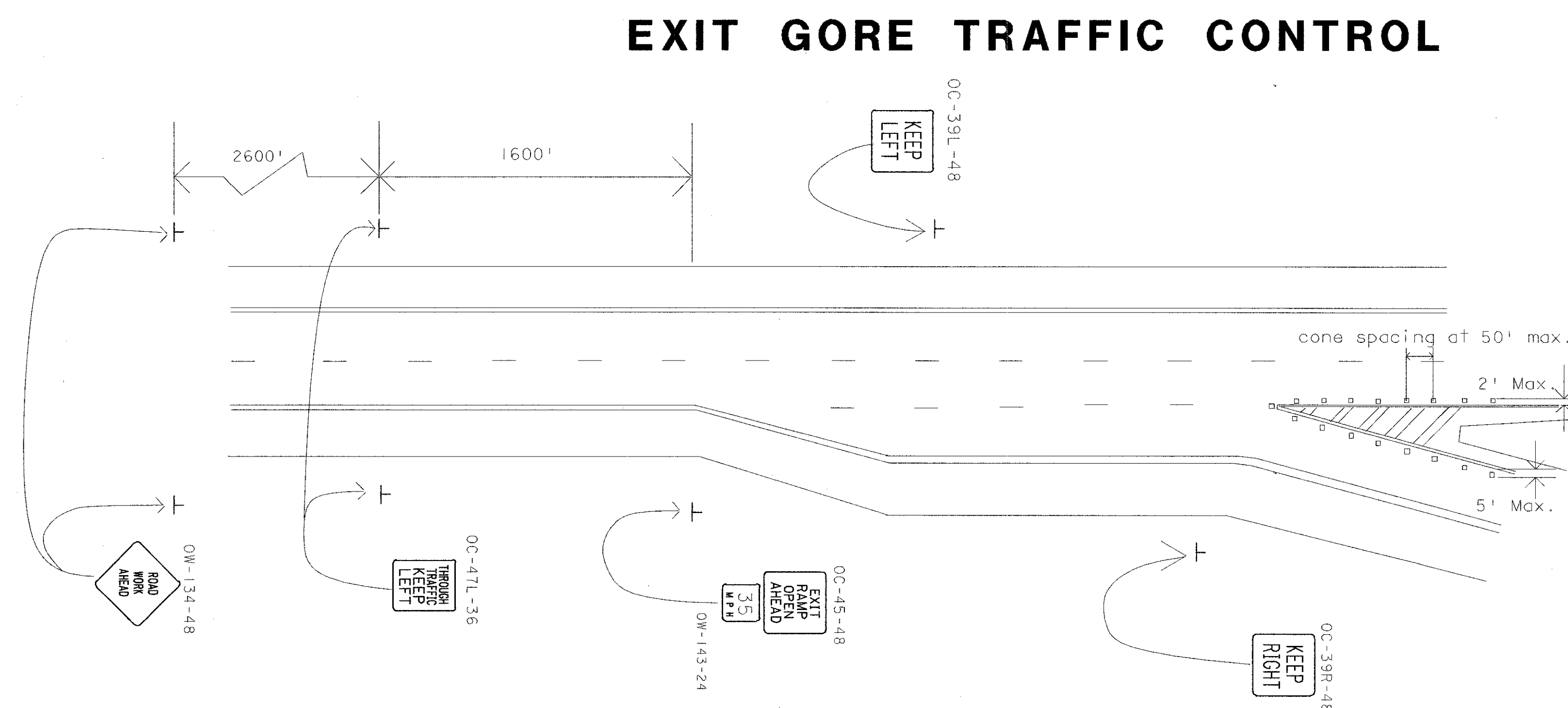
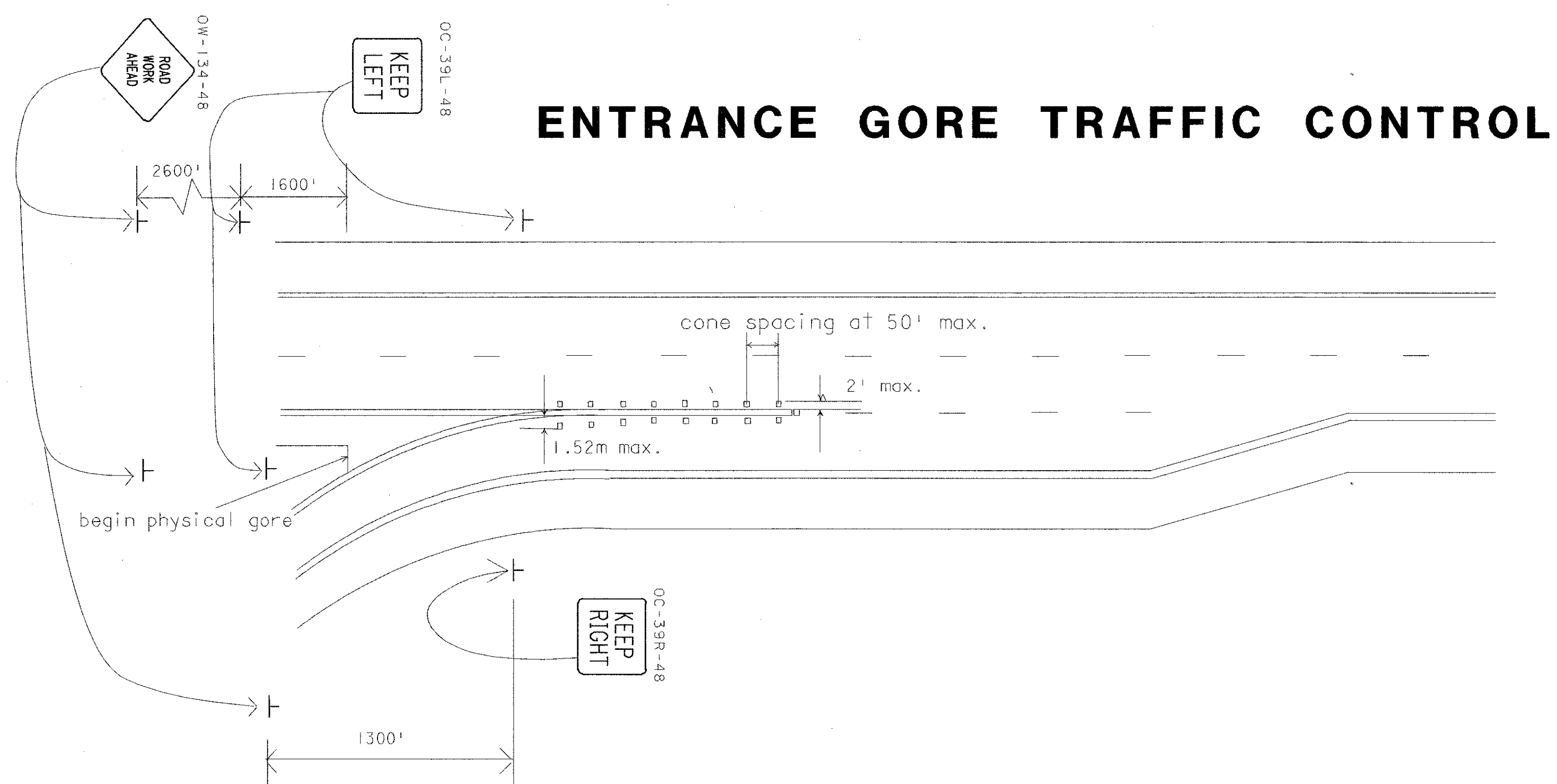


## TWO LANE MARKINGS



### NOTES :

1. The distance from the pavement edge to the nearside edge of the edgeline may be increased with the approval of the engineer in order to maintain uniform lane width.
2. See TC-72.20M for entrance and exit ramp markings.
3. The cycle length for dashed lines shall be 10 feet plus or minus 6". The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.



## GENERAL NOTES

1. The spacing between signs shown on this detail may be adjusted (increased or decreased) with the approval of the Engineer to position them no closer than 200 feet to existing signs which must remain in use.
2. At an isolated entrance gore area, a flashing arrow panel conforming to requirements in section 7G-8 of the OMUTCD and SCD MT-35.10 may be substituted for the advance OC-39 signs.
3. At an interchange where both exits and entrances are marked with traffic control in place at the same time, the OW-134 sign on the entrance ramp is not required.
4. Type A flashing warning lights are required on the OC-47L signs for all night closures.

PROTECTIVE COATING OF OVERHEAD  
SIGN SUPPORT SECTIONS

GENERAL

OVERHEAD SIGN SUPPORTS CAN BE SEPARATED INTO MAJOR SECTIONS SUCH AS END FRAMES, TRUSSES, VERTICAL POLES AND CANTILEVER ARMS. FOR THE IMPLEMENTATION OF THIS WORK ITEM IT WILL BE BENEFICIAL TO REFER TO THE MAJOR SECTIONS OF THE OVERHEAD SIGN SUPPORTS RATHER THAN THE WHOLE SUPPORT. MORE SPECIFIC INSTRUCTIONS AND FLEXIBILITY CAN BE GIVEN BASED UPON THE UNIT OF MEASURE AND PAYMENT PER MAJOR SUPPORT SECTION.

THE PROTECTIVE COATING OF OVERHEAD SIGN SUPPORT SECTIONS SHALL BE A FOUR PART PROCESS TO INCLUDE SURFACE PREPARATION FOLLOWED BY A THREE COAT PAINT SYSTEM. THIS THREE COAT SYSTEM SHALL CONSIST OF AN EPOXY PRIME COAT, AN EPOXY INTERMEDIATE COAT AND A URETHANE TOP COAT, WITH EACH COAT BEING A DIFFERENT COLOR. FOR AN EXPLANATION OF THE MATERIALS TO BE USED SEE NOTE ENTITLED "COATING SYSTEM." THE PURPOSE OF THIS COATING IS TO PROVIDE PROTECTION FOR NEW (UNWEATHERED) AND OLDER (WEATHERED) GALVANIZED STEEL SUPPORT SECTIONS FROM CORROSIVE ELEMENTS IN THE ATMOSPHERE. COATING AND SURFACE PREPARATION OF NEW GALVANIZED SUPPORT SECTIONS SHOULD BE DONE BY THE MANUFACTURER.

IN THE FIELD, THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO COMPLY WITH POLLUTION LAWS, RULES OR REGULATIONS OF FEDERAL, STATE OR LOCAL AGENCIES. THE COATING MATERIALS SPECIFIED FOR THE WORK CAN BE HAZARDOUS TO THE HEALTH OF THE APPLICATOR IF NOT APPLIED AS PER MANUFACTURER'S INSTRUCTION. THE CONTRACTOR SHALL FOLLOW THE DATA SHEET AND THE LABEL ON THE PAINT CONTAINERS. THESE PRECAUTIONS SHALL INCLUDE THE USE OF RESPIRATORS AND EYE AND SKIN PROTECTION AS SPECIFIED. THE CONTRACTOR SHALL ALSO INSURE THAT HIS PAINTING OPERATIONS AND LOCATIONS WILL NOT ENDANGER OR ADVERSELY AFFECT THE PUBLIC IN GENERAL.

THE PROPOSED CLEANING AND COATING OPERATIONS SHALL BE PERFORMED ONLY WHEN THE AMBIENT TEMPERATURE IS 50 DEGREES F OR ABOVE. PAINT SHALL NOT BE APPLIED DURING RAIN, FOG OR MIST, OR WHEN THE STEEL SURFACE TEMPERATURE IS LESS THAN 5 DEGREES F ABOVE THE DEW POINT. PAINT SHALL NOT BE APPLIED TO WET OR DAMP SURFACES OR ON FROSTED OR ICE-COATED SURFACES. PAINT SHALL NOT BE APPLIED WHEN THE RELATIVE HUMIDITY IS GREATER THAN 85%. ALL STEEL SURFACES OF TRUSSES AND END FRAMES INCLUDING THE WELDED AREAS, BALLAST ENCLOSURE MOUNTING BRACKET AND BASE PLATES ARE TO BE CLEANED AND COATED. BEFORE EACH COATING IS APPLIED, IT SHALL BE MIXED WITH AN APPROVED POWER MECHANICAL MIXER TO A UNIFORM CONSISTENCY WHICH SHALL BE MAINTAINED DURING ITS APPLICATION. EACH COAT SHALL BE APPLIED IN A WORKMANLIKE MANNER AS A CONTINUOUS FILM OF UNIFORM THICKNESS WHICH IS FREE OF HOLIDAYS, PORES, RUNS OR SAGS. ALL COATS SHALL BE APPLIED BY BRUSH. THINNING OF PAINT IS STRICTLY PROHIBITED. PAINT NOT CAPABLE OF BEING APPLIED AS SPECIFIED SHALL NOT BE USED. THE COATING SHALL PENETRATE ALL JOINTS AND CONNECTIONS. THE ENGINEER SHALL BE NOTIFIED 24 HOURS PRIOR TO ANY CLEANING OR COATING OPERATIONS SO THAT INSPECTION SERVICES CAN BE PROVIDED.

COATING SYSTEM

THE COATING SYSTEM SHALL CONSIST OF A POLYAMIDE-CURED EPOXY PRIME COAT, A POLYAMIDE-CURED INTERMEDIATE COAT AND AN ALIPHATIC POLYURETHANE TOP COAT. THE COATING MATERIALS USED SHALL BE THOSE AS LISTED FROM ONE OF THE FOLLOWING MANUFACTURERS:

- AMERON  
210 NORTH BERRY STREET  
BREA, CALIFORNIA 92621  
LOCAL TELEPHONE CONTACT: (216) 896-3602  
PRIME COAT: AMERCOAT 71  
INTERMEDIATE COAT: AMERLOCK 400 (LIGHT GREY)  
TOP COAT: AMERCOAT 450 HS (MEDIUM GREY)
- THE GLIDDEN COMPANY  
16651 SPRAGUE ROAD  
STRONGSVILLE, OHIO 44136  
LOCAL TELEPHONE CONTACT: ( 216 ) 826 - 5528  
PRIME COAT: GLID - GUARD CORROSION  
RESISTANT HS EPOXY NO. 5465  
INTERMEDIATE COAT: GLID - GUARD CORROSION  
RESISTANT HS EPOXY NO. 5466  
TOP COAT: GLID - THANE II POLYURETHANE  
NO. 6200 SERIES
- PORTER PAINT CO.  
400 SOUTH 13TH STREET  
LOUISVILLE, KY. 40201  
LOCAL TELEPHONE CONTACT: (216) 562-6709  
PRIME COAT: PORTER PAINTS MCR 4300  
INTERMEDIATE COAT: PORTER PAINTS MCR 4300 (OFF-WHITE)  
TOP COAT: PORTER PAINTS HYTHANE
- POLY-CARB, INC.  
33095 BAINBRIDGE ROAD  
CLEVELAND, OHIO 44139  
LOCAL TELEPHONE CONTACT: (216) 248-1223  
PRIME COAT: MARK-60 ULTRAPOX  
INTERMEDIATE COAT: MARK-60 ULTRAPOX (LIGHT GREY)  
TOP COAT: MARK-73 ULTRAKOTE (MEDIUM GREY)
- SHERWIN WILLIAMS COMPANY  
761 BETA DRIVE  
MAYFIELD VILLAGE, OHIO 44143  
LOCAL TELEPHONE CONTACT: (216) 461-8287  
PRIME COAT: TILE-CLAD II HI-BILD PRIMER  
INTERMEDIATE COAT: HI-SOLIDS CATALYZED EPOXY (PURE WHITE)  
(SLATE GREY)  
TOP COAT: HI-BILD ALIPHATIC POLYURETHANE ENAMEL

ALL THREE COATS OF THE SYSTEM SHALL BE MANUFACTURED BY THE SAME COMPANY TO INSURE COMPATIBILITY AMONG COATS.

630-SURFACE PREPARATION, EXISTING SUPPORT SECTIONS

EXISTING, WEATHERED GALVANIZED SUPPORT SECTIONS SHOULD HAVE THEIR SURFACE PREPARATION AS WELL AS THEIR PROTECTIVE COATING DONE UNDER CONDITIONS OF TEMPERATURE AND HUMIDITY WITHIN THE SAME RANGE AS SPECIFIED BY THE MANUFACTURER OF THE EPOXY-PRIME COAT MATERIAL TO BE USED IMMEDIATELY AFTER THIS CLEANING OPERATION. THE SUPPORT SECTIONS SHALL BE PREPARED FOR COATING BY SSPC-SPI (SOLVENT CLEANING) FOLLOWED BY SSPC-SP6 (COMMERCIAL BLAST CLEANING). BEFORE THE PREPARED SURFACE DEGRADES FROM THE PRESCRIBED STANDARDS, THE PRIME COAT SHALL BE APPLIED. IN EVERY CASE, THE SURFACE SHALL BE COATED WITH EPOXY PRIME COAT ON THE SAME DAY AS THE SURFACE PREPARATION. CAREFUL HANDLING AND STORAGE WILL BE REQUIRED TO PREVENT ANY SCRAPING, MARRING, OR OTHER DAMAGE TO THE PREPARED SURFACE. PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, HANDLING, TRANSPORTATION COSTS AND MATERIALS NECESSARY TO ACCOMPLISH THIS ITEM OF WORK PER MAJOR SUPPORT SECTION.

BASIS OF PAYMENT WILL BE AS FOLLOWS:

ITEM 630-SURFACE PREPARATION, EXISTING SUPPORT SECTIONS  
AT CONTRACT BID PRICE PER EACH MAJOR SUPPORT SECTION.

630-COATING, EPOXY-PRIME COAT, SUPPORT SECTIONS

THIS ITEM SHALL CONSIST OF THE APPLICATION OF ONE (1) COAT OF AN EPOXY PRIMER TO SUPPORT SECTIONS. THE TOTAL DRY FILM THICKNESS OF THIS COAT SHALL BE BETWEEN 1.5 TO 2.0 MILS. IF MORE THAN ONE PASS IS NECESSARY TO OBTAIN THE REQUIRED THICKNESS, THAT COST SHALL BE BORNE BY THE CONTRACTOR. THE COLOR OF THIS COAT SHALL BE NOTICEABLY DIFFERENT FROM THE BASE MATERIAL AND OTHER PROPOSED COATS. THIS COAT SHALL IN ALL CASES BE APPLIED OVER SURFACES THAT WERE PREPARED EARLIER THAT SAME DAY. THE THINNING OF THE EPOXY MATERIAL IS STRICTLY PROHIBITED. MATERIAL NOT CAPABLE OF BEING APPLIED AS SPECIFIED SHALL NOT BE USED. WHEN THE AVERAGE DRY FILM THICKNESS OF THIS COAT OVER THE ENTIRE SUPPORT SECTION IS LESS THAN THE SPECIFIED 1.5 TO 2.0 MILS BUT IS AT LEAST 1.25 MILS, THE CONTRACT BID PRICE FOR THIS ITEM SHALL BE REDUCED IN DIRECT PROPORTION TO THE PERCENT DEFICIENCY OF COATING UP TO 16-2/3%. IF THE DEFICIENCY OF COATING IS MORE THAN THAT 16-2/3% ( I.E., THE AVERAGE DRY FILM THICKNESS IS LESS THAN 1.25 MILS ) THE WORK FOR THIS ITEM SHALL BE CONSIDERED UNSATISFACTORY AND SHALL BE RECOATED AT THE FULL EXPENSE OF THE CONTRACTOR, INCLUDING ALL LABOR, EQUIPMENT AND MATERIAL.

THE EPOXY PRIME COAT CHOSEN BY THE CONTRACTOR SHALL BE ONE OF THE FOLLOWING TWO-COMPONENT COMPOSITIONS CONFORMING TO ITS LISTED PROPERTIES:

AMERCOAT 71:

% SOLIDS BY VOLUME: 47% ± 3%  
POT LIFE: 8 HRS. @ 77 DEGREES F.  
DRYING TIME: 4 HRS. @ 77 DEGREES F.

GLID-GUARD CORROSION RESISTANT HS EPOXY NO. 5465:  
% SOLIDS BY VOLUME: 54% ± 2%  
POT LIFE: 4 HRS. @ 70 DEGREES F.  
DRYING TIME: 1-2 HRS. TO TOUCH, 7 HRS. TO RECOAT  
AT 70 DEGREES F., 50% RELATIVE  
HUMIDITY  
VISCOSITY: 95 - 100 KU  
% SOLIDS BY WEIGHT: 71% ± 2%

MCR-4301 EPOXY PRIMER:  
% SOLIDS BY VOLUME 48.0% ± 2%  
POT LIFE: 30 HRS. @ 50-60 DEGREES F.  
16 HRS. @ 80-100 DEGREES F.  
DRYING TIME: 4-6 HRS. @ 50-60 DEGREES F.

MARK-60 ULTRAPOX:  
% SOLIDS BY VOLUME: 50% ± 5%  
% SOLIDS BY WEIGHT: 52% ± 5%  
POT LIFE: 6 HRS. @ 75 DEGREES F.  
DRYING TIME: 2-3 HRS. INITIAL SET @ 75 DEGREES F.  
VISCOSITY: 300-500 CPS @ 75 DEGREES F.

TILE-CLAD II HI-BILD PRIMER:  
% SOLIDS BY VOLUME: 48% ± 2%  
% SOLIDS BY WEIGHT: 63% ± 2%  
POT LIFE: 8 HRS. @ 77 DEGREES F.  
DRYING TIME: 1 HR. TO TOUCH, 6 HRS. TO RECOAT  
@ 77 DEGREES F.

FOR NEW SUPPORT SECTIONS THE PRIME COAT SHOULD BE DONE AT THE MANUFACTURER OF THE SUPPORT SECTIONS. VERIFICATION BY THE MANUFACTURER OF THE COATING MATERIAL FOR THE PRIME COAT WILL BE REQUIRED. CAREFUL HANDLING AND STORAGE WILL BE REQUIRED TO PREVENT ANY SCRAPING, MARRING OR OTHER SURFACE DAMAGE TO THE PRIME COAT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, HANDLING COSTS AND MATERIALS NECESSARY TO ACCOMPLISH THIS ITEM OF WORK. THIS PRIME COAT SHALL BE MANUFACTURED BY THE SAME COMPANY SUPPLYING THE INTERMEDIATE AND TOP COATS. A PROPERLY CALIBRATED DRY FILM THICKNESS INSTRUMENT WILL BE USED TO CHECK THE COATING.

BASIS OF PAYMENT WILL BE AS FOLLOWS:  
ITEM 630-COATING, EPOXY PRIME COAT, SUPPORT SECTIONS AT  
CONTRACT BID PRICE PER EACH MAJOR SUPPORT SECTION.



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630-COATING, EPOXY INTERMEDIATE COAT,  
SUPPORT SECTIONS

THIS ITEM SHALL CONSIST OF THE APPLICATION OF ONE (1) COAT OF EPOXY TO SUPPORT SECTIONS. THE TOTAL DRY FILM THICKNESS OF THIS COAT SHALL NOT BE LESS THAN SIX (6.0) MILS. IF MORE THAN ONE PASS IS NECESSARY TO OBTAIN THE REQUIRED THICKNESS, THAT COST SHALL BE BORNE BY THE CONTRACTOR. THINNING OF THE EPOXY MATERIAL IS STRICTLY PROHIBITED. MATERIAL NOT CAPABLE OF BEING APPLIED AS SPECIFIED SHALL NOT BE USED. THE COLOR OF THIS COAT SHALL BE LIGHT GREY.

WHEN THE AVERAGE DRY FILM THICKNESS OF THIS COAT OVER THE ENTIRE SUPPORT SECTION IS LESS THAN THE SPECIFIED SIX (6.0) MILS, BUT IS AT LEAST (5.0) MILS, THE CONTRACT PRICE FOR THIS ITEM SHALL BE REDUCED IN DIRECT PROPORTION TO THE PERCENT DEFICIENCY OF COATING UP TO 16-2/3%. IF THE DEFICIENCY OF COATING IS MORE THAN 16-2/3%, (I.E. THE AVERAGE DRY FILM THICKNESS IS LESS THAN 5.0 MILS), THE WORK FOR THIS ITEM SHALL BE CONSIDERED UNSATISFACTORY AND SHALL BE RECOATED AT THE FULL EXPENSE OF THE CONTRACTOR, INCLUDING ALL LABOR, EQUIPMENT AND MATERIAL. THE EPOXY INTERMEDIATE COAT CHOSEN BY THE CONTRACTOR SHALL BE ONE OF THE FOLLOWING TWO-COMPONENT COMPOSITIONS CONFORMING TO ITS LISTED PROPERTIES :

AMERLOCK 400:

% SOLIDS BY VOLUME:	83% ±2%
POT LIFE:	2-1/2 HRS. @ 70 DEGREES F.
DRYING TIME:	20 HRS. @ 70 DEGREES F.

GLID-GUARD CORROSION RESISTANT HS EXPOXY  
NO. 5466:

% SOLIDS BY VOLUME:	54% ± 2%
POT LIFE:	4 HRS. @ 70 DEGREES F.
DRYING TIME:	1 - 2 HRS. TO TOUCH 7 HRS. TO RECOAT AT 70 DEGREES F., 50% RELATIVE HUMIDITY
VISCOSITY:	95 - 100 KU
% SOLIDS BY WEIGHT:	71% ± 2%

MCR 4361 HIGH BUILD EPOXY:

% SOLIDS BY VOLUME:	49.4% ±2%
POT LIFE:	30 HRS. @ 50-60 DEGREES F.
DRYING TIME:	16 HRS. @ 80-100 DEGREES F. 1-2 HRS. @ 60-80 DEGREES F.

MARK-60 ULTRAPOX:

% SOLIDS BY VOLUME:	50% ± 5%
% SOLIDS BY WEIGHT:	52% ±5%
POT LIFE:	6 HRS. @ 75 DEGREES F.
DRYING TIME:	2-3 HRS. INITIAL SET @ 75 DEGREES F.
VISCOSITY:	300-500 CPS @ 75 DEGREES F.

HI-SOLIDS CATALYZED EPOXY:

% SOLIDS BY VOLUME:	61% ±2%
% SOLIDS BY WEIGHT:	77% ± 2%
POT LIFE:	5 HRS. @ 77 DEGREES F.
DRYING TIME:	1 HR. TO TOUCH, 4 HRS. TACK FREE, 6 HRS. TO RECOAT @ 77 DEGREES F.& 50% R.H.

AT LEAST 24 HOURS BUT NO MORE THAN THREE (3) DAYS SHALL ELAPSE AFTER THE APPLICATION OF THE EPOXY PRIME COAT AND BEFORE THE APPLICATION OF THE EPOXY INTERMEDIATE COAT. SURFACES SHALL IN ALL CASES BE CLEAN BEFORE THE INTERMEDIATE COAT IS APPLIED.

FOR NEW SUPPORT SECTIONS, THIS INTERMEDIATE COAT SHOULD BE DONE AT THE MANUFACTURER OF THE SUPPORT SECTIONS. VERIFICATION BY THE MANUFACTURER FOR THE INTERMEDIATE COAT WILL BE REQUIRED. CAREFUL HANDLING AND STORAGE WILL BE REQUIRED TO PREVENT ANY SCRAPING, MARRING OR OTHER SURFACE DAMAGE TO THE INTERMEDIATE COAT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, HANDLING COST AND MATERIAL NECESSARY TO ACCOMPLISH THIS ITEM OF WORK. THIS INTERMEDIATE COAT SHALL BE MANUFACTURED BY THE SAME COMPANY SUPPLYING THE PRIME AND TOP COATS. A PROPERLY CALIBRATED DRY FILM THICKNESS INSTRUMENT WILL BE USED TO CHECK THE COATING.

BASIS OF PAYMENT WILL BE AS FOLLOWS:

ITEM 630-COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTIONS  
AT CONTRACT BID PRICE PER EACH MAJOR SUPPORT SECTION.

630-COATING, URETHANE TOP COAT,  
SUPPORT SECTIONS

THIS ITEM SHALL CONSIST OF THE APPLICATION OF ONE (1) COAT OF URETHANE TO SUPPORT SECTIONS. THE TOTAL DRY FILM THICKNESS OF THIS COAT SHALL NOT BE LESS THAN ONE AND ONE-HALF (1.5) MILS. IF MORE THAN ONE PASS IS NECESSARY TO OBTAIN THE REQUIRED THICKNESS, THAT COST SHALL BE BORNE BY THE CONTRACTOR. THINNING OF THE URETHANE MATERIAL IS STRICTLY PROHIBITED. MATERIAL NOT CAPABLE OF BEING APPLIED AS SPECIFIED SHALL NOT BE USED. THE COLOR OF THIS COAT SHALL BE MEDIUM GREY.

WHEN THE AVERAGE DRY FILM THICKNESS OF THIS COAT OVER THE ENTIRE SUPPORT SECTION IS LESS THAN THE SPECIFIED ONE AND ONE-HALF (1.5) MILS BUT IS AT LEAST ONE (1.0) MIL, THE CONTRACT PRICE FOR THIS ITEM SHALL BE REDUCED IN DIRECT PROPORTION TO THE PERCENT DEFICIENCY OF COATING UP TO 33-1/3%. IF THE DEFICIENCY OF COATING IS MORE THAN 33-1/3%, (I.E. THE AVERAGE DRY FILM THICKNESS IS LESS THAN 1.0 MIL), THE WORK FOR THIS ITEM SHALL CONSIDERED UNSATISFACTORY AND SHALL BE RECOATED AT THE FULL EXPENSE OF THE CONTRACTOR, INCLUDING ALL LABOR, EQUIPMENT AND MATERIAL.

THE URETHANE TOP COAT CHOSEN BY THE CONTRACTOR SHALL BE ONE OF THE FOLLOWING MATERIALS CONFORMING TO ITS LISTED PROPERTIES:

AMERCOAT 450 HS:

% SOLIDS BY VOLUME:	66% ±3%
POT LIFE:	4 HRS. @ 70 DEGREES F.
DRYING TIME:	8 HRS. @ 70 DEGREES F DRY-THROUGH

GLID-THANE II POLYURETHANE NO. 6200:

% SOLIDS BY VOLUME:	40.3% ±2%
POT LIFE:	8 HRS.
DRYING TIME:	1 HR. TO TOUCH, 3 HRS. TO RECOAT AT 77 DEGREES F., 50% RELATIVE HUMIDITY
% SOLIDS BY WEIGHT:	57.8% ±2%

HYTHANE 4610 ALIPHATIC POLYURETHANE:

% SOLIDS BY VOLUME:	43.4% ±2%
POT LIFE:	12 HRS. @ 75 DEGREES F.
DRYING TIME:	¾ HRS. TO TOUCH @ 75 DEGREES F.

MARK-73 ULTRAKOTE:

% SOLIDS BY VOLUME:	51%
POT LIFE:	5½ HRS. @ 75 DEGREES F.
DRYING TIME:	4-5 HRS. @ 75 DEGREES F.
	TACK FREE
VISCOSITY:	1000 - 1500 CPS @ 75 DEGREES F.
% SOLIDS BY WEIGHT:	64%

HI-BILD ALIPHATIC POLYURETHANE ENAMEL:

% SOLIDS BY VOLUME:	40% ± 2% (CATALYZED)
% SOLIDS BY WEIGHT:	48% ±2%
POT LIFE:	6 HRS. @ 77 DEGREES F.
DRYING TIME:	30 MIN. TO TOUCH, 4 HRS. TACK FREE, 18 HRS. MINIMUM, 72 HRS. MAXIMUM TO RECOAT.

AT LEAST 24 HOURS BUT NO MORE THAN THREE (3) DAYS SHALL ELAPSE AFTER THE APPLICATION OF THE EPOXY INTERMEDIATE COAT AND BEFORE THE APPLICATION OF THE URETHANE TOP COAT. SURFACES SHALL IN ALL CASES BE CLEAN BEFORE THE TOP COAT IS APPLIED.

FOR NEW SUPPORT SECTIONS, THIS TOP COAT SHOULD BE DONE BY THE MANUFACTURER OF THE SUPPORT SECTIONS. VERIFICATION BY THE MANUFACTURER FOR THE TOP COAT WILL BE REQUIRED. CAREFUL HANDLING AND STORAGE WILL BE REQUIRED TO PREVENT ANY SCRAPING, MARRING OR OTHER SURFACE DAMAGE TO THE TOP COAT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, HANDLING COST AND MATERIALS NECESSARY TO ACCOMPLISH THIS ITEM OF WORK. THIS TOP COAT SHALL BE MANUFACTURED BY THE SAME COMPANY SUPPLYING THE PRIME AND INTERMEDIATE COATS. A PROPERLY CALIBRATED, DRY FILM THICKNESS INSTRUMENT WILL BE USED TO CHECK THE COATING.

BASIS OF PAYMENT WILL BE AS FOLLOWS:

ITEM 630-COATING, URETHANE TOP COAT, SUPPORT SECTIONS  
AT CONTRACT BID PRICE PER EACH MAJOR SUPPORT SECTION.

PREQUALIFICATION

PRIOR TO USE, THE CONTRACTOR SHALL SUBMIT TO THE DIRECTOR COPIES OF THE MANUFACTURER'S CERTIFIED TEST DATA SHOWING THAT THE MATERIAL COMPLIES WITH THE REQUIREMENTS OF THIS SPECIFICATION. THE TEST DATA SHALL INCLUDE THE BRAND NAME OF THE PAINT, NAME OF MANUFACTURER, NUMBER OF THE LOT TESTED AND DATE OF MANUFACTURE. WHEN THE PAINT HAS BEEN APPROVED BY THE DIRECTOR, FURTHER PERFORMANCE TESTING BY THE MANUFACTURER WILL NOT BE REQUIRED UNLESS THE FORMULATION OR MANUFACTURING PROCESS HAS BEEN CHANGED, IN WHICH CASE NEW CERTIFIED TEST RESULTS WILL BE REQUIRED.

ACCEPTANCE

THE MANUFACTURER SHALL SUBMIT CERTIFIED TEST DATA IN ACCORDANCE WITH THE REQUIREMENTS OF THIS SPECIFICATION. THE STATE RESERVES THE RIGHT TO SAMPLE AND TEST DELIVERED LOTS FOR COMPLIANCE.

CONVERSION OF METRIC STANDARD DRAWING

THE METRIC STANDARD DRAWINGS REFERENCED IN THIS PLAN SHALL BE CONVERTED TO ENGLISH UNITS USING THE SI (METRIC) TO ENGLISH CONVERSION FACTORS PROVIDED IN SECTION 109.011 OF THE 1997 ODOT CONSTRUCTION AND MATERIAL SPECIFICATION. THE APPENDIX OF ASTM E 380 SHALL BE UTILIZED FOR ANY ADDITIONAL CONVERSION FACTORS REQUIRED. CONVERSIONS SHALL BE APPROPRIATELY PRECISE AND SHALL REFLECT STANDARD INDUSTRY ENGLISH VALUES WHERE SUITABLE.

LOGO SIGNS

OHIO LOGOS, INC. IS RESPONSIBLE FOR REMOVAL AND ERECTION OF LOGO SIGNS. THE CONTRACTOR SHALL CONTACT OHIO LOGOS (1-800-860-LOGO) AT LEAST 60 DAYS PRIOR TO THE DATE THAT ANY SIGNS MUST BE REMOVED OR ADJUSTED.

631 SIGN SERVICE, AS PER PLAN

THIS WORK SHALL BE IN ACCORDANCE WITH 631.06, WITH THE FOLLOWING EXCEPTIONS:

THE CONTRACTOR SHALL DISCONNECT THE EXISTING SIGN SERVICE CONDUCTORS FROM THE ADJACENT PULLBOX TO THE DISCONNECT SWITCH AND SHALL PULL IN THE NEW CONDUCTORS (5000 VOLT SECONDARY FEEDER CABLE, 3 CONDUCTORS, NO. 4 AWG) AND TERMINATE THEM.

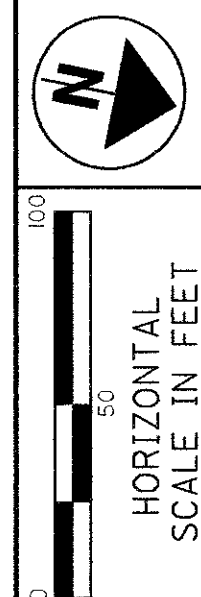
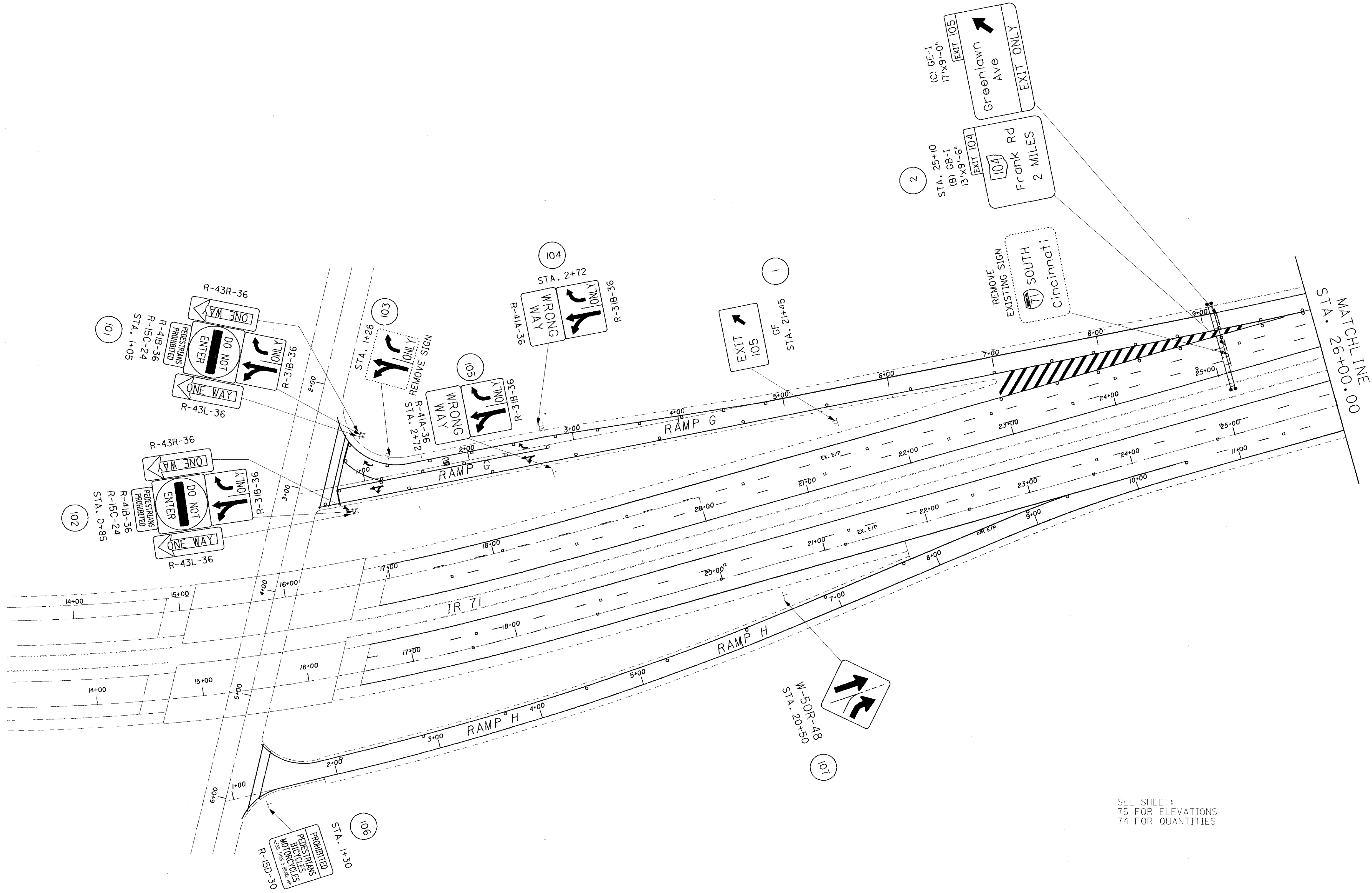
IN THE EVENT THE CONDUCTORS CANNOT BE PULLED IN WITHOUT DAMAGE, THE CONTRACTOR SHALL CLEAN, REPAIR OR REPLACE THE CONDUIT TO PROVIDE A RACEWAY IN COMPLIANCE WITH THE NATIONAL ELECTRIC CODE AND THEN INSTALL THE CONDUCTORS.

THE CONTRACTOR SHALL MODIFY THE DISCONNECT SWITCH TO PROPERLY TERMINATE THE NEW 480 VOLT, 3-WIRE SYSTEM AT THOSE LOCATIONS WHERE THE CIRCUITS ARE BEING CHANGED.

631 SIGN WIRED, AS PER PLAN - AND -  
631 SIGN WIRED, OVERPASS STRUCTURE MOUNTED, AS PER PLAN

THIS WORK SHALL BE IN ACCORDANCE WITH 631.07, WITH THE FOLLOWING EXCEPTIONS:

THE CONTRACTOR SHALL REMOVE THE EXISTING SIGN WIRING AND INSTALL NEW WIRING IN ACCORDANCE WITH 631.07, ALL CONDUIT, JUNCTION BOXES, FITTINGS, FLEXIBLE CONDUITS AND RELATED ITEMS SHALL BE INSPECTED AND CLEANED, REPAIRED OR REPLACED AS NECESSARY TO PROVIDE A CONDUCTOR RACEWAY WHICH CONFORMS TO THE NATIONAL ELECTRIC CODE.

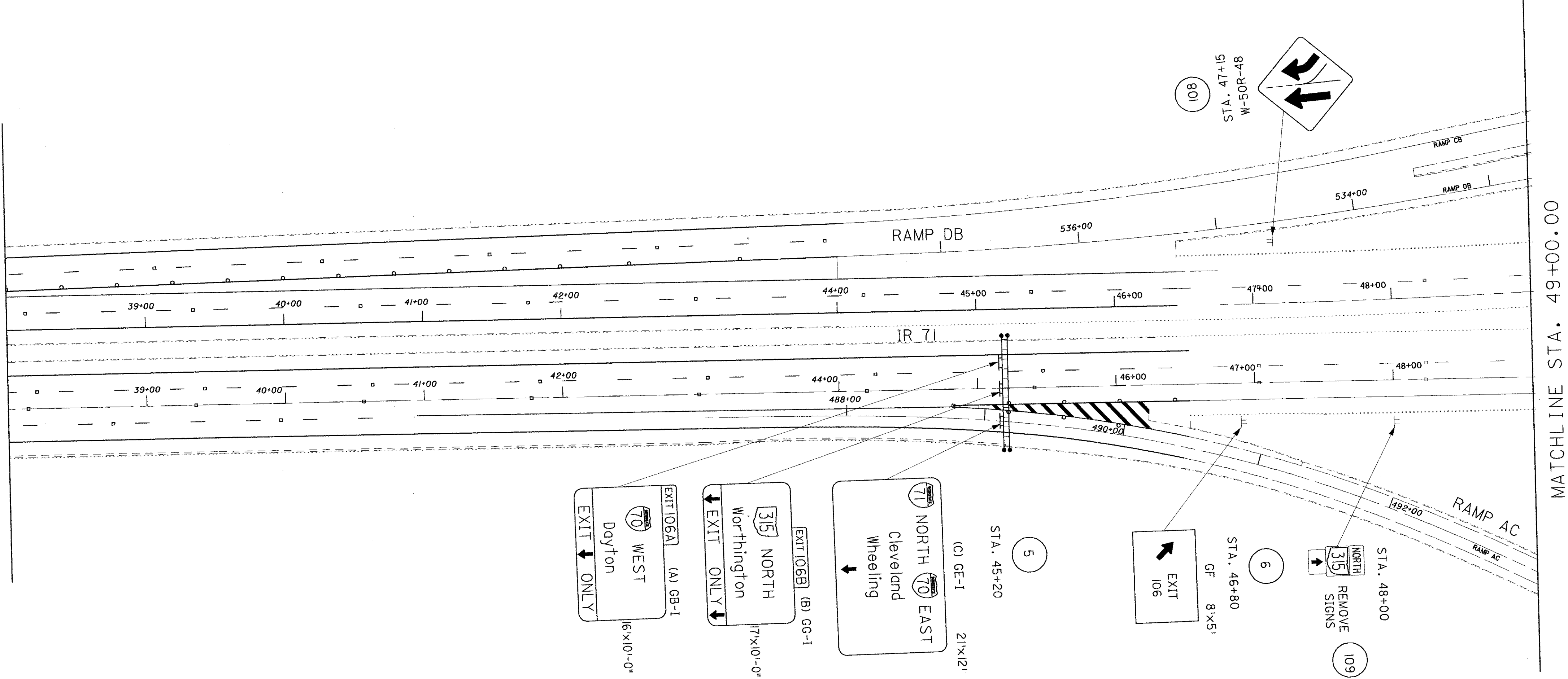


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**SIGNING PLAN**  
**IR71 STA. 16+62.75 TO STA. 26+00.00**

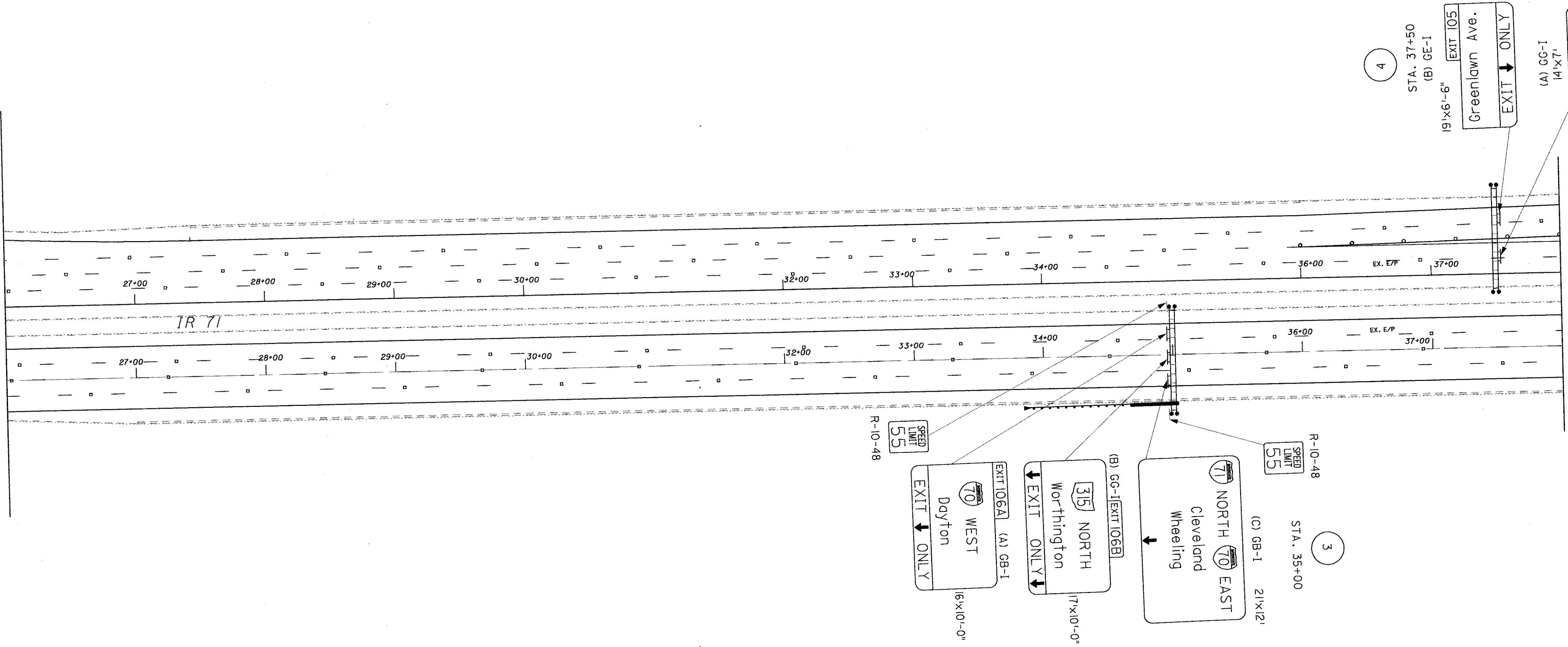
**FRA-71-14.39,**  
**FRA-315-0.00**

MATCHLINE STA. 38+00.00



MATCHLINE STA. 49+00.00

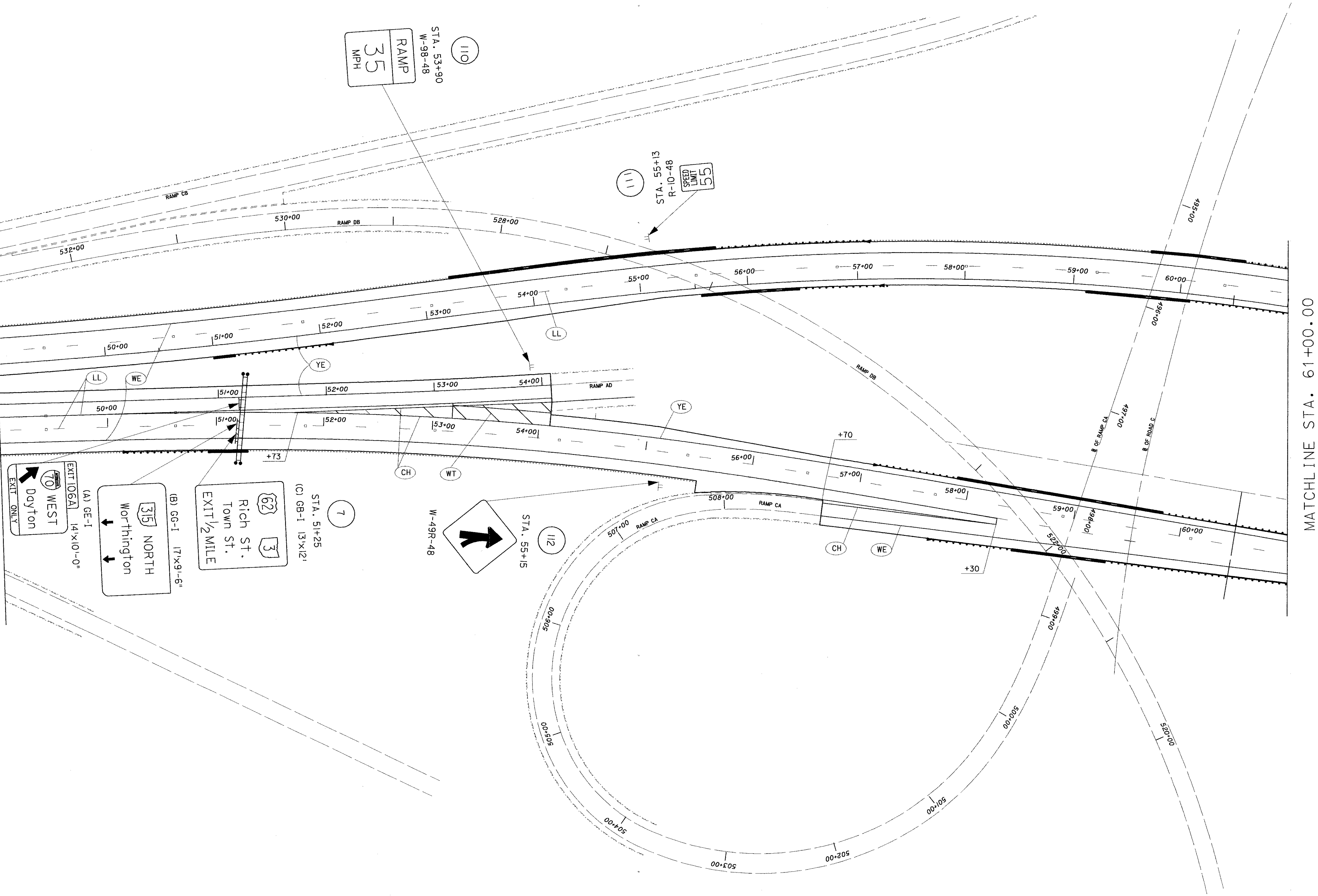
MATCHLINE STA. 26+00.00



SEE SHEET:  
75/76 FOR ELEVATIONS  
74 FOR QUANTITIES

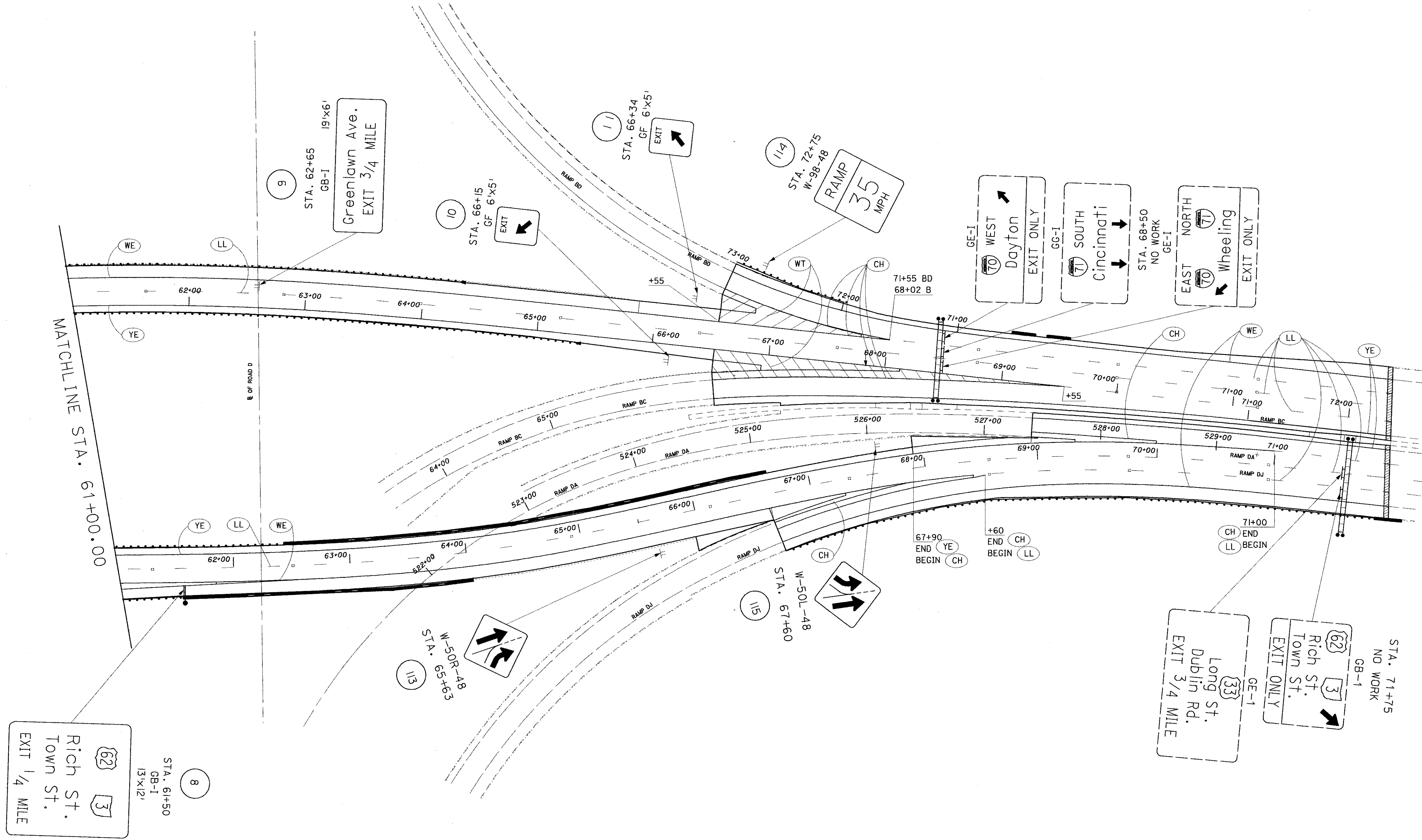


(FOR PAVEMENT MARKINGS SOUTH OF  
STA. 49+00, SEE SHEET 64)  
MATCHLINE STA. 49+00.00 SEE SHEET 71



MATCHLINE STA. 61+00.00

SEE SHEET:  
76 FOR ELEVATIONS  
74 FOR QUANTITIES  
62 FOR PAVEMENT MARKING LEGEND

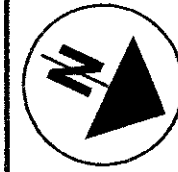
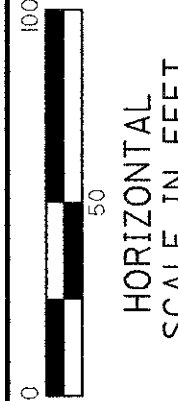


SEE SHEET:  
76 FOR ELEVATIONS  
74 FOR QUANTITIES  
62 FOR PAVEMENT MARKING LEGEND

FRA-71-14.39,  
FRA-315-0.00

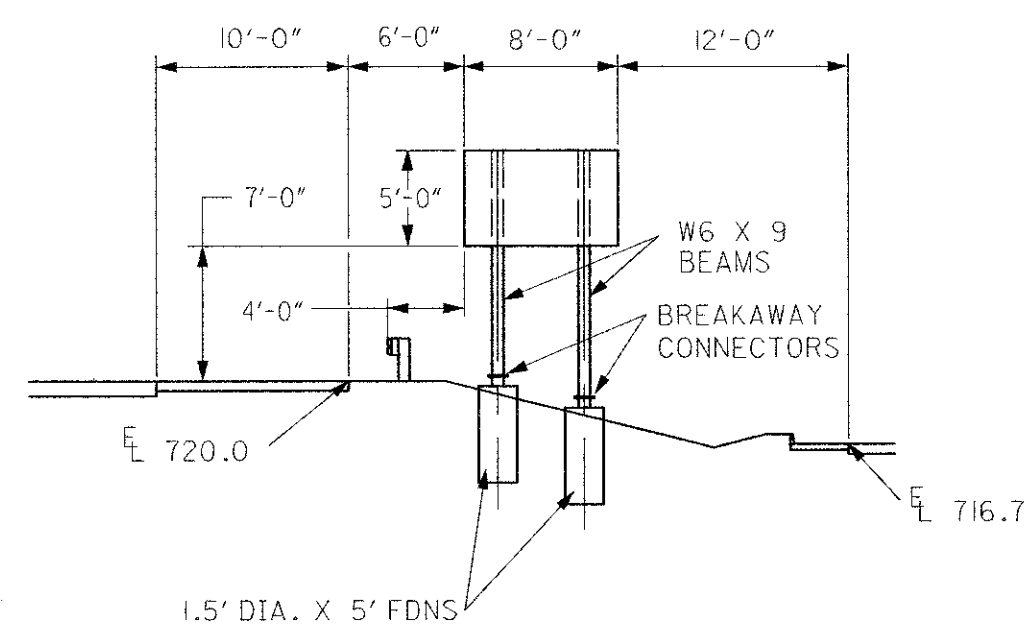
SIGNING PLAN  
SR315 STA. 61+00.00 TO STA. 72+00.00

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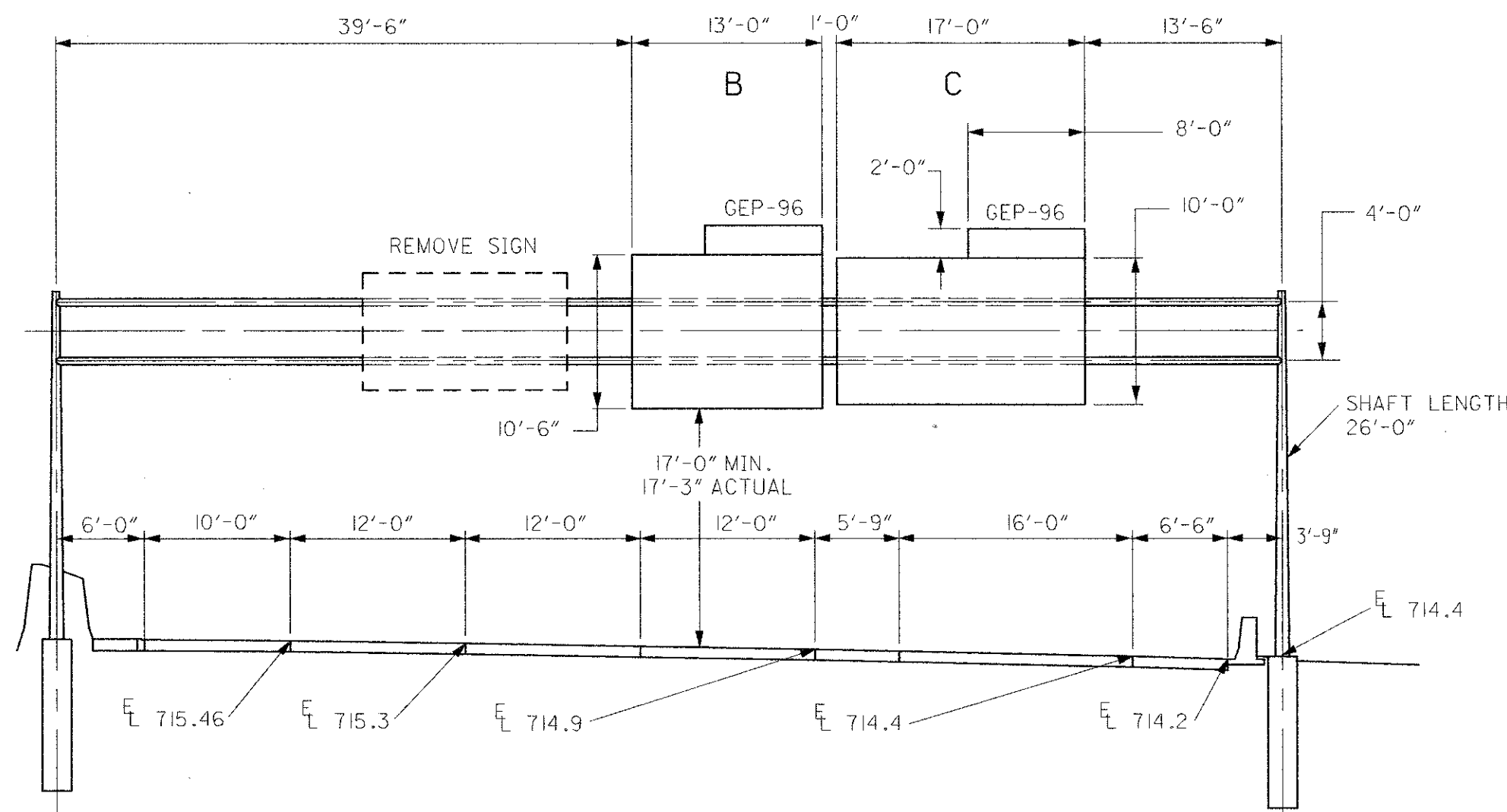


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DATE:	10-20-00



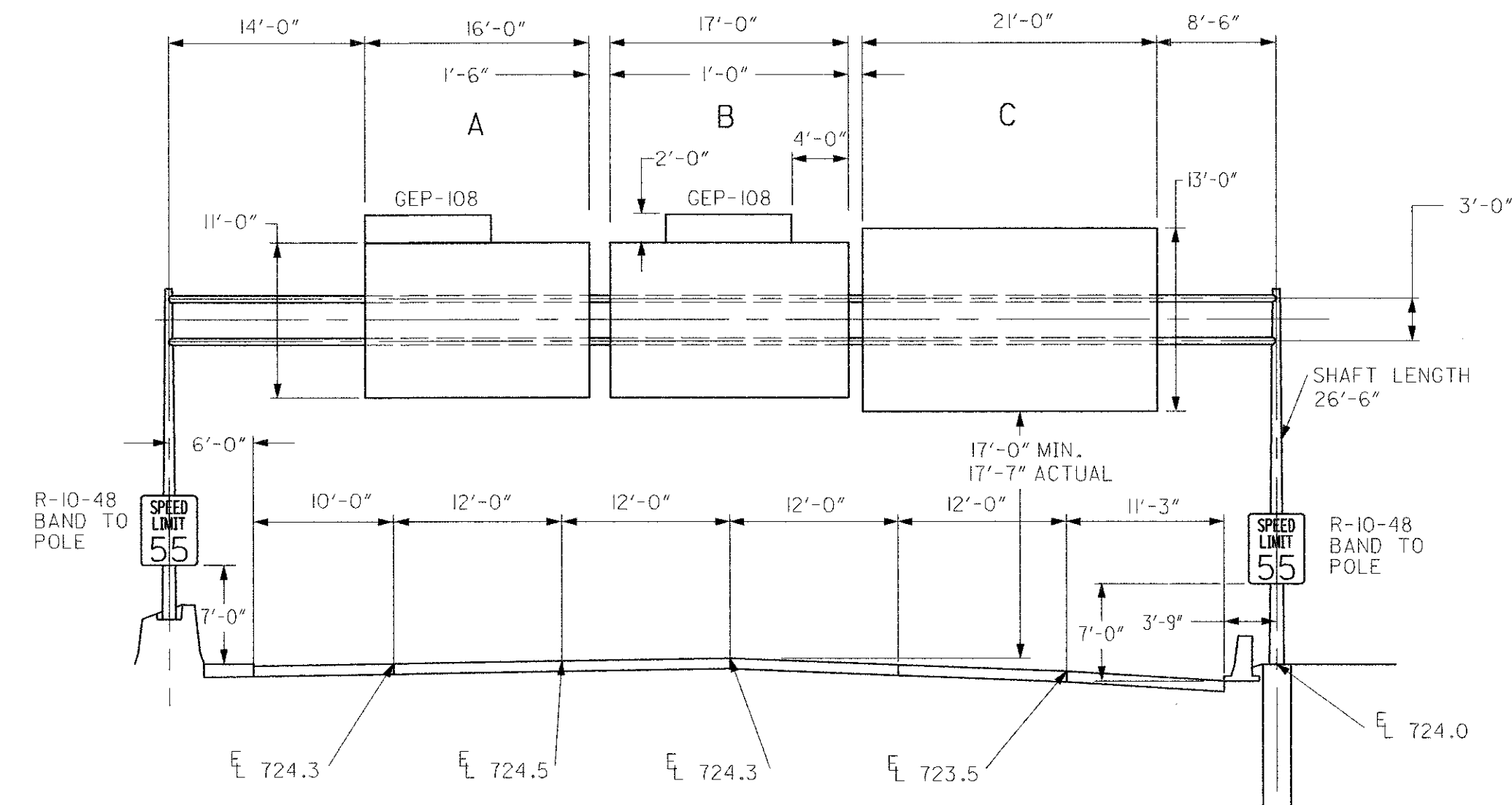


① STA. 21+45 LT. SB ROAD B



② STA. 25+10 LT. SB ROAD B

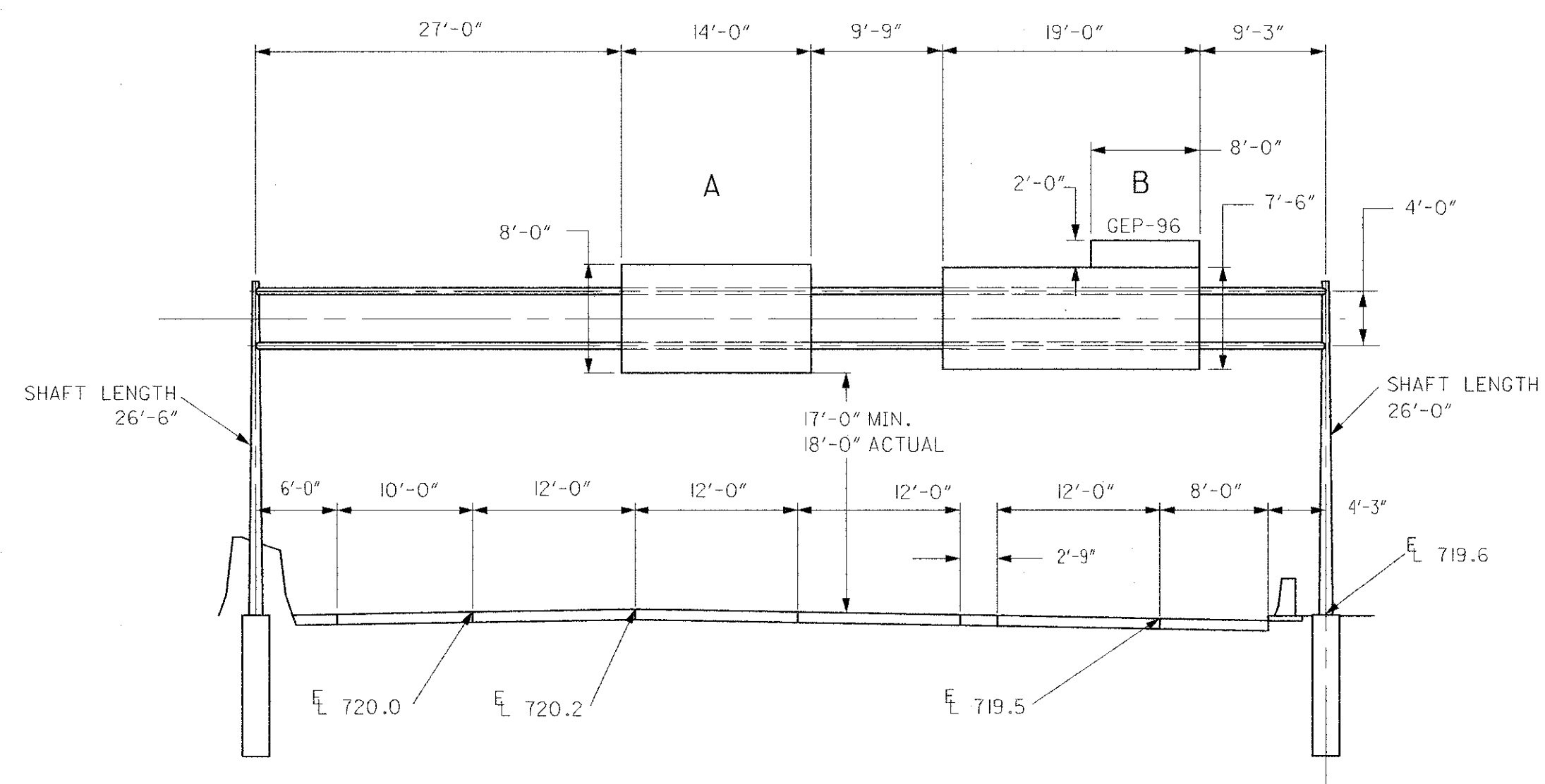
	B	C
SIGN ATTACHMENT ASSEMBLIES	3 @ 10'-6"	3 @ 9'-6"
LUMINAIRE SUPPORT ASSEMBLIES	2 @ 4'-3"	2 @ 4'-3"



③ STA. 35+00 RT. NB ROAD A

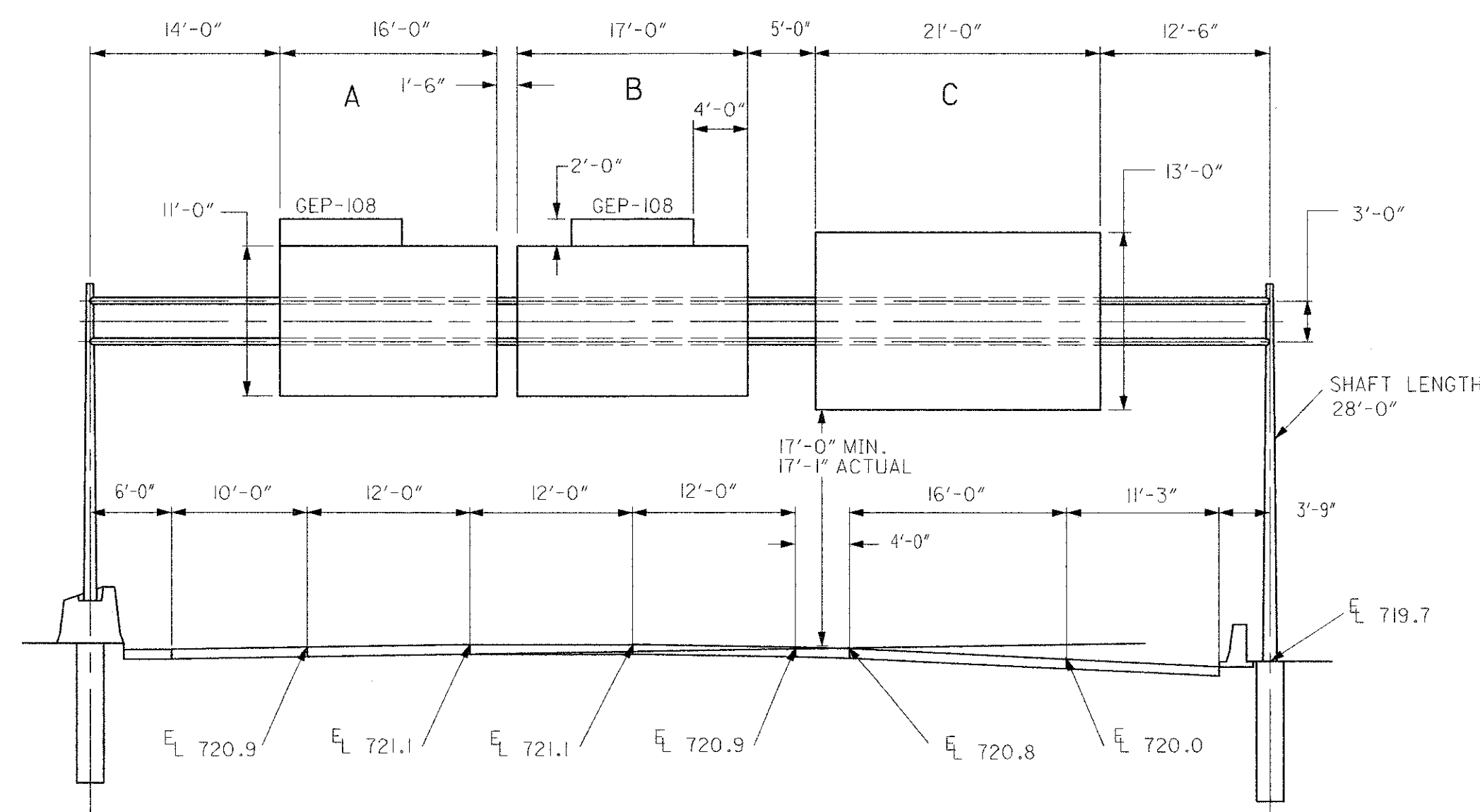
	A	B	C
SIGN ATTACHMENT ASSEMBLIES	3 @ 10'-6"	3 @ 10'-6"	7 @ 13'-0"
LUMINAIRE SUPPORT ASSEMBLIES	2 @ 4'-3"	2 @ 4'-3"	3 @ 5'-9"

PROVIDE ADDITIONAL INTERMEDIATE ATTACHMENT ASSEMBLIES FOR SIGN C.



④ STA. 37+50 LT. SB ROAD B

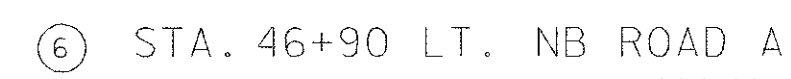
	A	B
SIGN ATTACHMENT ASSEMBLIES	3 @ 8'-0"	3 @ 7'-0"
LUMINAIRE SUPPORT ASSEMBLIES	2 @ 4'-3"	2 @ 3'-3"



⑤ STA. 45+20 RT. NB ROAD A

	A	B	C
SIGN ATTACHMENT ASSEMBLIES	3 @ 10'-6"	3 @ 10'-6"	7 @ 13'-0"
LUMINAIRE SUPPORT ASSEMBLIES	2 @ 4'-3"	2 @ 4'-3"	3 @ 5'-9"

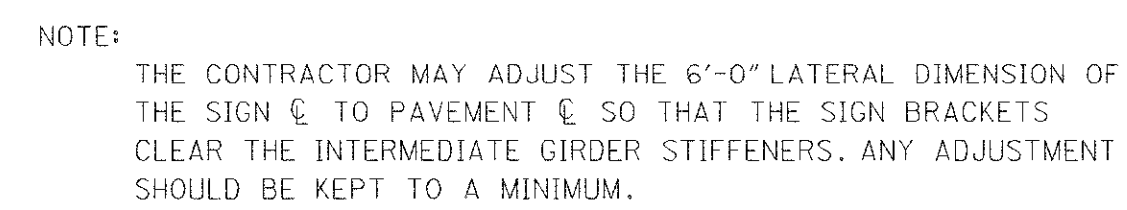
PROVIDE ADDITIONAL INTERMEDIATE ATTACHMENT ASSEMBLIES FOR SIGN C.



PROVIDE ADDITIONAL INTERMEDIATE ATTACHMENT ASSEMBLIES FOR SIGN C.



PROVIDE ADDITIONAL INTERMEDIATE ATTACHMENT ASSEMBLIES.



⑨ ROAD D BRIDGE OVER ROAD B (BRIDGE MOUNTED)

⑩ STA. 66+15 RT. SB ROAD B AND RAMP BC



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THE DESCRIPTION OF THE WORK IS AS FOLLOWS:

EXISTING HIGH MAST LIGHTS WILL BE PROVIDED WITH NEW 400W HPS FIXTURES (INCREASE FROM 4 TO 6 FIXTURES AT 3 LOCATIONS). ALL OTHER EXISTING COMPONENTS WILL BE REUSED WITH THE EXCEPTION OF COMPONENTS FOUND TO BE DAMAGED WHICH SHALL BE REPAIRED OR REPLACED WITH SIMILAR COMPONENTS CONFORMING TO THE CONSTRUCTION AND MATERIAL SPECIFICATIONS.

EXISTING UNDERDECK LUMINAIRES WITHIN THE INTERCHANGE OF I-71, I-70, SR 315 SHALL BE CLEANED AND INSPECTED. BROKEN OR DAMAGED PARTS SHALL BE REPLACED AND THE UNIT ADJUSTED AND RETURNED TO SERVICE WITH A NEW LAMP. THIS WORK IS LIMITED TO LUMINAIRES ON CIRCUITS B, C, D AND J.

EXISTING CONVENTIONAL LUMINAIRES ON CIRCUITS B, E, C, I AND J SHALL BE CLEANED AND INSPECTED. BROKEN OR DAMAGED PARTS SHALL BE REPLACED AND THE UNITS ADJUSTED AND RETURNED TO SERVICE WITH A NEW LAMP.

ALL UNUSED CONDUCTORS, CONDUITS OR APPARATUS WILL BE REMOVED.

625 LIGHTING, MISCELLANEOUS: CLEAN, INSPECT AND REPAIR LUMINAIRES

THE CONTRACTOR SHALL CLEAN THE LUMINAIRE AND REPLACE THE LAMP. ANY BROKEN, DAMAGED OR INOPERABLE PARTS SHALL BE REPAIRED OR REPLACED AS A PART OF THIS WORK. COVER SCREWS AND MOVING PARTS SHALL BE LUBRICATED. MIS-AIMED LUMINAIRES SHALL BE REALIGNED.

625 LIGHTING, MISCELLANEOUS: UPGRADE CIRCUITS FROM 2/C TO 3/C (BY SIZE)

THE CONTRACTOR SHALL CAREFULLY IDENTIFY EACH CABLE OF EACH CIRCUIT AND REMOVE CONNECTIONS. THE CABLES TO BE REMOVED SHALL BE LUBRICATED TO THE EXTENT POSSIBLE AND SHALL BE ATTACHED TO THE REPLACEMENT CABLES. THE OLD CABLES SHALL BE USED TO PULL IN THE REPLACEMENT CABLES FOR EACH RUN OF DUCT. IN THE EVENT THE EXISTING CABLES CANNOT BE MOVED IN THE EXISTING CONDUIT OR CABLE DUCT, THE CONTRACTOR SHALL REPLACE BOTH THE CABLES AND THE DUCT OR CONDUIT OF THAT PARTICULAR SECTION OF CIRCUIT. IN CASES WHERE MORE THAN ONE CIRCUIT OCCUPIES A COMMON CONDUIT, ALL CABLES IN THAT SEGMENT SHALL BE PULLED IN AT ONE TIME.

625 LIGHTING, MISCELLANEOUS, CLEAN, INSPECT AND REPAIR PULLBOX

THE CONTRACTOR SHALL INSPECT EACH PULLBOX ALONG THE MODIFIED CIRCUITS. EXISTING PULLBOXES NOT MEETING 713.08 AND ANY PULLBOX DAMAGED BEYOND REPAIR SHALL BE REPLACED WITH A NEW CONCRETE PULLBOX MEETING THE REQUIREMENTS OF 713.08 AND OF THE SAME NOMINAL SIZE. PULLBOXES SHALL BE CLEANED. EXISTING PULLBOX DRAINS SHALL BE CLEANED AND OPERABLE. MISSING OR BROKEN LIDS SHALL BE REPAIRED OR REPLACED AS NECESSARY. UNUSED OPENINGS WILL BE CLOSED.

713.14 LAMPS

HIGH PRESSURE SODIUM LAMPS SHALL BE GENERAL ELECTRIC "LUCALOX" PHILLIPS "CERAMALUX", SYLVANIA "LUMALUX", OR EQUAL APPROVED BY THE ENGINEER.

LIGHTING NOTES

ELECTRICAL TESTS

ALL NEWLY INSTALLED EQUIPMENT AND CABLES SHALL BE TESTED IN ACCORDANCE WITH 625.22. FAILURE OF ANY ITEM FURNISHED OR REBUILT BY THE CONTRACTOR SHALL REQUIRE THE CONTRACTOR TO REPLACE OR REBUILD THE ITEM IN ORDER TO PASS THE TEST.

625 LIGHTING, MISCELLANEOUS: REVISE CONTROL CENTER

THIS ITEM OF WORK SHALL INCLUDE REVISIONS TO ALL CIRCUITS AT THE CONTROL CENTER TO ACCOMMODATE THE PROPOSED 3-CONDUCTOR, 480 VOLT GROUNDED NEUTRAL SYSTEM. IT SHALL INCLUDE MODIFICATION OF THE CABINETS, ADDITION OF CONTACTORS AND FUSES AND ALL OTHER WORK NEEDED TO BRING THE CENTER INTO CONFORMANCE WITH THE NATIONAL ELECTRICAL CODE AND THE CONSTRUCTION AND MATERIAL SPECIFICATIONS. THIS ITEM SHALL ALSO INCLUDE COSTS OF THE POWER SUPPLY AGENCY TO ARRANGE FOR THE REQUIRED SERVICE. THE REQUIRED WORK AT EACH CONTROL CENTER IS AS FOLLOWS:

CONTROL CENTER NO. 1

CIRCUIT A : CONVERT TO 3-WIRE 480 VOLT GROUNDED NEUTRAL SYSTEM;LOAD IS 8.9 AMPS

CIRCUIT B : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; LOAD IS 12.0 AMPS

CIRCUIT D : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; CALCULATED LOAD IS 12.0 AMPS

CIRCUIT E : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; CALCULATED LOAD IS 20.7 AMPS

CIRCUIT H : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; LOAD IS 21.2 AMPS

CONTROL CENTER NO. 2 NO WORK

CONTROL CENTER NO. 3

REMOVE EXISTING LIGHTING CONTROL CENTER NO. 3 IN PUMP STATION ENCLOSURE.

CONSTRUCT NEW CONTROL CENTER NO. 3 AS SHOWN ON PLANS, INCLUDING: CONCRETE PAD, LIGHTING PANEL BRACKETS AND SUPPORTS, TRANSFORMERS WITH ENCLOSURES, CONTROL ENCLOSURES, METERS, PHOTOCELL AND BOLLARDS FOR PROTECTION. THE CONTROL CENTER SHALL HAVE TWO PURPOSES: THE FIRST TO SUPPLY THE LIGHTING CIRCUIT AND THE SECOND, TO SUPPLY THE WEATHER STATION. IT SHALL BE FED BY A NEW UNDER-GROUND CIRCUIT FROM AN EXISTING POWER POLE ABOUT 245 FT. TO THE EAST.

CIRCUIT C : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; CALCULATED LOAD IS 14.8 AMPS

CIRCUIT I : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; CALCULATED LOAD IS 29.6 AMPS

CIRCUIT J : CONVERT TO 3-WIRE, 480 VOLT GROUNDED NEUTRAL SYSTEM; CALCULATED LOAD IS 14.4 AMPS

HIGH MAST LUMINAIRES

THE LUMINAIRE ARRAYS AND ASSOCIATED ILLUMINATION TEST AREAS SPECIFIED IN SECTION 713.21 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS ARE HEREBY WAIVED FOR THIS PROJECT. INSTEAD, LUMINAIRES FOR TOWER LIGHTING SHALL MEET THE FOLLOWING REQUIREMENTS.

SYMMETRIC, TYPE V, LUMINAIRES FOR TOWER LIGHTING MAY BE HOLOPHANE "HMST" TEST #36383, OR GENERAL ELECTRIC "HM" TEST #6312, OR COOPER "HAL" TEST #48381. ALL HIGH MAST LUMINAIRES PROVIDED SHALL BE OF THE SAME MANUFACTURER.

IN ADDITION, OTHER LUMINAIRES WILL BE CONSIDERED IF THE DESIGNED INTENSITY AND UNIFORMITY ARE PROVIDED USING THE DESIGNED POLE LOCATIONS AND THE DESIGNED NUMBER AND TYPE OF FIXTURE PER POLE.

625 SERVICE TO UNDERPASS LIGHTING, AS PER PLAN

THE CONTRACTOR SHALL REPLACE THE TWO CONDUCTORS LEADING TO THE UNDERPASS LUMINAIRES, FROM THE SPLICE TO DISTRIBUTION CABLE AT THE ADJACENT PULLBOX OR JUNCTION BOX, WITH NEW 3-CONDUCTOR # 10 AWG POLE AND BRACKET CABLE. BROKEN OR DISCONNECTED CONDUIT, OR CONDUIT AND FITTINGS THAT ARE NOT SECURELY ATTACHED, SHALL BE REPAIRED, REPLACED AND/OR FASTENED SECURELY.

625 LIGHTING, MISCELLANEOUS: MODIFY LIGHT TOWER

- ADD AND RESPACE LUMINAIRE MOUNTING ARMS ON LUMINAIRE RING AS REQUIRED.
- MODIFY AND REPLACE LUMINAIRE RING WIRING AS REQUIRED.
- WIRING LABELS AND POLE DECALS SHALL BE SERVICEABLE AND REFLECT THE COMPLETED CIRCUITRY.
- ANY BROKEN, DAMAGED OR INOPERABLE PARTS SHALL BE REPAIRED OR REPLACED AS A PART OF THIS WORK.

PAYMENT

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM "SPECIAL - MISCELLANEOUS: LIGHTING".

625 SERVICE TO UNDERPASS LIGHTING

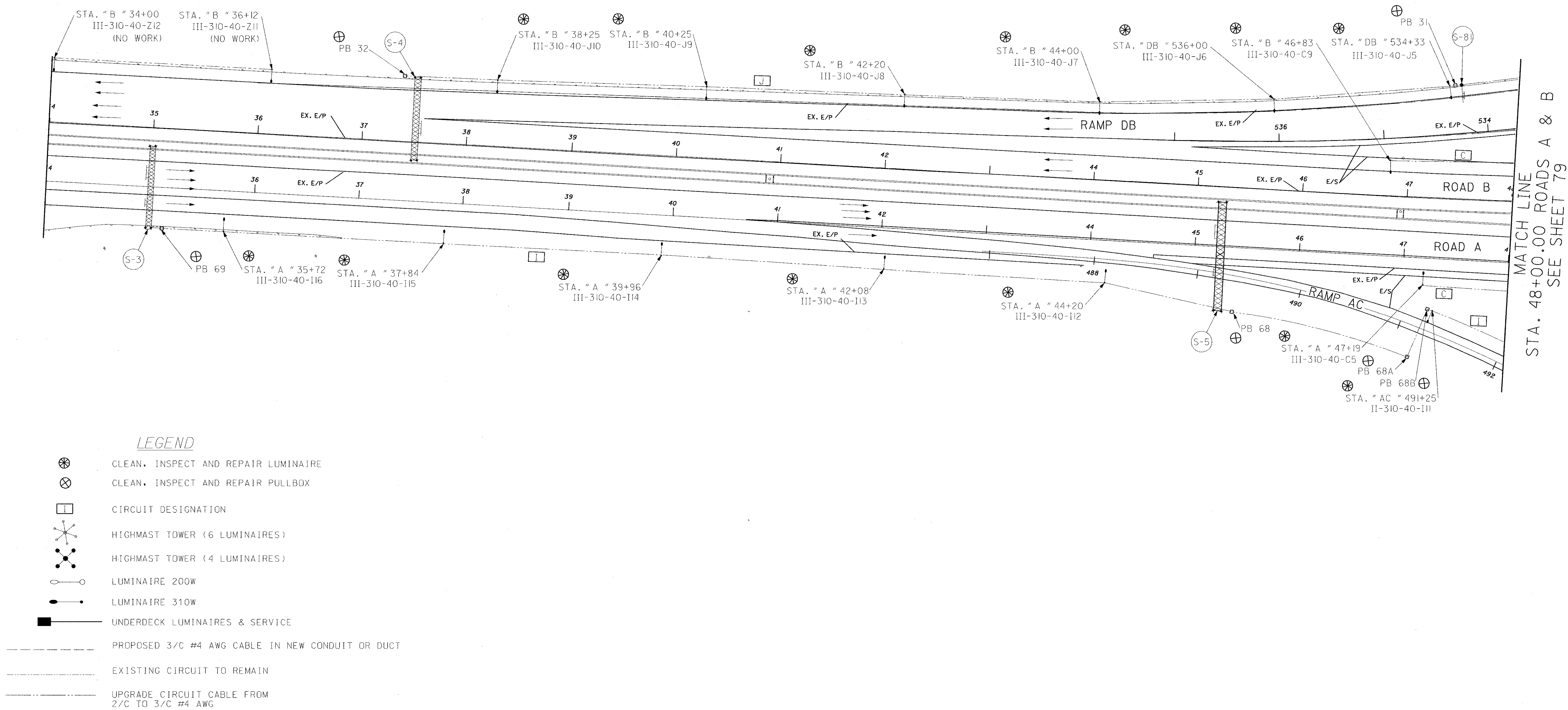
THIS ITEM SHALL INCLUDE ALL LABOR EQUIPMENT AND MATERIALS REQUIRED TO PROVIDE ELECTRICAL SERVICE TO A GROUP OF UNDERPASS LUMINAIRES, FROM AN ADJACENT PULLBOX. CONDUCTORS SHALL BE (3) NO. 10 AWG POLE AND BRACKET CABLE, WHICH SHALL BE ENCLOSED IN RIGID METAL CONDUIT AND FITTINGS IN ACCORDANCE WITH ITEM 713.04.

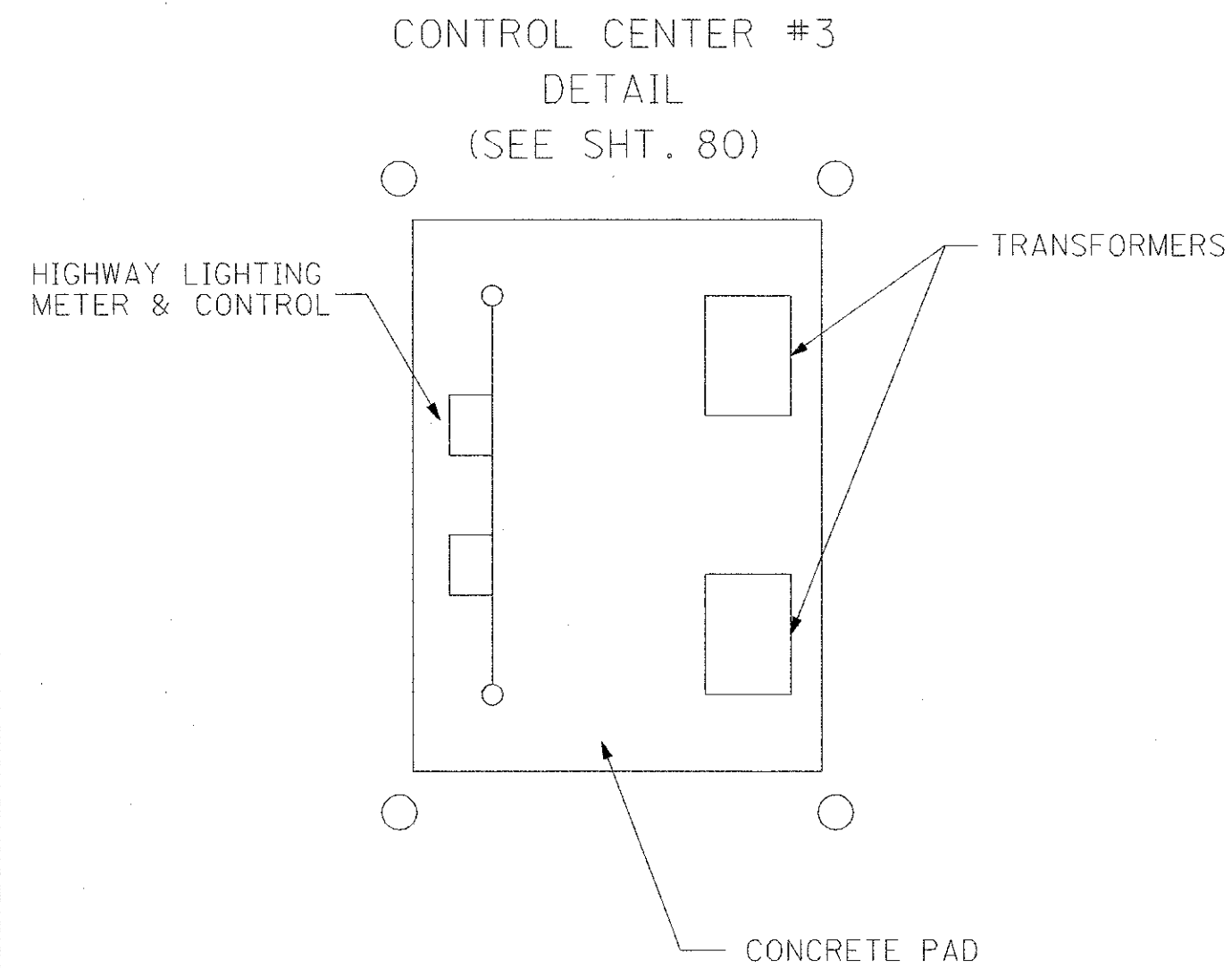
CALCULATED  
GSC-  
CHECKED

LIGHTING PLAN NOTES








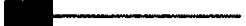



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FRA - 315-0.00

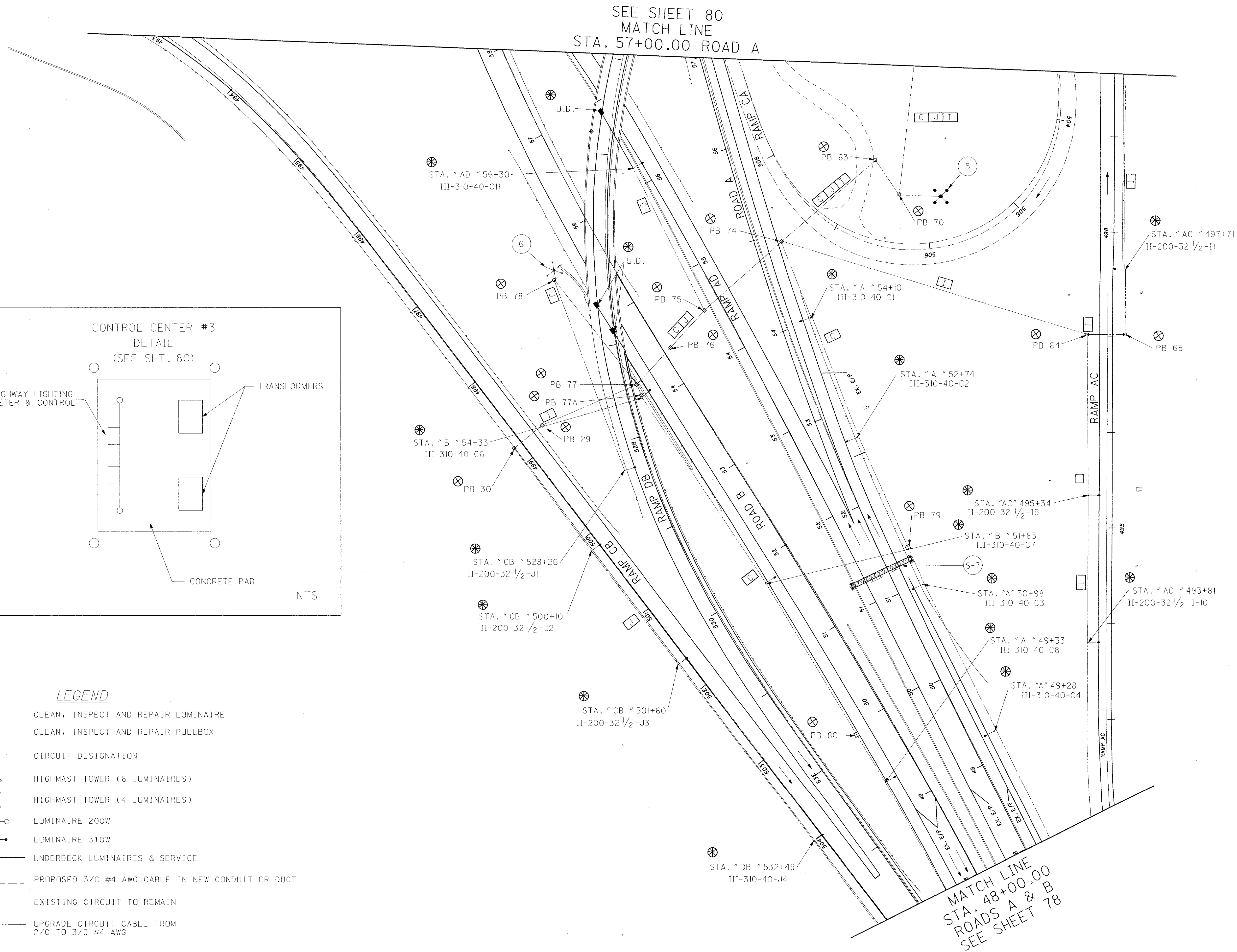






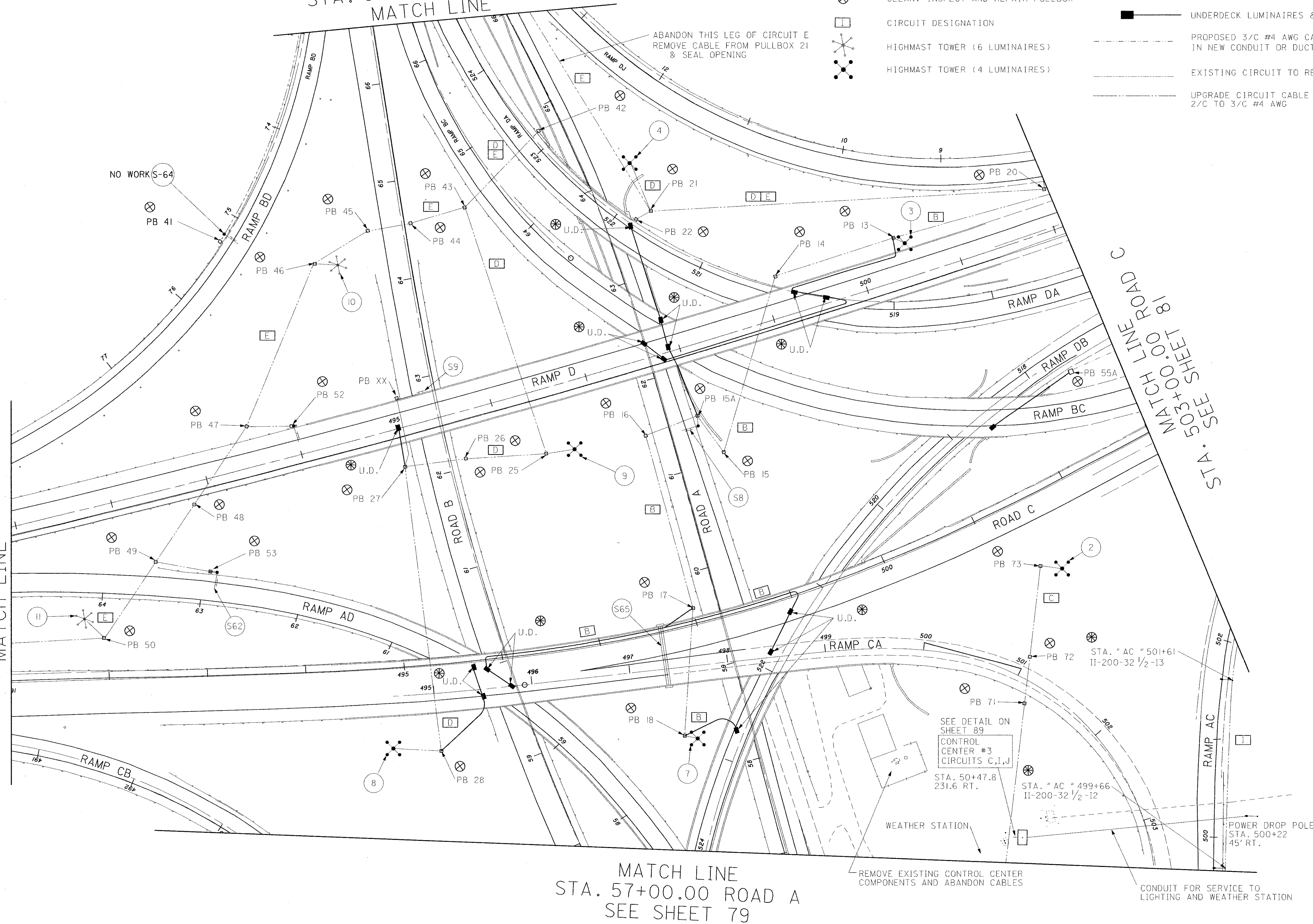
LEGEND

- |   |  |
|---|--|
|  | CLEAN, INSPECT AND REPAIR LUMINAIRE              |
|  | CLEAN, INSPECT AND REPAIR PULLBOX                |
|  | CIRCUIT DESIGNATION                              |
|  | HIGHMAST TOWER ( 6 LUMINAIRES )                  |
|  | HIGHMAST TOWER ( 4 LUMINAIRES )                  |
|  | LUMINAIRE 200W                                   |
|  | LUMINAIRE 310W                                   |
|  | UNDERDECK LUMINAIRES & SERVICE                   |
|  | PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT |
|  | EXISTING CIRCUIT TO REMAIN                       |
|  | UPGRADE CIRCUIT CABLE FROM<br>2/C TO 3/C #4 AWG  |



SEE SHEET 84  
STA. 491+00.00 ROAD C  
MATCH LINE

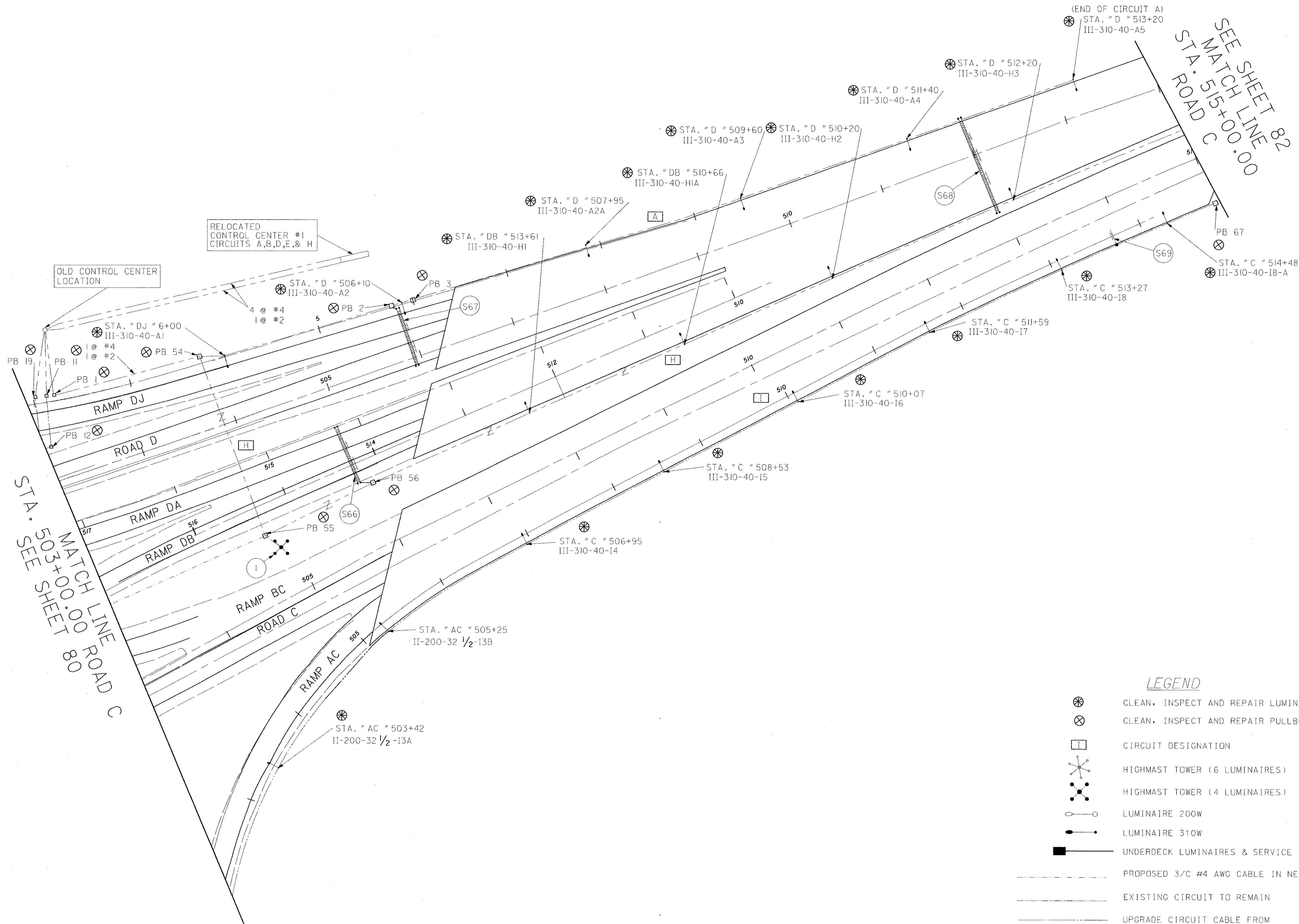
SEE SHEET 85  
STA. 66+00.00 ROAD A  
MATCH LINE



MATCH LINE  
STA. 57+00.00 ROAD A  
SEE SHEET 79

- LEGEND**
- CLEAN, INSPECT AND REPAIR LUMINAIRE
  - CLEAN, INSPECT AND REPAIR PULLBOX
  - CIRCUIT DESIGNATION
  - HIGHMAST TOWER (6 LUMINAIRES)
  - HIGHMAST TOWER (4 LUMINAIRES)
  - LUMINAIRE 200W
  - LUMINAIRE 310W
  - UNDERDECK LUMINAIRES & SERVICE
  - PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
  - EXISTING CIRCUIT TO REMAIN
  - UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG





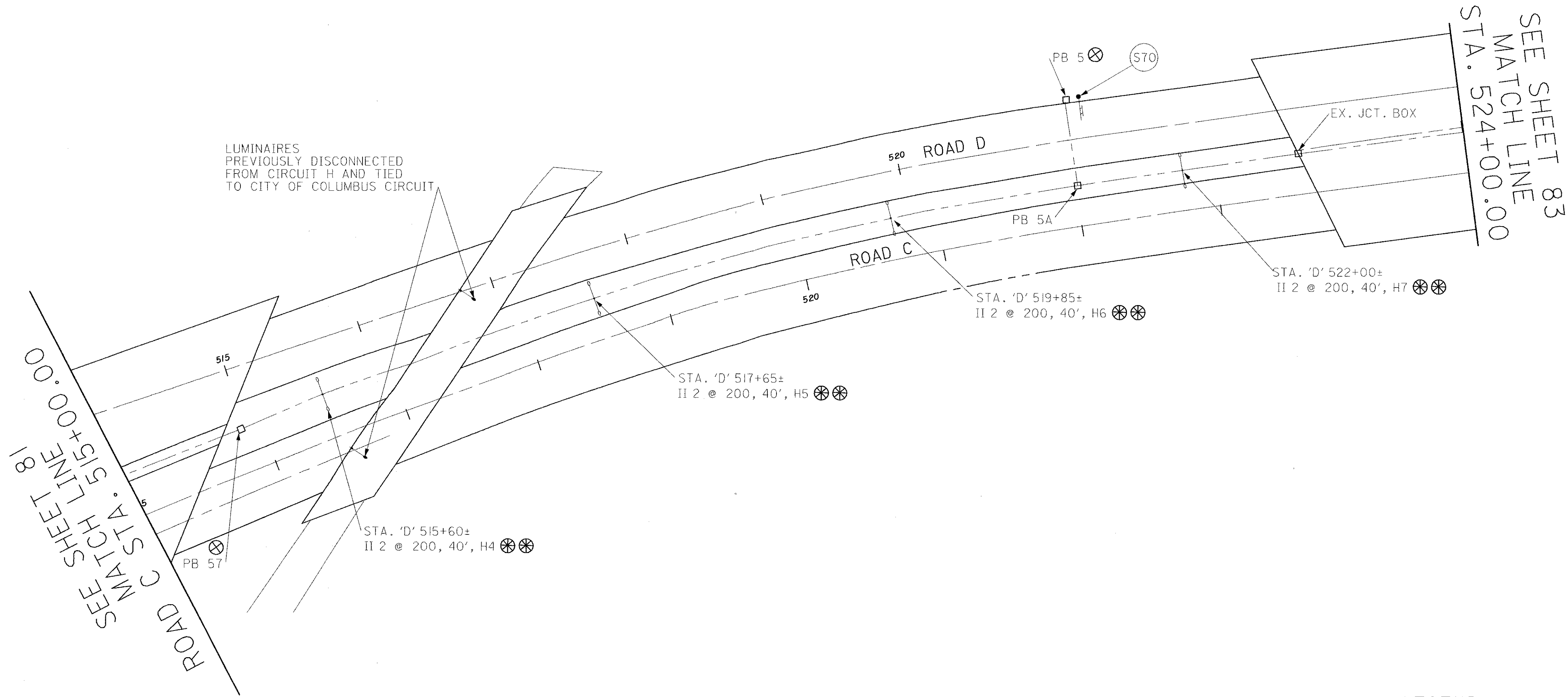
LEGEND

- CLEAN, INSPECT AND REPAIR LUMINAIRE
- CLEAN, INSPECT AND REPAIR PULLBOX
- CIRCUIT DESIGNATION
- HIGHMAST TOWER (6 LUMINAIRES)
- HIGHMAST TOWER (4 LUMINAIRES)
- LUMINAIRE 200W
- LUMINAIRE 310W
- UNDERDECK LUMINAIRES & SERVICE
- PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
- EXISTING CIRCUIT TO REMAIN
- UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG
- 2/C TO 3/C #2 AWG



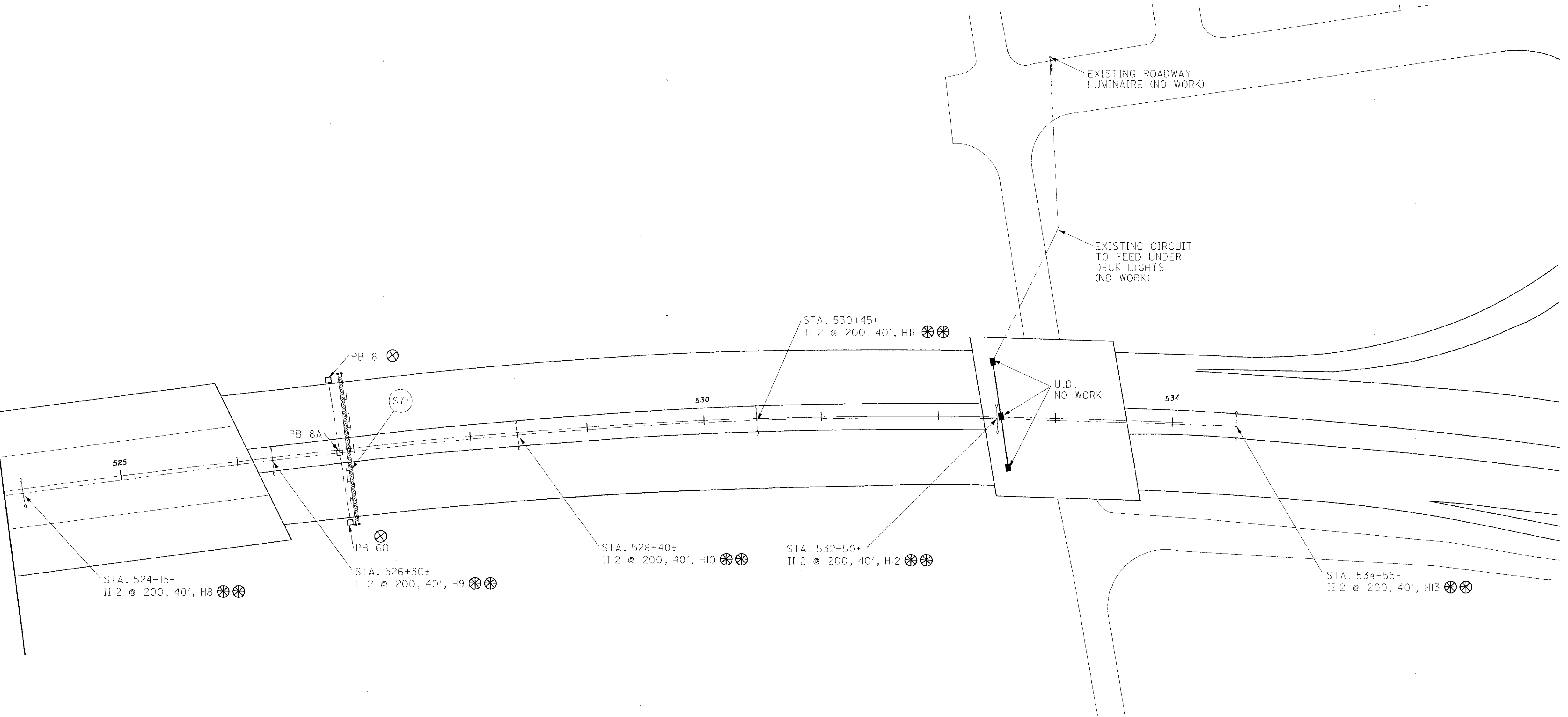
LIGHTING PLAN  
ROAD C STA. 503+00 TO ROAD C 514+00

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FRA-315-0.0



- LEGEND**
- CLEAN, INSPECT AND REPAIR LUMINAIRE
  - CLEAN, INSPECT AND REPAIR PULLBOX
  - CIRCUIT DESIGNATION
  - HIGHMAST TOWER (6 LUMINAIRES)
  - HIGHMAST TOWER (4 LUMINAIRES)
  - LUMINAIRE 200W
  - LUMINAIRE 310W
  - UNDERDECK LUMINAIRES & SERVICE
  - PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
  - EXISTING CIRCUIT TO REMAIN
  - UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG
  - TWIN LUMINAIRE 200W

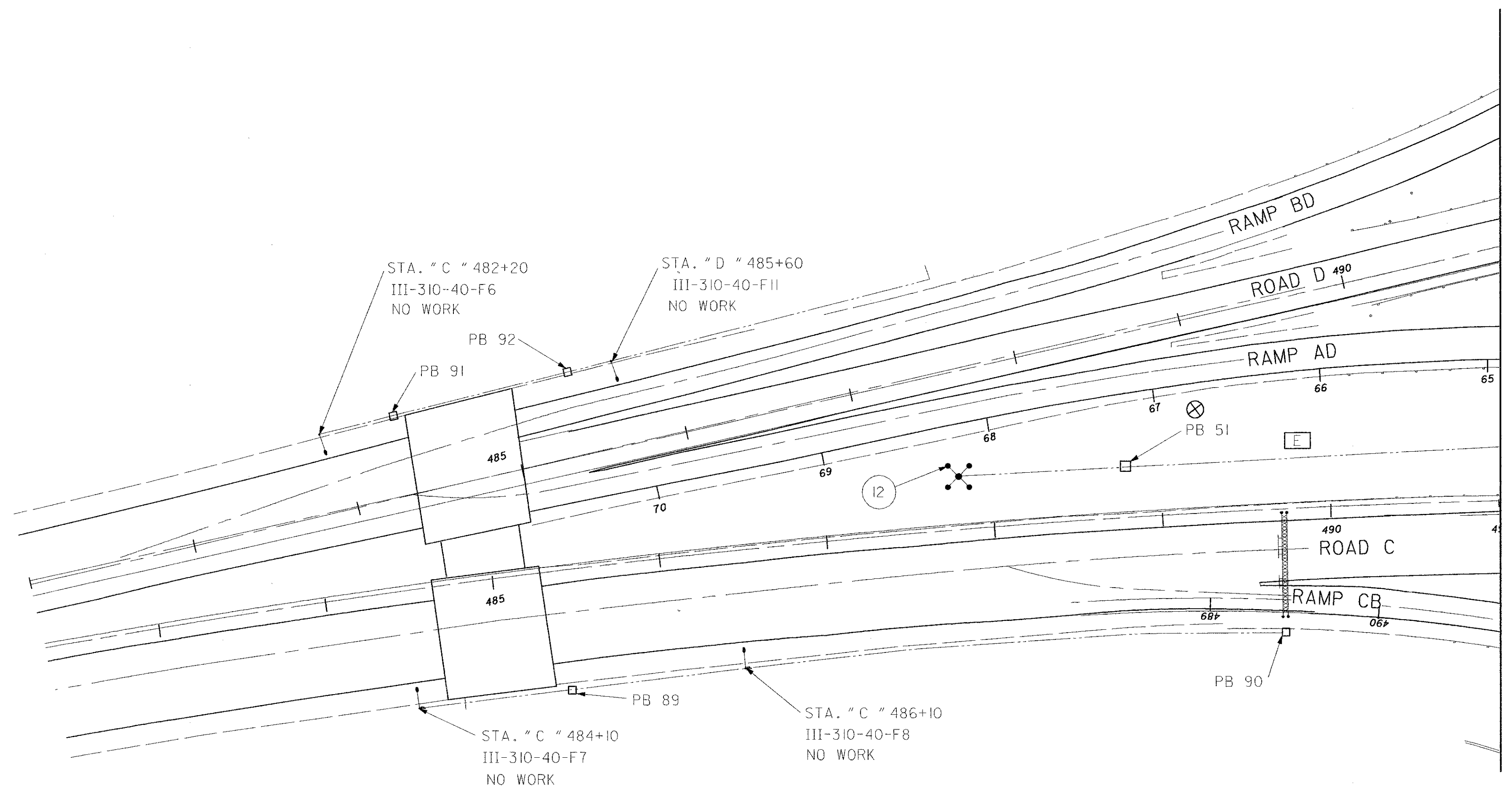
SEE SHEET 82  
MATCH LINE  
STA. 524+00.00



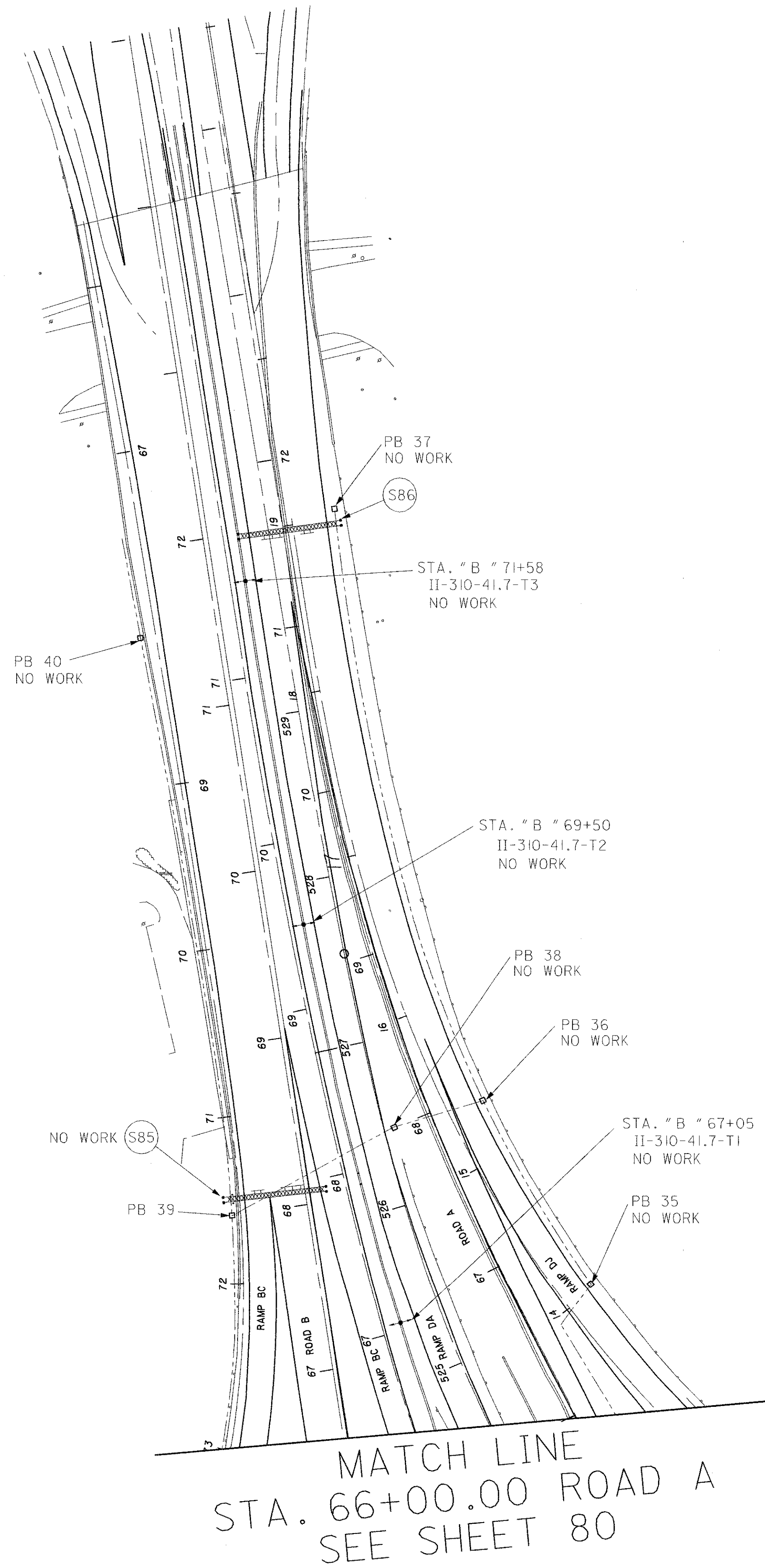
- LEGEND**
- CLEAN, INSPECT AND REPAIR LUMINAIRE
  - CLEAN, INSPECT AND REPAIR PULLBOX
  - CIRCUIT DESIGNATION
  - HIGHMAST TOWER (6 LUMINAIRES)
  - HIGHMAST TOWER (4 LUMINAIRES)
  - LUMINAIRE 200W
  - LUMINAIRE 310W
  - UNDERDECK LUMINAIRES & SERVICE
  - PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
  - EXISTING CIRCUIT TO REMAIN
  - UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG
  - TWIN LUMINAIRE 200W



- LEGEND**
- CLEAN, INSPECT AND REPAIR LUMINAIRE
  - CLEAN, INSPECT AND REPAIR PULLBOX
  - CIRCUIT DESIGNATION
  - HIGHMAST TOWER (6 LUMINAIRES)
  - HIGHMAST TOWER (4 LUMINAIRES)
  - LUMINAIRE 200W
  - LUMINAIRE 310W
  - UNDERDECK LUMINAIRES & SERVICE
  - PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
  - EXISTING CIRCUIT TO REMAIN
  - UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG



MATCH LINE  
ROAD C STA. 491+00  
SEE SHEET 80

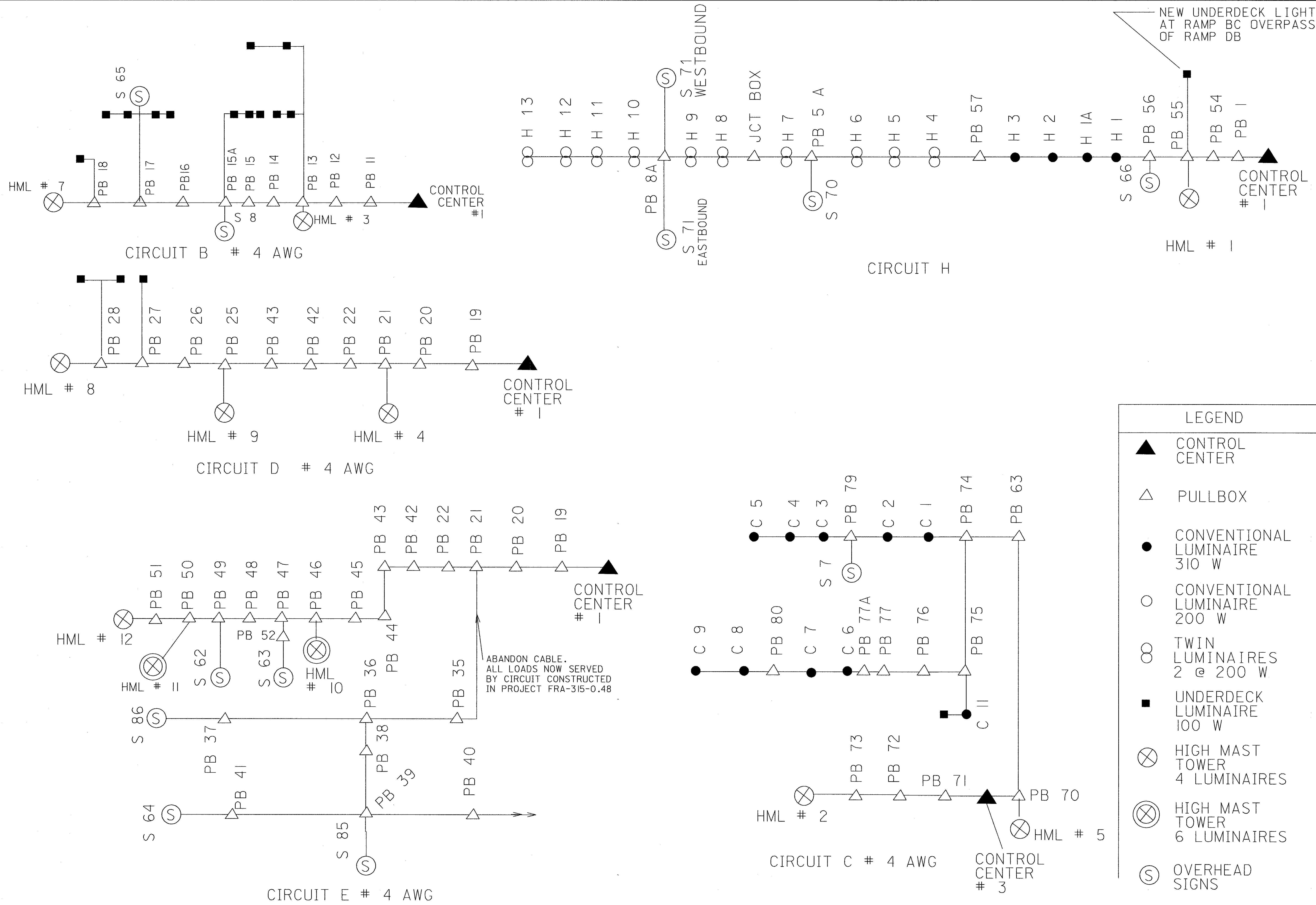


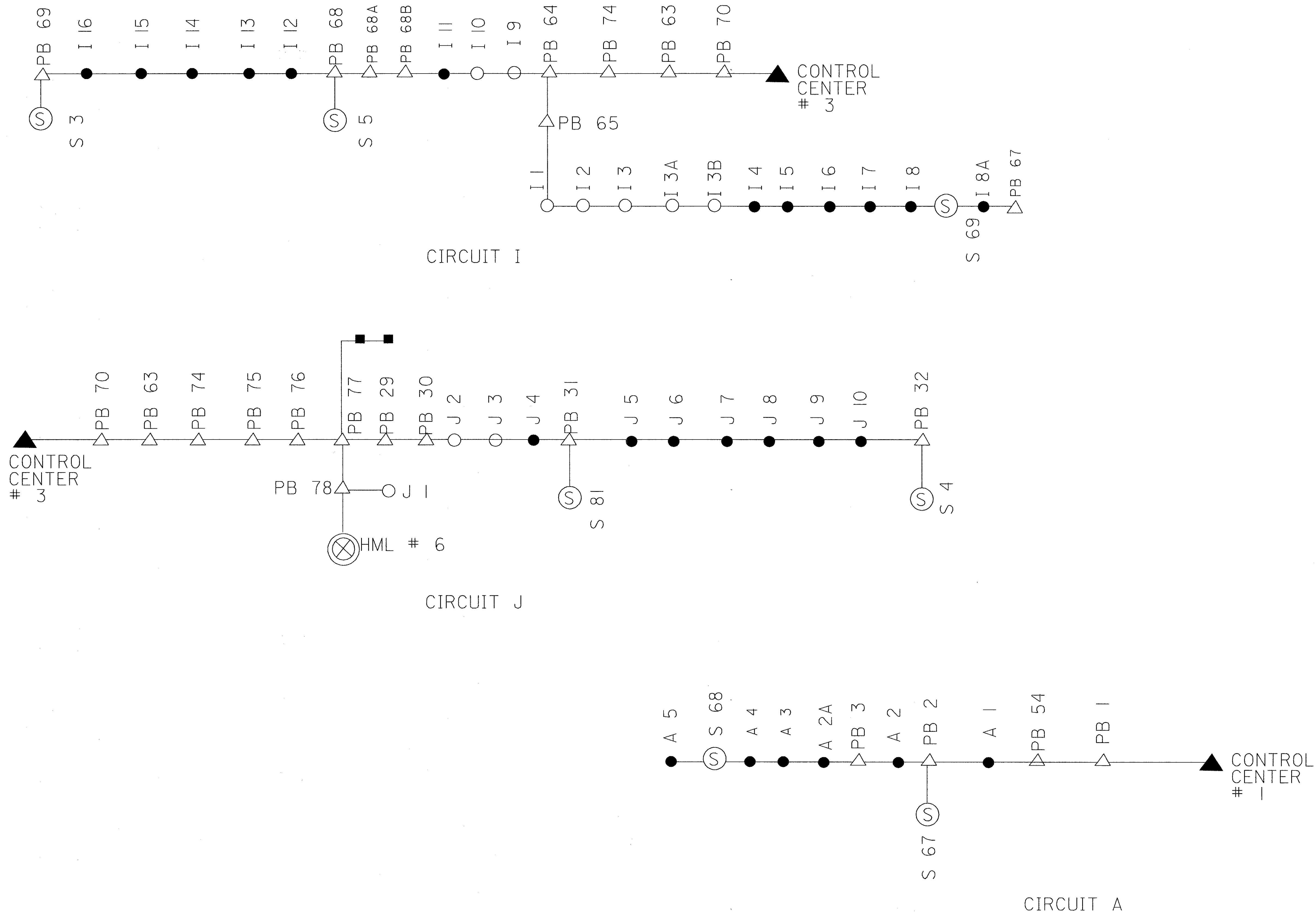
- LEGEND**
- CLEAN, INSPECT AND REPAIR LUMINAIRE
  - CLEAN, INSPECT AND REPAIR PULLBOX
  - CIRCUIT DESIGNATION
  - HIGHMAST TOWER (6 LUMINAIRES)
  - HIGHMAST TOWER (4 LUMINAIRES)
  - LUMINAIRE 200W
  - LUMINAIRE 310W
  - UNDERDECK LUMINAIRES & SERVICE
  - PROPOSED 3/C #4 AWG CABLE IN NEW CONDUIT OR DUCT
  - EXISTING CIRCUIT TO REMAIN
  - UPGRADE CIRCUIT CABLE FROM 2/C TO 3/C #4 AWG

**FRA-71-14.39**  
**FRA-315-0.00**



CALCULATED	GSC	CHECKED
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LEGEND	
▲	CONTROL CENTER
△	PULLBOX
●	CONVENTIONAL LUMINAIRE 310 W
○	CONVENTIONAL LUMINAIRE 200 W
■	UNDERDECK LUMINAIRE 100 W
⊗	HIGH MAST TOWER 4 LUMINAIRES
⊗	HIGH MAST TOWER 6 LUMINAIRES
Ⓢ	OVERHEAD SIGNS